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Community &  
Residential  
Commercial &  
Institutional  
Environmental  
Restoration

## 1101 Baxter Road

### Planning Rationale

Prepared for: Baxter Regional Inc.

**1101 Baxter Road**

**Ottawa, Ontario**

**Planning Rationale**

**in support of**

**Draft Plan of Subdivision**

Prepared for:

**Baxter Regional Inc.**

Prepared By:

**NOVATECH**

Suite 200, 240 Michael Cowpland Drive  
Ottawa, Ontario  
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December / 23 / 2025

Novatech File: 121019  
Ref: R-2025-123

December 23, 2025

City of Ottawa  
Planning, Development, and Building Services Department  
110 Laurier Avenue West, 4<sup>th</sup> Floor  
Ottawa, ON, K1P 1J1

**Attention: John Bernier, Planner II, Development Review West**

**Reference: Draft Plan of Subdivision  
1101 Baxter Road  
Our File No.: 121019**

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The following Planning Rationale has been prepared in support of a Draft Plan of Subdivision to facilitate a mixed-use development at 1101 Baxter Road (the "Subject Site").

The Subject Site is designated Hub in the Inner Urban Transect of the City of Ottawa Official Plan (2022). The Subject Site is designated Hub, located within the Iris-Baxter Sector of the Pinecrest and Queensview Stations Secondary Plan. The Subject Site is zoned Mixed Use Centre, Subzone 20, Schedule 508 (MC20 S508) in the City of Ottawa Zoning By-law 2008-250.

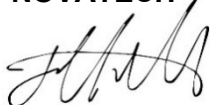
It is proposed to subdivide the Subject Site into six blocks to facilitate future development of mixed-use buildings ranging from mid to high-rise, a central park block, a private street, private blocks for combined servicing and a public block for a pedestrian pathway. The intent of this Draft Plan application is to subdivide the land and secure adequate servicing capacity for future development. Separate applications for Site Plan Control and Rezoning will be submitted for each future mixed-use block.

This Planning Rationale examines the location and context of the Subject Site, provides a description of the proposed subdivision, sets out the applicable planning policy and regulatory framework for the Subject Site in support of the proposed subdivision.

Should you have any questions regarding any aspect of this application please feel free to contact me at your earliest convenience.

Yours truly,

**NOVATECH**



Jeffrey Kelly, MCIP, RPP  
Project Manager | Planning & Development

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## EXECUTIVE SUMMARY

Novatech has been retained by Baxter Regional Inc. to prepare this Planning Rationale in support of a Plan of Subdivision application to facilitate future development of a mixed-use subdivision on 1101 Baxter Road (“the Subject Site”).

The Subject Site is located in College Ward (Ward 8) in the City of Ottawa. The Subject Site is located in an area bounded by Highway 417 to the north, Iris Street to the south and Greenbank Road to the west. Woodroffe Avenue is located further to the east of the Subject Site. The Subject Site has approximately 354 metres of frontage along Baxter Road and an approximate area of 3.13 hectares. The Subject Site is located in an area comprised of retail, office and commercial uses to the east, west and north across Highway 417. To the south, the area is comprised primarily of residential uses along Dempsey Avenue, Elmira Drive and the rest of the Ridgeview Neighbourhood. The Subject Site is currently developed with the former Ottawa Citizen building which will be demolished at some time in the future to facilitate redevelopment.

The proposed subdivision will create six blocks for future mixed-use development. The blocks are oriented to permit the development of mid- to high-rise buildings organized around a central pedestrian-oriented area along a new private street (Street One). A public park block will be located at the centre of the community, accessible via Street One and a planned multi-use pathway along the southern lot line.

This proposal is consistent with the 2024 Provincial Planning Statement by supporting growth within a settlement area near rapid transit. It will enable the creation of additional housing units within the urban boundary, diversify dwelling types, and establish a complete community by providing a mix of uses on each newly created block. The conceptual development proposal promotes active transportation by incorporating sidewalks, a pathway and cycling connections to Baxter Road and Iris Street. The location within an established area supports efficient use of land and minimizes servicing costs.

The Subject Site is designated Hub in the Inner Urban Transect under the City of Ottawa Official Plan (2022) and within the Iris-Baxter Sector of the Pinecrest and Queensview Stations Secondary Plan. The proposed subdivision will support residential intensification by creating blocks for approximately 1,400 dwelling units in a mix of mid-rise and high-rise forms, with opportunities for ground-floor commercial uses along Baxter Road, at Street One intersections, and near the Queensview Station pedestrian bridge. The plan conforms to the Official Plan and Secondary Plan policies.

The Subject Site is zoned Mixed Use Centre, Subzone 20, Schedule 508 (MC20 S508) in the City of Ottawa Zoning By-law 2008-250. The proposed subdivision will create blocks that will each be suitable to support mixed use, mid to high-rise development. With the exception of maximum building height limits, the proposed development will conform with the provisions of the Zoning By-law for the MC20 zone. Separate Site Plan and Zoning By-law amendment applications are required to bring the Zoning By-law into conformity with the City’s approved Secondary Plan regarding heights.

**The Plan of Subdivision application is appropriate for the development of the Subject Site and represents good land use planning.**

## **1.0 INTRODUCTION**

Novatech has prepared this Planning Rationale in support of a Plan of Subdivision application to facilitate the future development of a mixed-use subdivision on the property municipally known as 1101 Baxter Road (the “Subject Site”). The subdivision will consist of multiple mixed-use blocks. The blocks are oriented to permit the development of buildings ranging from mid to high-rise, a central park block, a private street off Baxter Road and blocks for combined servicing and pathways.

The Subject Site is designated Hub in the Inner Urban Transect of the City of Ottawa Official Plan (2022). The Subject Site is designated Hub, located within the Iris-Baxter Sector of the Pinecrest and Queensview Stations Secondary Plan. The Subject Site is zoned Mixed Use Centre, Subzone 20, Schedule 508 (MC20 S508) in the City of Ottawa Zoning By-law 2008-250.

This Planning Rationale will demonstrate that the Plan of Subdivision will:

- Be consistent with the policies of the Provincial Planning Statement (2024);
- Conform to the policies of the City of Ottawa Official Plan (2022);
- Conform to Zoning standards for the Subject Site specifically with respect to the creation of blocks; and
- Ensure the conceptual development proposal is sensitive to surrounding land uses and the broader community context.

1.1 Site Location and Description

Figure 1: Subject Site and Surrounding Area (Source: GeoOttawa, 2022)



The Subject Site is located in College Ward (Ward 8) in the City of Ottawa. The Subject Site is located in an area bounded by Highway 417 to the north, Iris Street to the south and Greenbank Road to the west (see Figure 1). Woodroffe Avenue is located further to the east of the Subject Site. The Subject Site has approximately 354 metres of frontage along Baxter Road and an approximate area of 3.13 hectares. The Subject Site is a single parcel with the municipal address of 1101 Baxter Road.

**Figure 2: Subject Site from Baxter Road (Looking South)** (Source: Google Streetview, Oct 2024)



The Subject Site currently contains the Ottawa Citizen building and surface parking. A recently completed pedestrian bridge connects the Subject Site to Queensview Station at the north across Highway 417 (see Figure 2). The existing building on the Subject Site will be demolished at some time in the future to facilitate redevelopment.

### 1.2 Community Context

The Subject Site is located in an area comprised of retail, office and commercial uses to the east, west and north across Highway 417. To the south, the area is comprised primarily of residential uses along Dempsey Avenue.

Directly to the north of the Subject Site, is Baxter Road, and Highway 417. Further to the north, a recently completed pedestrian crossing provides access to Queensview Station and a mixed employment area. Directly to the south of the Subject Site is Dempsey Avenue, Elmira Drive and the rest of the Ridgeview Neighbourhood. Further to the south of the Subject Site is Iris Street, providing a roadway connection west to Greenbank Road. Directly to the west of the Subject Site is IKEA Ottawa and a mix of large format retail uses. Directly to the east of the Subject Site is a mixed commercial area.

**Figure 3: Land uses to the north of the Subject Site** (Source: Google Streetview, Oct 2024)



**North:** Immediately north of the Subject Site across Baxter Road is Highway 417 and a pedestrian bridge.

**Figure 4: Land uses to the south of the Subject Site** (Source: Google Streetview, Oct 2024)



**South:** Immediately south of the Subject Site are low rise residential homes along Dempsey Avenue. A local retail plaza is located to the south of the Subject Site along Baxter Road.

**Figure 5: Land uses to the west of the Subject Site** (Source: Google Streetview, October 2024)



**West:** Immediately west of the Subject Site is IKEA Ottawa and a range of large retail and restaurant uses.

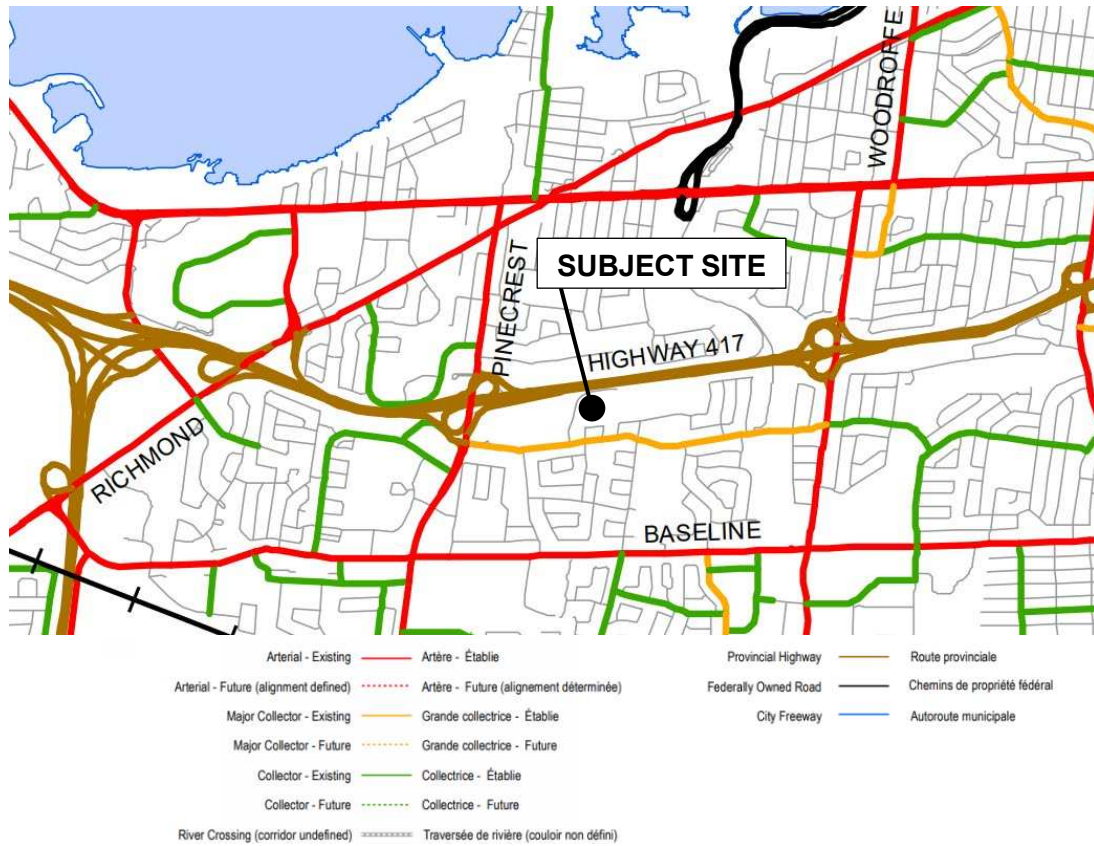
**Figure 6: Land uses to the east of the Subject Site** (Source: Google Streetview, October 2024)



**East:** Immediately east of the Subject Site is a mixed commercial area.

1.3 Connectivity

Figure 7: Official Plan Schedule C4 Excerpt

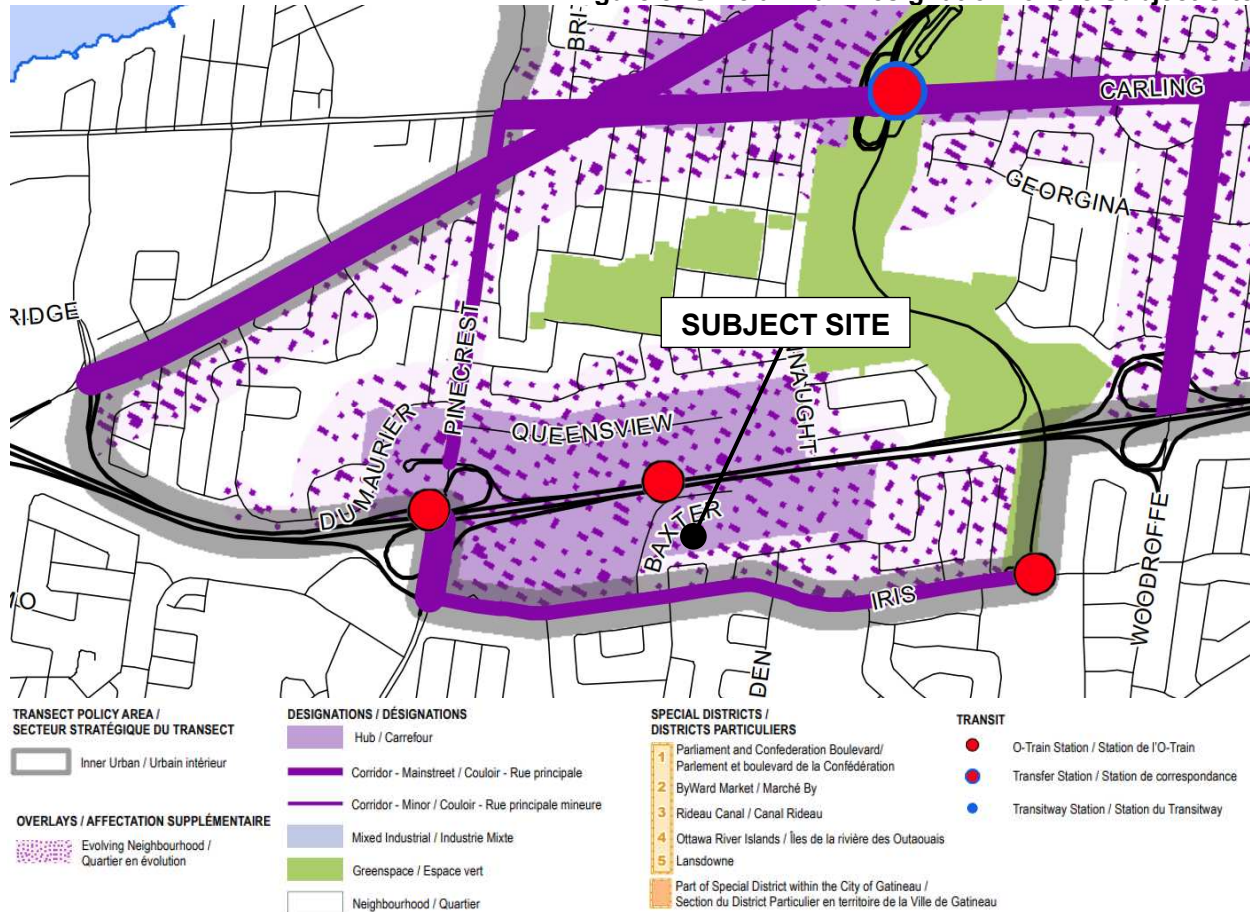


The Subject Site has frontage on Baxter Road with a direct connection to Iris Street. Iris Street is designated as *Major Collector - Existing* on *Schedule C4 – Urban Road Network* of the Official Plan (see Figure 7). Queensview LRT station is located immediately to the north of the Subject Site across Highway 417, accessed via a pedestrian bridge. Queensview Station provides access to rapid transit within a 5 minute walk from the Subject Site.

## 1.4 Planning and Regulatory Context

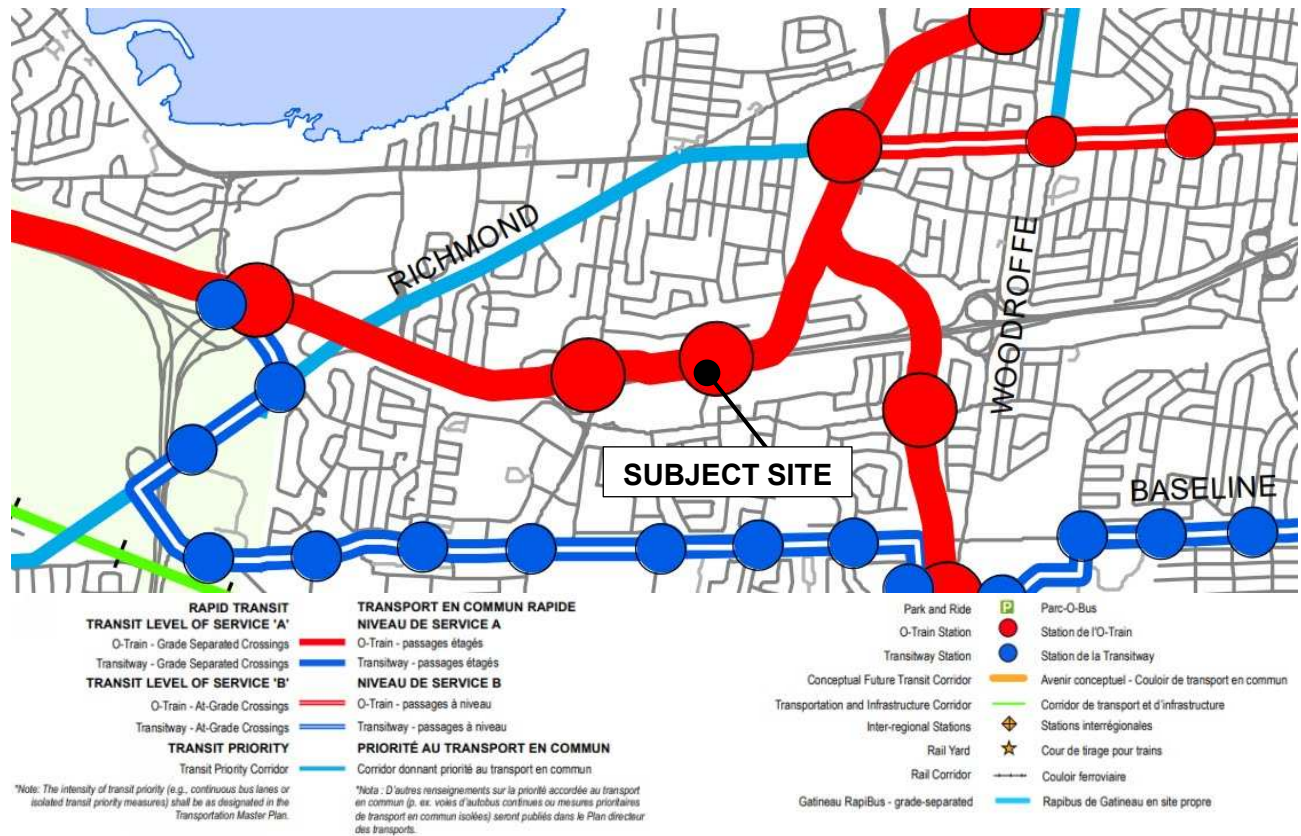
### 1.4.1 City of Ottawa Official Plan

Figure 8: Official Plan Designation for the Subject Site



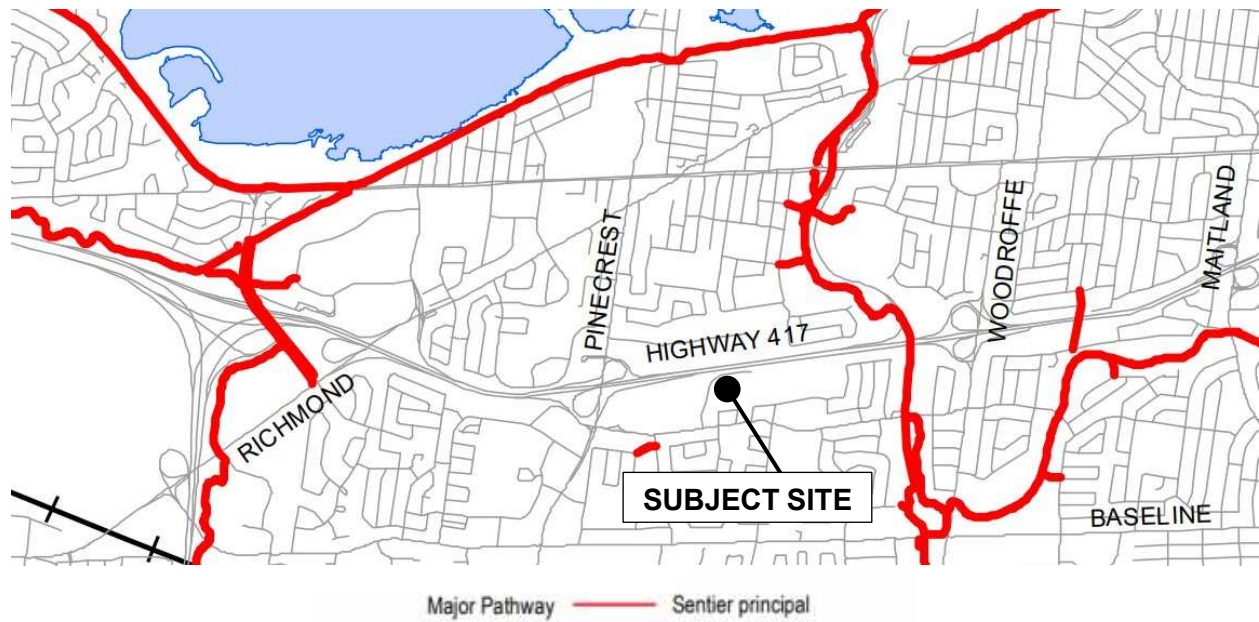
The Subject Site is designated Hub in the Inner Urban Transect of the City of Ottawa Official Plan (2022) (see Figure 8). The Subject Site is designated Hub, located within the Iris-Baxter Sector of the Pinecrest and Queensview Stations Secondary Plan. Policies of the Secondary Plan take precedence over policies of the City of Ottawa Official Plan.

Figure 9: Official Plan Schedule C2 Excerpt



Schedule C2 – Transit Network Ultimate identifies “O-Train – Grade Separated Crossings” to the north of the Subject Site across Highway 417. Queensview Station will provide rapid transit service on Line 3 of the O-Train West connection, accessed via a pedestrian bridge from the Subject Site.

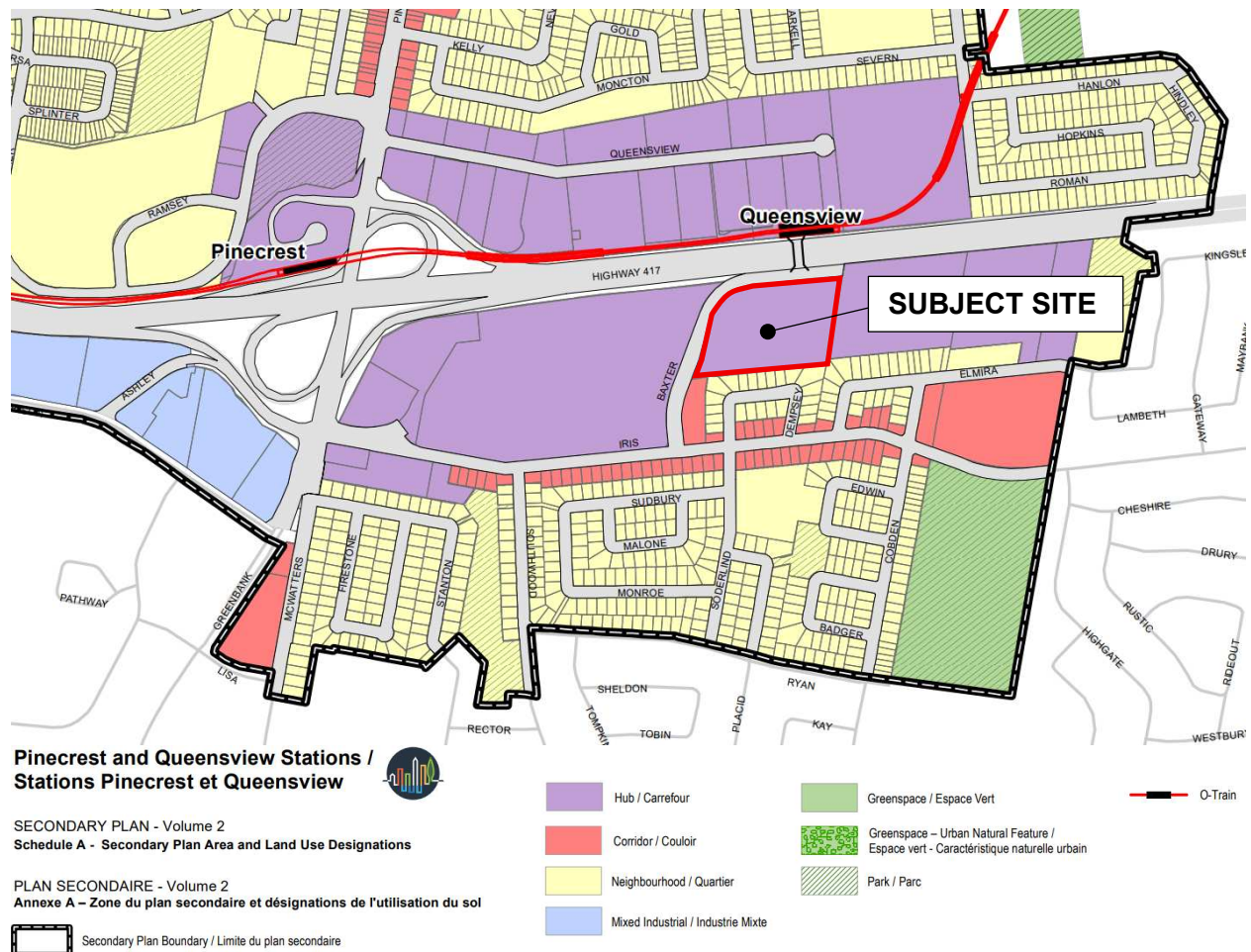
Figure 10: Official Plan Schedule C3 Excerpt



*Schedule C3 – Active Transportation Network (Urban – Major Pathways)* Active transportation infrastructure is provided to the south of the Subject Site along Iris Street with connections to Greenbank Road to the west and to the Pinecrest Creek Pathway located to the east of the Subject Site (see Figure 10).

### 1.4.2 Pinecrest and Queensview Stations Secondary Plan

Figure 11: Pinecrest and Queensview Stations Secondary Plan Schedule A Excerpt



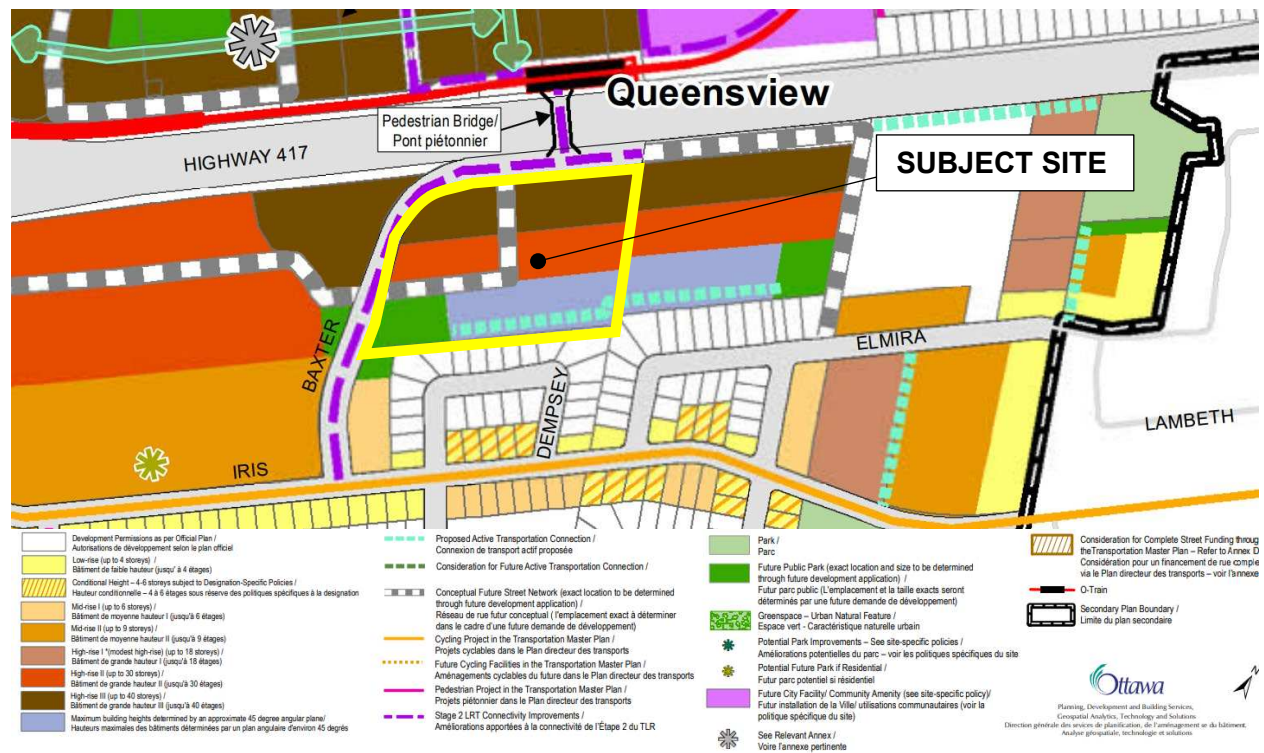
The Subject Site is designated Hub, located within the Iris-Baxter Sector of the Pinecrest and Queensview Stations Secondary Plan. Policies of the Secondary Plan take precedence over policies of the City of Ottawa Official Plan (see Figure 11).

Section 3.1 of the Pinecrest and Queensview Stations Secondary Plan provides policy direction with respect to Infrastructure and Servicing capacity for the entire plan area. The preamble to infrastructure policies under Section 3.1 of the Secondary Plan states:

*“there is limited sewer capacity in the Plan Area and any available sewer capacity is considered on a first-come, first-served basis through development applications”.*

The intent of this plan of subdivision application is to reserve servicing capacity on the basis of “first come, first-served”.

Figure 12: Pinecrest and Queensview Stations Secondary Plan – Schedule C Excerpt



Section 3.4, Policy 1 of the Secondary Plan states:

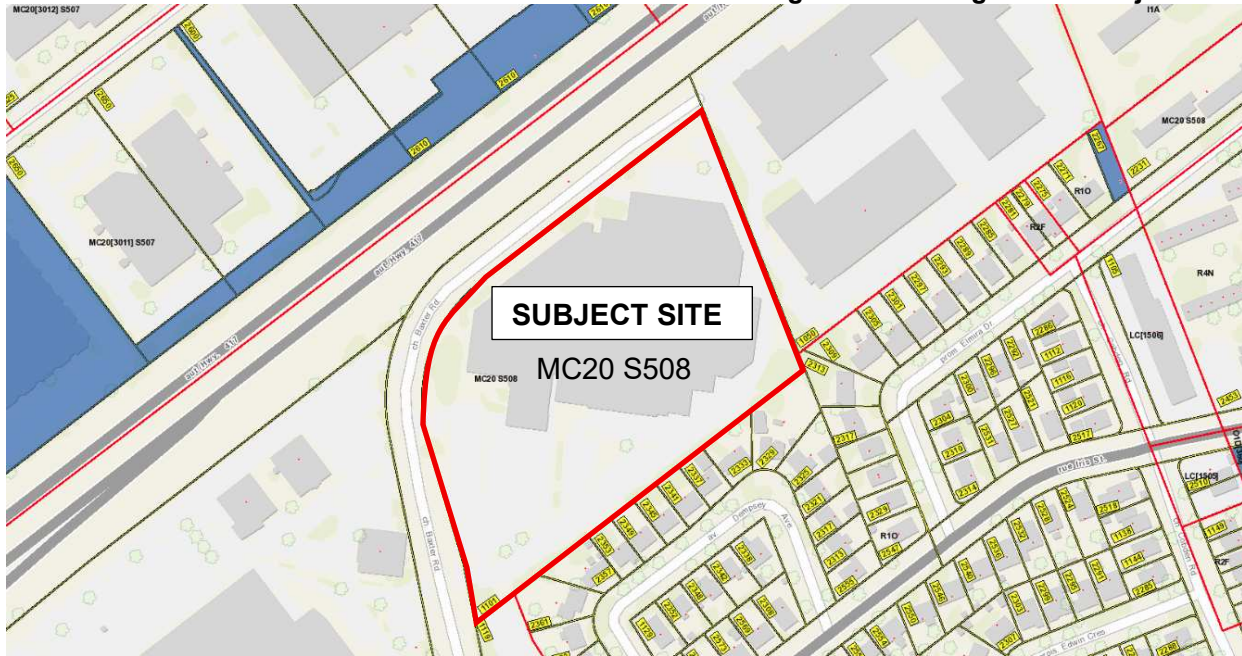
*“Maximum building heights are identified in Schedule C – Maximum Building Heights, Connectivity and Parks, and in some cases are further detailed in Designation-Specific Policies and Site-Specific Policies.”*

As shown on Figure 12 above, policies of the Secondary Plan permit maximum building heights up to 40 storeys abutting Highway 417, up to 30 storeys in the central portion, and as determined by an approximate 45 degree angular plane where abutting the low-rise community to the south of the Subject Site. Building heights will be determined through separate applications for Site Plan and Rezoning on a site-specific basis.

Further discussion of applicable Secondary Plan policies are provided in Section 3.4 of this Planning Rationale.

1.4.3 City of Ottawa Zoning By-law (2008-250)

Figure 13: Zoning for the Subject Site

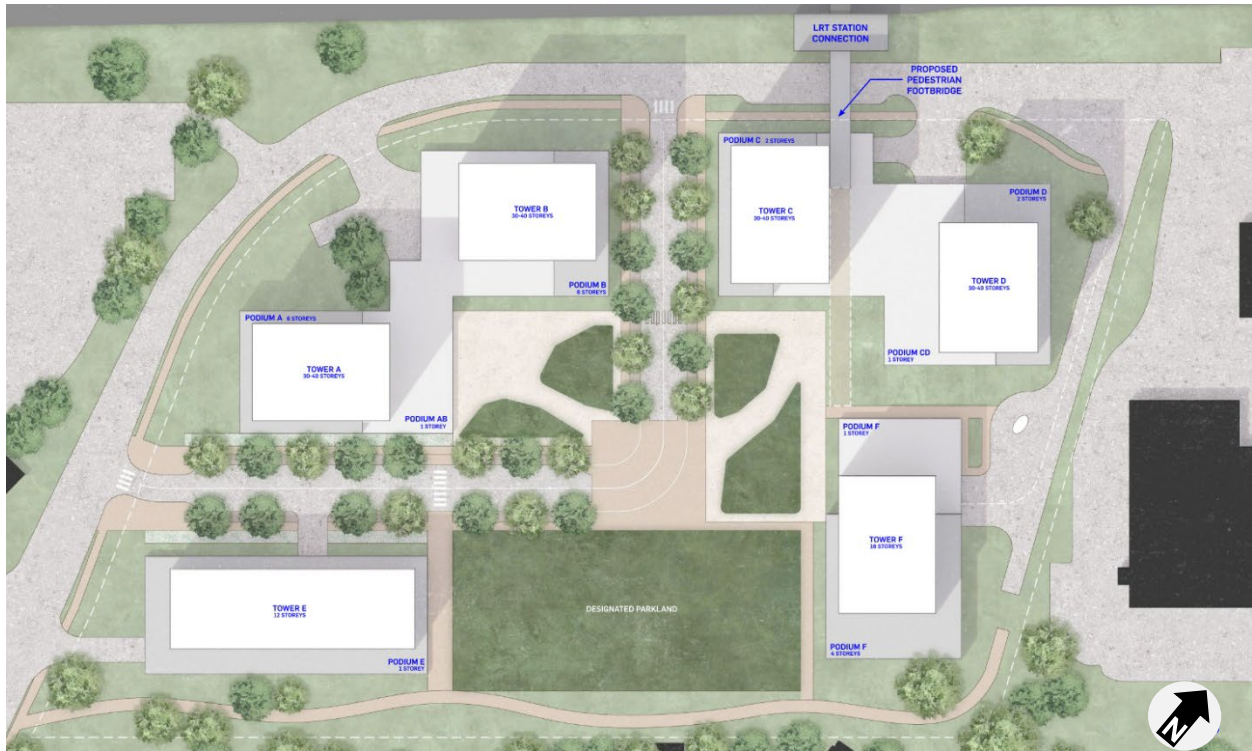


The Subject Site is zoned Mixed Use Centre, Subzone 20, Schedule 508 (MC20 S508) in the City of Ottawa Zoning By-law 2008-250 (see Figure 13).

2.0 PROPOSED DEVELOPMENT

2.1 Description of Conceptual Development Proposal

Figure 14: Conceptual Development Proposal



The plan of subdivision will create six blocks to facilitate a future mixed-use development on the property municipally known as 1101 Baxter Road (the “Subject Site”). The residential component is anticipated to consist of approximately 1,400 dwelling units, complemented by limited retail and commercial spaces. A centrally located park block would be accessible from Baxter Road via a new private street (“Street One”) (see Figure 14).

Figure 15: Precedent Images – Proposed Central Square



The proposed subdivision is designed to feature mid- to high-rise mixed-use buildings framing a pedestrian-oriented urban plaza along Street One (“Central Square”), which will function as a shared street to prioritize walkability and foster community interaction rather than orienting development toward Highway 417.

Pending detailed design as part of a complete Site Plan application, please refer to precedent images for the proposed Central Square in Figure 15 above.

Figure 16: Precedent Images – Proposed Park Block



A centrally located park block will serve as a focal point for the community, with connections from Baxter Road and Queensview Station via Street One and a planned multi-use pathway along the southern lot line.

Pending detailed design as part of a complete Site Plan application, please refer to precedent images for the proposed Park Block in Figure 16 above.

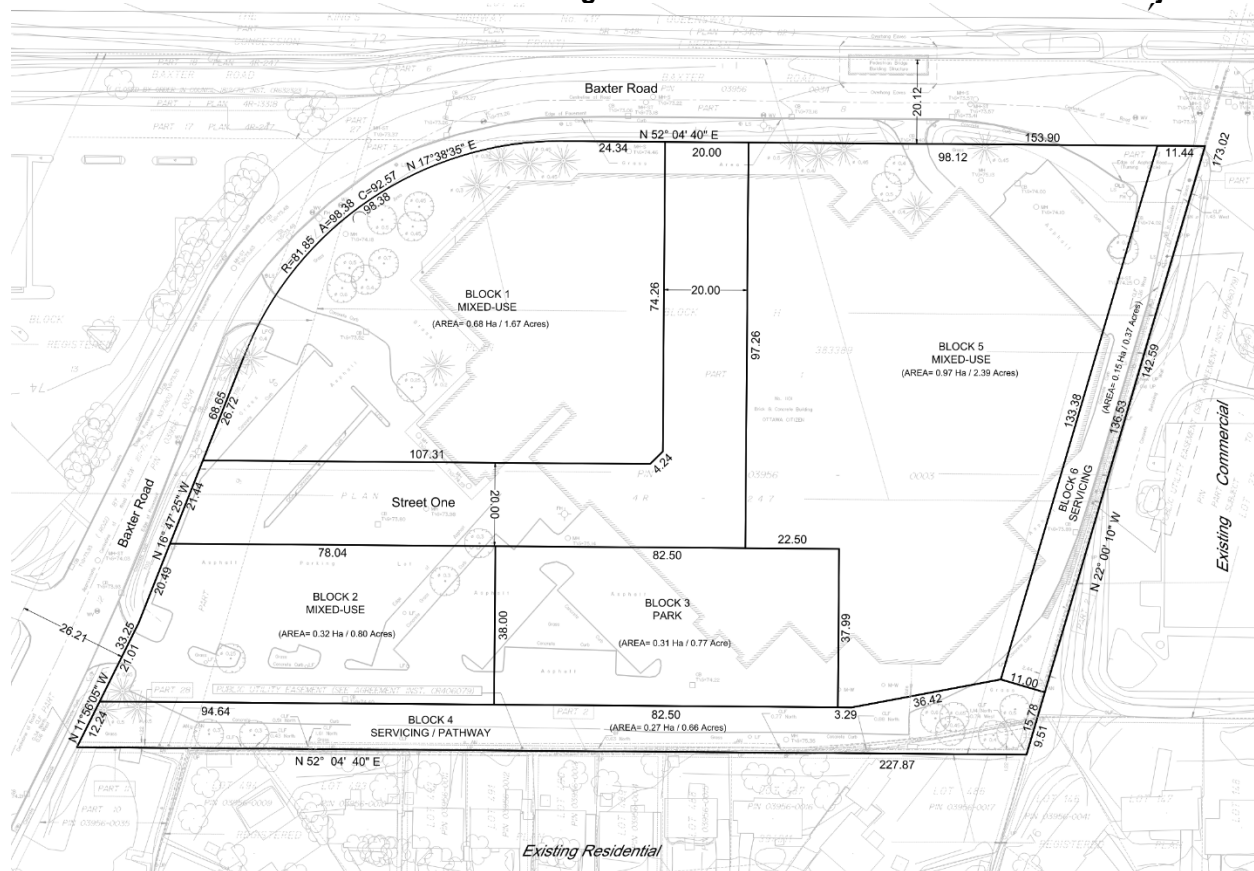
Street One will provide vehicular access into the centre of the subdivision and off-street parking areas for the proposed mixed-use towers located above. Along the south and east lot lines will be a servicing corridor providing water, storm and sanitary connections looped from Baxter Road through the Subject Site.

Table 1 provides details of the proposed lots, blocks, and streets that make up the Draft Plan of Subdivision for the Subject Site.

**Table 1: Breakdown of Proposed Subdivision**

Block/Lot Number	Proposed Land Use	Area
Block 1	Mixed-Use	0.68 ha
Block 2	Mixed-Use	0.32 ha
Block 3	Park [to be dedicated to the City]	0.31 ha
Block 4	Servicing/Pathway [to be dedicated to the City]	0.27 ha
Block 5	Mixed-Use	0.97 ha
Block 6	Servicing / Access [Private] (subject to easement to the City for Servicing)	0.15 ha

**Figure 17: Draft Plan of Subdivision for the Subject Site**



As shown on Figure 17, a 0.31 hectare park (Block 3) will be located to the north of the Servicing/Pathway to be dedicated to the City (Block 4). The park block will have frontage on Street One. As shown on Schedule C of the Secondary Plan, the proposed location of the park abutting Baxter Road is fragmented across three separate properties and divided by the roadway. To provide a more functional and cohesive public space, the park is proposed in an interior location at the centre of the subdivision. This central placement will create a larger, rectangular public park space with direct access from Street One, connections to the pedestrian pathway along the southern lot line, and connection to the planned urban plaza. The park will serve as a

buffer to the low-rise residential area to the south while integrating with Citizen Plaza and planned mixed-use buildings. This approach allows the City to establish a larger, centrally located park earlier in the redevelopment process.

Section 51.1(3.24(a)) of the Planning Act limits the conveyance percentage to a maximum of 10% of the gross land area where the Subject Site is less than 5 hectares in area. Based on a maximum parkland dedication rate of 10% of the gross land area, approximately 0.31 hectares of parkland will be required for the proposed plan of subdivision.

Separate applications for site plan and rezoning will be required at later date to facilitate development of each proposed mixed-use block. The proposed plan of subdivision meets the parkland dedication requirements.

## **2.2 Previous Consultations, Applications and Approvals**

A formal pre-consultation meeting was held with the City of Ottawa on February 20, 2025. Applications for Plan of Subdivision and Rezoning were discussed at the pre-consultation meeting. The intent of this application is for plan of subdivision to establish blocks for phased development on the Subject Site. A Rezoning application to address height and separate Site Plan application will be required at later date to facilitate development of each newly created block.

## **3.0 PLANNING POLICY JUSTIFICATION**

### **3.1 Planning Act**

The Planning Act is provincial legislation that regulates land use planning in Ontario. Section 51 of the Planning Act regulates Plan of Subdivision approvals. Section 51(24) sets out the criteria that must be considered when reviewing a draft plan of subdivision.

Section 51(24) states:

*“51(24) In considering a draft plan of subdivision, regard shall be had, among other matters, to the health, safety, convenience, accessibility for persons with disabilities and welfare of the present and future inhabitants of the municipality and to,”*

*(a) the effect of development of the proposed subdivision on matters of provincial interest as referred to in section 2;*

The proposed Plan of Subdivision will have regard for the following matters of provincial interest:

- the supply, efficient use and conservation of energy and water
- the adequate provision and efficient use of communication, transportation, sewage and water services and waste management system
- the orderly development of safe and healthy communities
- the adequate provision and distribution of education, health, social, cultural and recreational facilities
- the adequate provision of a full range of housing, including affordable housing
- the appropriate location of growth and development

*(b) whether the proposed subdivision is premature or in the public interest;*

The Subject Site is located within the City of Ottawa's urban boundary. The proposed subdivision is located within an area with a number of residential, commercial and retail uses that are compatible with and supportive of future mixed-use development. Based on Section 3.1 of the Pinecrest and Queensview Stations Secondary Plan, servicing capacity for new development is limited in the surrounding area and services will be provided on a "first-come, first served basis". The subdivision process will be used to confirm availability and secure adequate services to support the proposed development. The proposed subdivision is not premature and is in the public interest.

*(c) whether the plan conforms to the official plan and adjacent plans of subdivision, if any;*

The Subject Site is designated Hub in the Inner Urban Transect of the City of Ottawa Official Plan (2022). The Subject Site is designated Hub, located within the Iris-Baxter Sector of the Pinecrest and Queensview Stations Secondary Plan. The proposed subdivision will support residential growth within the urban area by establishing blocks to accommodate approximately 1,400 dwelling units within the urban area. The proposed development will provide a mix of mid-rise and high-rise built forms that can incorporate some limited commercial uses at grade along Baxter Road, at each intersection with Street One and near the entrance to the pedestrian bridge from Queensview Station. The proposed subdivision conforms to policies of the Official Plan and policies of the Pinecrest and Queensview Stations Secondary Plan. A Rezoning application to address building height and separate Site Plan applications are required at some later date to facilitate development of each newly created block.

*(d) the suitability of the land for the purposes for which it is to be subdivided;*

The proposed subdivision will create blocks that will each be suitable to support mixed use, mid to high-rise development. The conceptual development proposal will conform with the provisions of the Zoning By-law for the MC20 zone. The proposed open space block is appropriately located at the centre of the Subject Site with access off Baxter Road and Street One. A Rezoning application to address building height and separate Site Plan applications are required at some later date to facilitate development of each newly created block.

*(d.1) if any affordable housing units are being proposed, the suitability of the proposed units for affordable housing;*

Not applicable.

*(e) the number, width, location and proposed grades and elevations of highways, and the adequacy of them, and the highways linking the highways in the proposed subdivision with the established highway system in the vicinity and the adequacy of them;*

The proposed transportation design for the subdivision includes a private 20 metre ROW for Street One (local) accessed off Baxter Road (local). As described in the TIA study, the design of Street One meets Policy 7.2.18 of the Pinecrest and Queensview Stations Secondary Plan, which identifies that redevelopment shall include a new public or private street that improves site connectivity, direct access to Queensview Station, prioritized active transportation modes, and connections to the broader street network. The proposed curbless design and wide sidewalks for

Street One establish a “liveable” street with standard vehicle and pedestrian traffic functions on a day to day basis that can be closed to traffic for special events. The proposed roadway width for Street One is consistent with the City’s direction on roadway widths.

Sidewalks are proposed on at least one side of Baxter Drive abutting the Subject Site and both sides of Street One. A pathway is proposed along the south lot line abutting rear yards of low-rise homes along Dempsey Avenue. Additional details on the proposed transportation design and potential impacts can be found in the Transportation Impact Assessment included with this submission.

The proposed subdivision has regard for the number, width, location, proposed grades, and adequacy of roads and highways.

*(f) the dimensions and shapes of the proposed lots;*

The proposed subdivision will include three Blocks for future mid to high-rise, mixed use buildings, one Block for a public park and two Blocks for servicing and pathways off Baxter Road based on the proposed development concept. The proposed Blocks are an appropriate size to support the future development, open space and servicing areas and are generally arranged in a regular shape following the Baxter Road frontage. The dimensions and shapes of the proposed Blocks are appropriate.

*(g) the restrictions or proposed restrictions, if any, on the land proposed to be subdivided or the buildings and structures proposed to be erected on it and the restrictions, if any, on adjoining land;*

The proposed subdivision considers potential restrictions on the Subject Site, including:

- phased development (demolition of the existing Citizen building);
- location of transitway infrastructure;
- coordination of active transportation infrastructure along Baxter Road;
- location of existing and planned hydro transmission lines;
- location of existing or proposed easements;
- location and adequate capacity of existing and planned servicing connections; and,
- future extension of Baxter Road.

*(h) conservation of natural resources and flood control;*

The Subject Site is not located in any floodplains or areas of natural interest. No direct or indirect impacts on natural resource areas are expected as a result of proposed development on the Subject Site.

*(i) the adequacy of utilities and municipal services;*

The Servicing and Stormwater Management Report prepared by Novatech, dated December 22, 2025, confirms that there is adequate servicing to support the proposed subdivision.

*(j) the adequacy of school sites;*

The Subject Site is located near Joan of Arc Academy, Our Lady of Victory Elementary School, Bishop Hamilton Montessori School, and Pinecrest Public School. The proposed subdivision will not include a school. The proposed subdivision is not expected to have any impacts on existing or planned school sites. Circulation to school boards for comment and feedback will be part of the standard application process.

*(k) the area of land, if any, within the proposed subdivision that, exclusive of highways, is to be conveyed or dedicated for public purposes;*

The plan of subdivision includes a 0.31 hectare park that is proposed to be conveyed to the City. This park will have frontage on Street One and along the south lot line where a pathway is proposed. The proposed park is an adequate size to accommodate recreational amenities. Servicing blocks will also be conveyed to the City.

*(l) the extent to which the plan's design optimizes the available supply, means of supplying, efficient use and conservation of energy; and*

The proposed subdivision will more efficiently use the Subject Site by facilitating a mixed-use development on underutilized land within the City's urban boundary, located within 300 metres radius of Queensview Station on the City's rapid transit network. The proposed subdivision will connect to existing municipal services in the surrounding area.

*(m) the interrelationship between the design of the proposed plan of subdivision and site plan control matters relating to any development on the land, if the land is also located within a site plan control area designated under subsection 41 (2) of this Act or subsection 114 (2) of the City of Toronto Act, 2006. 1994, c. 23, s. 30; 2001, c. 32, s. 31 (2); 2006, c. 23, s. 22 (3, 4); 2016, c. 25, Sched. 4, s. 8 (2).*

The intent of this Draft Plan application is to subdivide the land. A Rezoning application to address building height and separate Site Plan applications are required at some later date to facilitate development of each newly created block; however, a conceptual development proposal has been advanced to address the relationship between the size and configuration of the blocks to facilitate the future Site Plan applications and building configuration and placement.

### **3.2 Provincial Planning Statement, 2024**

The Provincial Planning Statement, 2024 (PPS) provides policy direction on matters of provincial interest and sets the foundation for regulating the development and use of all land. The PPS was issued under the authority of Section 3 of the Planning Act and came into effect on October 20, 2024. All decisions affecting planning matters must be consistent with the policies of the PPS.

Section 2.1 of the PPS provides policy direction for Planning for People and Homes.

Section 2.1, Policy 4 of the PPS states:

*"To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:*

- a) maintain at all times the ability to accommodate residential growth for a minimum of 15 years through lands which are designated and available for residential development; and*
- b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned, including units in draft approved or registered plans."*

The proposed subdivision will create lots to facilitate future development of a range of mid to high-rise, mixed use buildings on the Subject Site. Future development will provide an appropriate range and mix of housing types to establish higher density in the neighbourhood and ground level commercial spaces will provide a variety of land uses.

Section 2.1, Policy 6 of the PPS states (*emphasis added*):

*“Planning authorities should support the achievement of complete communities by:*

- a) accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses (including schools and associated child care facilities, long-term care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs;*
- b) improving accessibility for people of all ages and abilities by addressing land use barriers which restrict their full participation in society; and*
- c) improving social equity and overall quality of life for people of all ages, abilities, and incomes, including equity-deserving groups.”*

The proposed subdivision will include three Blocks for future mid to high-rise, mixed use buildings, two Blocks for servicing and pathways off Baxter Road and one Block for a 0.31 hectare public park for use by residents of the neighbourhood. These uses will support the achievement of complete communities within urban areas of the City by providing a range of residential, commercial uses and recreational amenities within walking distance of home.

Section 2.2 of the PPS provides policy direction for Housing.

Section 2.2, Policy 1of the PPS states (*emphasis added*):

*“Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by:*

- a) establishing and implementing minimum targets for the provision of housing that is affordable to low and moderate income households, and coordinating land use planning and planning for housing with Service Managers to address the full range of housing options including affordable housing needs;*
- b) permitting and facilitating:*
  - 1. all housing options required to meet the social, health, economic and wellbeing requirements of current and future residents, including additional needs housing and needs arising from demographic changes and employment opportunities; and*
  - 2. all types of residential intensification, including the development and redevelopment of underutilized commercial and institutional sites (e.g., shopping malls and plazas) for residential use, development and introduction of new housing options within previously developed areas, and redevelopment, which results in a net increase in residential units in accordance with policy 2.3.1.3;*
- c) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation; and*

- d) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations.

The proposed subdivision will provide for a mix of housing types and uses, which will support residential growth in the City and diversify housing choices in the area that cater to people of all ages and life stages. The proposed development is located in an established area of the City which minimizes servicing costs. The proposal will not create environmental or public health and safety concerns. The Subject Site is transit-supportive being located within 300 metres radius of Queensview Station, accessed via the pedestrian bridge across Highway 417.

Section 2.3 of the PPS provides policy direction for Settlement Areas.

Section 2.3.1, Policy 1 of the PPS states (emphasis added):

“Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.”

The Subject Site is located within a settlement area and the Pinecrest-Queensview Major Transit Station Area (PMTSA). The Subject Site is designated as Hub in the Official Plan. Policies of the Official Plan establish a targeted residential density of 250 dwellings per net hectare for the Pinecrest-Queensview Hub. Based on the conceptual development proposal, approximately 1,400 dwelling units are proposed on the Subject Site, which has a net buildable area of approximately 1.97 ha (19,700 square metres). The density of the conceptual development proposal is approximately 711 dwelling units per net hectare, which exceeds the residential density targets for the Pinecrest-Queensview Hub in the Official Plan. The proposed development will contribute to growth and development within the settlement area boundary.

Section 2.3.1, Policy 2, of the PPS states (emphasis added):

“Land use patterns within settlement areas should be based on densities and a mix of land uses which:

- a) efficiently use land and resources;
- b) optimize existing and planned infrastructure and public service facilities;
- c) support active transportation
- d) are transit-supportive, as appropriate; and
- e) are freight-supportive”

The proposed development will optimize underutilized land by introducing a mix of uses within a 5-minute walk of Queensview Station on the City’s rapid transit network. The proposed development will have adequate servicing, as indicated in the Servicing and Stormwater Management Report prepared by Novatech, dated December 22, 2025.

Section 2.4 of the PPS provides policy direction for Strategic Growth Areas.

Section 2.4.2, Policy 2, of the PPS states (emphasis added):

“Within major transit station areas on higher order transit corridors, planning authorities shall plan for a minimum density target of:

- a) 200 residents and jobs combined per hectare for those that are served by subways;

- b) 160 residents and jobs combined per hectare for those that are served by light rail or bus rapid transit; or*
- c) 150 residents and jobs combined per hectare for those that are served by commuter or regional rail.”*

The proposed development is located within the Pinecrest-Queensview PMTSA on the City's rapid transit network. Policies of the Official Plan establish a targeted residential density of 250 dwellings per net hectare for the Pinecrest-Queensview Hub. Based on the conceptual development plan a density of approximately 711 dwelling units per net hectare is proposed, which exceeds the residential density targets for the Pinecrest-Queensview Hub in the Official Plan and is consistent with growth targets of the PPS.

Section 2.9 of the PPS provides policy direction on Energy Conservation, Air Quality, and Climate Change.

Section 2.9, Policy 1 of the PPS states (*emphasis added*):

*“Planning authorities shall plan to reduce greenhouse gas emissions and prepare for the impacts of a changing climate through approaches that:*

- a) support the achievement of compact, transit-supportive, and complete communities;*
- b) incorporate climate change considerations in planning for and the development of infrastructure, including stormwater management systems, and public service facilities;*
- c) support energy conservation and efficiency;*
- d) promote green infrastructure, low impact development, and active transportation, protect the environment and improve air quality; and*
- e) take into consideration any additional approaches that help reduce greenhouse gas emissions and build community resilience to the impacts of a changing climate.”*

As indicated on Schedule B2 of the City of Ottawa Official Plan, the Subject Site is located within 300 metres radius of Queensview Station connected via a pedestrian bridge located to the north across Highway 417 (see Figure 8). The proposed development concept will provide a compact transit-supportive built form within proximity of rapid transit service.

Section 3.1 of the PPS provides policy direction for Infrastructure and Public Service Facilities.

Section 3.1, Policy 1 of the PPS states:

*“Before consideration is given to developing new infrastructure and public service facilities:*

- a) the use of existing infrastructure and public service facilities should be optimized; and*
- b) opportunities for adaptive re-use should be considered, wherever feasible.”*

The Servicing and Stormwater Management Report prepared by Novatech and included in this submission details how the proposed development will connect and utilize existing municipal sewage, water, and stormwater services. Further details can be found in the report.

The use of existing public service facilities such as schools, recreation centres, and libraries in the area will be optimized through proposed intensification of the community.

Section 3.2 of the PPS provides policy direction for Transportation Systems.

Section 3.2, Policy 1 of the PPS states:

*“Transportation systems should be provided which are safe, energy efficient, facilitate the movement of people and goods, are appropriate to address projected needs, and support the use of zero- and low- emission vehicles.”*

Section 3.2, Policy 2 of the PPS states:

*“Efficient use should be made of existing and planned infrastructure, including through the use of transportation demand management strategies, where feasible.”*

Section 3.2, Policy 3 of the PPS states:

*“As part of a multimodal transportation system, connectivity within and among transportation systems and modes should be planned for, maintained and, where possible, improved, including connections which cross jurisdictional boundaries.”*

The Subject Site is located within a 300 metre radius of Queensview Station, accessed via a pedestrian bridge across Highway 417. The proposed transportation design for the subdivision includes a private 20 metre ROW for Street One (local) accessed off Baxter Road (local). As described in the TIA study, the design of Street One meets Policy 7.2.18 of the Pinecrest and Queensview Stations Secondary Plan, which identifies that redevelopment shall include a new public or private street that improves site connectivity, direct access to Queensview Station, prioritized active transportation modes, and connections to the broader street network. The proposed curbless design and wide sidewalks for Street One establish a “liveable” street with standard vehicle and pedestrian traffic functions on a day to day basis that can be closed to traffic for special events.

Sidewalks are proposed on at least one side of Baxter Drive abutting the Subject Site and both sides of Street One. A pathway is proposed along the south lot line abutting rear yards of low-rise residential properties along Dempsey Avenue. The proposed road network, pedestrian sidewalks, pathways and proximity to Queensview Station will support safe, multi-modal transportation options for residents and visitors.

Section 3.6 of the PPS provides policy direction for Sewage, Water, and Stormwater.

Section 3.6, Policy 2 of the PPS states:

*“Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. For clarity, municipal sewage services and municipal water services include both centralized servicing systems and decentralized servicing systems.”*

Municipal services are proposed to connect the subdivision with water, stormwater and sanitary services. The Servicing and Stormwater Management Report prepared by Novatech and included in this submission confirms adequate capacity is available. The proposed subdivision is consistent with policies of the PPS for provision of site servicing. More details on the servicing strategy can be found in the Servicing and Stormwater Management Report prepared by Novatech.

Section 3.6, Policy 8 of the PPS states:

*“Planning for stormwater management shall:*

- a) be integrated with planning for sewage and water services and ensure that systems are optimized, retrofitted as appropriate, feasible and financially viable over their full life cycle;*
- b) minimize, or, where possible, prevent or reduce increases in stormwater volumes and contaminant loads;*
- c) minimize erosion and changes in water balance including through the use of green infrastructure;*
- d) mitigate risks to human health, safety, property and the environment;*
- e) maximize the extent and function of vegetative and pervious surfaces;*
- f) promote best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development; and*
- g) align with any comprehensive municipal plans for stormwater management that consider cumulative impacts of stormwater from development on a watershed scale.”*

The Servicing and Stormwater Management Report prepared by Novatech, dated December 23, 2025, details the stormwater management strategy for the proposed subdivision.

Section 3.9 of the PPS provides policy direction for Public Spaces, Recreation, Parks, Trails and Open Spaces.

Section 3.9, Policy 1 of the PPS states:

*“Healthy, active, and inclusive communities should be promoted by:*

- a) planning public streets, spaces and facilities to be safe, meet the needs of persons of all ages and abilities, including pedestrians, foster social interaction and facilitate active transportation and community connectivity;*
- b) planning and providing for the needs of persons of all ages and abilities in the distribution of a full range of publicly-accessible built and natural settings for recreation, including facilities, parklands, public spaces, open space areas, trails and linkages, and, where practical, water-based resources;*
- c) providing opportunities for public access to shorelines; and*
- d) recognizing provincial parks, conservation reserves, and other protected areas, and minimizing negative impacts on these areas.”*

A 0.31 hectare park is proposed at the centre of the subdivision. The park will have frontage on Street One and will be located with access to the pathway along the south lot line abutting the park Block. This will provide pedestrian access to the proposed park, as well as connections to Baxter Road.

**The Plan of Subdivision application is consistent with the policies of the Provincial Planning Statement (2024).**

### 3.3 City of Ottawa Official Plan

The Subject Site is designated Hub in the Inner Urban Transect of the City of Ottawa Official Plan (2022). The Subject Site is designated Hub, located within the Iris-Baxter Sector of the Pinecrest and Queensview Stations Secondary Plan. Policies of the Secondary Plan take precedence over policies of the Official Plan.

#### 3.3.1 Strategic Directions & Cross Cutting Issues

Section 2 of the Official Plan (2022) provides Strategic Directions for the City. This includes five Big Policy Moves and six Cross-Cutting Issues.

The five Big Policy Moves provide broad policy directions and are the foundation of the Official Plan. The Big Policy Moves are:

1. *Achieve, by the end of the planning period, more growth but intensification than by greenfield development.*
2. *By 2046, the majority of trips in the city will be made by sustainable transportation.*
3. *Improve our sophistication in urban and community design and put this knowledge to the service of good urbanism at all scales, from the largest to the very small.*
4. *Embed environmental, climate and health resiliency and energy into the framework of our planning policies.*
5. *Embed economic development into the framework of our planning policies.*

These Big Policy Moves inform the six themes, or Cross-Cutting Issues, that are embedded throughout the policies and sections of the Official Plan. The Cross-Cutting Issues are:

- Intensification and Diversifying Housing Options
- Economic Development
- Energy and Climate Change
- Healthy and Inclusive Communities
- Gender and Racial Equity
- Culture

The proposed Plan of Subdivision application addresses the objectives of the following Cross-Cutting Issues:

##### 3.3.1.1 Intensification and Diversifying Housing Options

The Plan of Subdivision application will create blocks to facilitate the future development of approximately 1,400 dwelling units on the Subject Site based on the proposed development concept. Separate application processes for site plan and rezoning will be used to establish a diverse range of housing options in the neighbourhood. This will contribute to intensification within the urban area and will provide for a variety of housing options in the neighbourhood. The proposed development will improve the condition of underutilized land near rapid transit and contribute to meeting the goals of the City's Growth Management Framework. The proposed development concept will contribute to the 47% of household growth that is allocated to the built-up or developed portion of the urban area, as per Policy 3.1(4)(a)(i). The proposed development concept will support the City in meeting its target of 51% of dwelling growth occurring through intensification, as per Policy 3.2(1).

### *3.3.1.2 Economic Development*

The proposed Plan of Subdivision application will provide approximately 1,400 dwelling units on the Subject Site, which supports the City's Growth Management Framework. The proposed development will contribute to the 93% of new housing that is allocated to the urban area, as per Policy 3.1(4)(a). A mix of commercial and residential uses is proposed within the future development blocks. The increased density on the Subject Site will support local businesses, transit and active transportation use near Queensview Station and in the surrounding community.

### *3.3.1.3 Energy and Climate Change*

The proposed development efficiently use the Subject Site and will contribute to the creation of a compact and complete community. Providing increased density on the Subject Site will support transit services with Queensview Station within a 5-minute walking distance. The proximity to existing and planned commercial, retail, employment and recreational opportunities in the surrounding area will decrease reliance on private motor vehicles to complete daily tasks. Sidewalks and a pedestrian pathway are proposed for the subdivision to encourage active transportation within the neighbourhood and connecting to surrounding areas.

### *3.3.1.4 Healthy and Inclusive Communities*

The proposed subdivision will facilitate future development opportunities for a variety of more compact, higher density, mixed residential and commercial uses in proximity to frequent transit service at Queensview Station. The proposed development will include sidewalks, and a pathway to encourage residents to walk and cycle. This will support the development of healthy, walkable 15-minute neighbourhoods and will add to the mix of housing options available to all residents of the future subdivision.

### *3.3.1.5 Gender and Racial Equality*

As part of future applications for site plan and rezoning the proposed development will provide access to ground floor commercial uses within walking distance, which will allow for easy access to daily and weekly needs for all residents. The proposed pathway will connect to Baxter Road and the Park block fronting along Street One. Pedestrian prioritization through the subdivision is ideal for families and will allow for easy access to recreational spaces for all residents, regardless of their gender, race, age, or abilities.

### *3.3.1.6 Culture*

The plan of subdivision and conceptual development proposal will contribute to a well-designed public realm. The subdivision is planned around a curbless, raised intersection at the bend in Street One that functions as a pedestrian plaza at the Centre of the community. Surrounding mixed-use buildings will activate the public realm with limited retail, services, and opportunities for art and cultural programming. The centrally located park block will serve as a gathering space for residents, linked to nearby transit and active transportation routes. The proposed subdivision design prioritizes placemaking to strengthen the sense of community within the immediate area rather than focusing development outward along Highway 417.

### 3.3.2 Growth Management Framework

Section 3 of the Official Plan (2022) provides a Growth Management Framework for the City of Ottawa. Section 3 states:

*“Most growth will occur within the urban area of the City, with a majority of residential growth to be within the built-up area through intensification, increasing over time during the planning horizon.”*

Section 3 also states:

*“Within the Greenbelt, where most of the housing growth in the built-up area is expected to occur, new housing development will be both in the form of larger dwelling units and apartments.”*

The Subject Site is located within the urban area of the City. The proposed development concept will accommodate residential growth within the built-up area by providing approximately 1,400 new residential dwelling units along with at grade retail and commercial uses. The proposed development will help accommodate the City’s expected growth through infill rather than greenfield development.

Section 3.1, Policy 3 of the Official Plan states:

*“The urban area and villages shall be the focus of growth and development.”*

Section 3.1, Policy 4 of the Official Plan states:

*“The City will allocate household growth targets as follows:*

- a) 93 per cent within the urban area where:
  - i) 47 per cent is within the urban area that is built-up or developed as of July 1, 2018; and*
  - ii) 46 per cent is within the greenfield portion of the urban area;**
- b) 7 per cent within the rural area where:
  - i) 5 per cent is within the villages; and*
  - ii) 2 per cent is outside of villages.”**

The proposed Plan of Subdivision will facilitate the development of approximately 1,400 dwelling units within the urban area. This will contribute to meeting the City’s target of 93% of household growth being within the urban area and 47% of household growth being the built-up or developed portion of the urban area.

Section 3.2 encourages intensification within the built-up area and provides policy direction for future infill growth.

Section 3.2, Policy 1 of the Official Plan states (*emphasis added*):

*“The target amount of dwelling growth in the urban area that is to occur through intensification is 51 per cent and represents the proportion of new residential dwelling units, excluding institutional and collective units such as senior’s and student residences, based upon building permit issuance within the built-up portion of the urban area. This overall target is anticipated to be achieved through a*

*gradual increase in intensification throughout the urban area that was developed or built-up as of June 30, 2018 as follows:*

- a) 2018 to 2021: 40 per cent;
- b) 2022 to 2026: 45 per cent;
- c) 2027 to 2031: 50 per cent;
- d) 2032 to 2036: 54 per cent;
- e) 2037 to 2041: 57 per cent; and
- f) 2042 to 2046: 60 per cent.”

The proposed development is within the urban area and is considered intensification. The proposed development will contribute to the City’s target of 51% of dwelling growth occurring through intensification.

Section 3.2, Policy 4 of the Official Plan states: *“Intensification is permitted in all designations where development is permitted taking into account whether the site has municipal water and sewer services. This Plan supports intensification and the approval of applications for intensification shall be in conformity with transect and overlay policies as applicable. When reviewing planning applications for intensification, the City shall ensure that surface water and groundwater resources are protected, particularly where the groundwater resource is used for drinking water.”*

The Subject Site is located within the built-up area and has access to municipal water and sewer service. Adequate capacity will be confirmed as part of this subdivision application. The proposed development concept will conform with the appropriate transect and land use designation policies regarding intensification on the Subject Site.

Policy 10 of Section 3.2 of the Official Plan sets out residential density targets that apply to Hubs and states: *“The residential density and proportion of large household dwelling targets as shown on Schedules B1 through B8 are established in Table 3a for Hubs and Mainstreet Corridors and Table 3b for Neighbourhoods and Minor Corridors.”*

The Subject Property is designated as Hub in the Official Plan. Policies of the Official Plan establish a targeted residential density of 250 dwellings per net hectare for the Pinecrest-Queensview Hub. Based on the conceptual development plan, approximately 1,400 dwelling units are proposed on the Subject Site, which has a net buildable area of approximately 1.97 ha (19,700 square metres). The density of the conceptual development is approximately 711 dwelling units per net hectare, which exceeds the residential density targets for the Pinecrest-Queensview Hub in the Official Plan.

### **3.3.3 City-wide Policies**

Section 4 of the Official Plan (2022) sets out city-wide policies to be considered where all new development is proposed.

#### **3.3.3.1 - Mobility**

Section 4.1 of the Official Plan provides policy direction for transportation and mobility.

Section 4.1.1, Policy 1 of the Official Plan states:

*“In the Urban area and Villages, people who walk, cycle and use transit shall, by default, be given priority for safety and movement. The resolution of overlapping*

*priorities in the Urban area and Villages, as well as the establishment of priorities in the Rural area, will be informed by Multi-Modal Level of Service targets outlined in the Transportation Master Plan (TMP) and Multi-Modal Level of Service Guidelines.”*

The proposed subdivision will include sidewalks on at least one side of Baxter Road and both sides of Street One. A pathway will be provided on the south side of the Subject Site. Proposed sidewalks and bicycle infrastructure will support the safe movement of pedestrians and cyclists.

Section 4.1.1, Policy 5 of the Official Plan states:

*“New subdivision development shall connect to existing pedestrian, cycling, transit and street networks and provide for the potential future extension of these networks up to abutting property boundaries, including those lands beyond an existing Urban boundary or Village boundary.”*

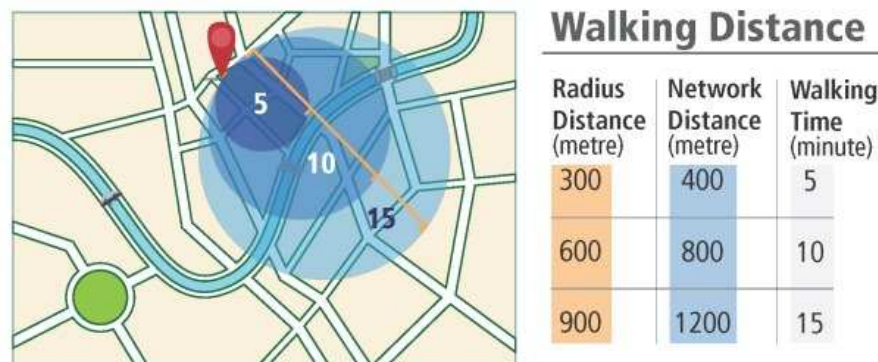
The proposed development will consider a direct connection to Queensview Station from the Subject Site via the pedestrian bridge over Highway 417. The proposed development will also contribute to the existing and future street and pedestrian network by creating a pathway along the south lot line and Street One providing a connection between the Subject Site and Baxter Road.

Section 4.1.2, Policy 1 of the Official Plan sets out the framework for establishing 15-minute neighbourhoods across the City and states that:

*“1) In general, this Plan equates a walking time of:*

- a) 5 minutes to be equivalent to a radius of 300 metres, or 400 metres on the pedestrian network;*
- b) 10 minutes to be equivalent to a radius of 600 metres, or 800 metres on the pedestrian network; and*
- c) 15 minutes to be equivalent to a radius of 900 metres or 1,200 metres on the pedestrian network.”*

**Figure 18 – Official Plan Figure 11**



The Subject Property is located fully within 300 metres radius and within 400 metres network distance of Queensview Station. Queensview Station is planned as a future LRT route with at grade crossings providing connections to local transit routes and stations along the O-Train network. The proximity of the Subject Site to Queensview Station represents a walking distance of 5 minutes or less for residents to access rapid transit from their home (see Figure 18).

Section 4.1.2, Policy 6 of the Official Plan states:

*“New developments will provide direct connections to the existing or planned network of public sidewalks, pathways and cycling facilities.”*

The proposed development will provide a pathway along the south lot line, connecting to the existing pathway network on Baxter Road. A dedicated pathway block will extend from Baxter Road to Street One, enhancing pedestrian connectivity. Proposed sidewalks are planned on both sides of Street One, linking to the current pedestrian infrastructure along Baxter Road. Sidewalks will also be provided on at least one side of every local street, ensuring direct access to the broader sidewalk network and promoting active transportation throughout the neighbourhood.

### 3.3.3.2 - Housing

Section 4.2 of the Official Plan provides policy direction related to housing.

Section 4.2.1, Policy 1 of the Official Plan states (*emphasis added*):

*“A diverse range of flexible and context-sensitive housing options in all areas of the city shall be provided through the Zoning By-law, by:*

- a) Primarily regulating the density, built form, height, massing and design of residential development, rather than regulating through restrictions on building typology;*
- b) Promoting diversity in unit sizes, densities and tenure options within neighbourhoods including diversity in bedroom count availability;*
- c) Permitting a range of housing options across all neighbourhoods to provide the widest possible range of price, occupancy arrangements and tenure;*
- d) Establishing development standards for residential uses, appropriately balancing the value to the public interest of new policies or development application requirements against the impacts to housing affordability; and*
- e) The City shall maintain, at all times, land with servicing capacity sufficient to provide at least a three year supply of residential units available through lands suitably zoned to facilitate intensification and land in draft approved and registered plans.”*

Based on the conceptual development plan, the proposed Plan of Subdivision application will establish blocks to facilitate future development of approximately 1,400 dwelling units in a range of mid to high-rise, mixed use buildings. A Rezoning application to address building height and separate Site Plan applications are required at some later date to facilitate development of each newly created block. The proposed development of subdivided blocks will contribute to housing diversity in the neighbourhood and will provide for a range of housing options for future residents. The proposed development will support the City’s anticipated growth within the Urban Area.

### 3.3.3.3 Section 4.4 Parks and Recreation Facilities

Section 4.4 of the Official Plan provides policy direction for parks and recreation facilities.

Section 4.4.1, Policy 2 of the Official Plan states:

*“All development, regardless of use, shall meet all of the following criteria to the satisfaction of the City:*

- a) Consider land acquisition for parks as directed by the Parkland Dedication By-law to meet community needs for both residential and non-residential development, with an emphasis on active recreation amenities and potential cultural development with new parks acquired to address gaps or community needs; and*
- b) Prioritize land for parks on-site over cash-in-lieu of parkland. Cash-in-lieu of parkland shall only be accepted when land or location is not suitable. The land to be conveyed shall, wherever feasible:
  - i) Be a minimum of 400 square metres or as described in the upcoming Land First Policy and updated Park Development Manual as directed by the Parks and Recreation Facilities Master Plan;*
  - ii) Be free of encumbrances above and below ground when land for parks is obtained by parkland dedication; or in the case of land purchases for the creation of new parks in established areas, unless the encumbrances have been approved by the City where reasonable;*
  - iii) Be of a usable shape, topography and size that reflects its intended use*
  - iv) Meet applicable provincial soil regulations; and*
  - v) Meet the minimum standards for drainage, grading and general condition.”**

The proposed development will include a 0.31 hectare park located at the centre of the proposed community accessed via Street One and the planned pathway along the south lot line. The park will have frontage on Street One. The multi-use pathway provides connections through the Subject Site off Baxter Road. The proposed park will be an appropriate size and shape for recreational use (see Figure 13).

As per Section 51.1(3.2) of the Planning Act, a parkland dedication rate of one hectare per 600 net residential units must be provided. Section 51.1(3.4(a)) of the Planning Act limits the conveyance percentage to a maximum of 10% of the gross land area where the Subject Site is less than 5 hectares in area. 0.31 hectares of parkland must be provided for the proposed mixed use blocks. The proposed 0.31 hectare park meets the parkland dedication requirements.

Section 4.4.3 provides policy direction for parks in the Inner Urban Transect.

Policy 1 of Section 4.4.3 states (*emphasis added*):

*“To provide new parks in the Downtown Core and Inner Urban Transects, as identified in the Parks and Recreation Facilities Master Plan, the following will apply:*

- a) *An Urban Parks Strategy or similar document shall identify the City's preferred location for parks and recreation facilities where higher-density neighbourhoods exist or are expected;*
- b) *New park sites will be supported in Hubs, Corridors and, when in Neighbourhoods, in the centre of neighbourhoods;*
- c) *The acquisition of large parks in the Downtown Core and Inner Urban area will be prioritized where opportunities arise, for example as part of the LeBreton development;*
- d) *The City shall seek opportunities for urban plazas and parkettes that, alongside recreational uses, consider cultural development opportunities such as providing space for performance, exhibitions, commemoration and ceremony; and*
- e) *The City shall direct that all cash-in-lieu of park land collected through development applications within these transects, as described in Subsection 4.4.1, Policy 4 d) and Subsection 4.4.2, Policy 3), be used for the acquisition of new park land and the improvements to the existing parks within these transects."*

The Subject Site is located within the Pinecrest-Queensview Hub. The proposed park is 0.31 hectares in area and will be able to support a range of recreational spaces. The park is located to the north of a pathway connecting to Baxter Road and has frontage on Street One with the planned Central Square urban plaza area located further to the north. Proximity to the proposed Central Square extends available open space for pedestrians beyond the park boundary. The park and public square contribute to creating a large urban greenspace and amenity area with flexibility to provide a variety of neighbourhood programming for residents and visitors. No schools, recreation centres, or libraries are proposed as part of the Plan of Subdivision application.

#### *3.3.3.4 Section 4.6: Urban Design*

Section 4.6 of the Official Plan sets out policies related to Urban Design and is intended to promote design excellence in Design Priority areas, encourage innovation in site planning and building design, support the objectives of Corridors, Hubs and Neighbourhoods and enable integration of new development with existing communities

Section 4.6.5, Policy 1 of the Official Plan states:

*"Development throughout the City shall demonstrate that the intent of applicable Council-approved plans and design guidelines are met."*

The proposed subdivision will facilitate future development of mid to high-rise, mixed use buildings with a variety of commercial and residential units that will be designed in line with the City's approved plans and guidelines.

Section 4.6.5, Policy 2 of the Official Plan states:

*"Development in Hubs and along Corridors shall respond to context, transect area and overlay policies. The development should generally be located to frame the adjacent street, park or greenspace, and should provide an appropriate setback within the street context, with clearly visible main entrances from public sidewalks. Visual impacts associated with above grade utilities should be mitigated."*

The Subject Site is located within the Hub designation. The proposed development concept included with this Plan of Subdivision application has been designed with buildings that frame Baxter Road and Street One, provide appropriate building height and setbacks. Prominent main entrances and active frontages are proposed along Baxter Road near the pedestrian bridge to Queensview Station and along Street One providing direct pedestrian connections to the street. The proposed curbsless design and wide sidewalks for Street One where it meets the proposed central Square and the public park establishes a “liveable” street with standard vehicle and pedestrian traffic functions on a day to day basis that can be closed to traffic for special events. Ground level retail and commercial uses will add vibrancy to the streetscape. Detailed plans specific to each block including proposed building heights, transition and mitigation measures will be provided through separate Site Plan and Rezoning applications.

Section 4.6.5, Policy 3 of the Official Plan states:

*“Development shall minimize conflict between vehicles and pedestrians and improve the attractiveness of the public realm by internalizing all servicing, loading areas, mechanical equipment and utilities into the design of the building, and by accommodating space on the site for trees, where possible. Shared service areas, and accesses should be used to limit interruptions along sidewalks. Where underground parking is not viable, surface parking must be visually screened from the public realm.”*

Direct pedestrian access to the Subject Site will be provided from principal building entrances to the public sidewalk along Baxter Road and Street One, as applicable. Entrances to grade level commercial spaces will be provided off Baxter Road and Street One. A combination of soft and hard landscaping treatments and street trees will be provided along Baxter Road and Street One. A multi-use path with soft landscape area, vegetated trees and shrubs provides additional screening from the low-rise residential dwellings abutting the Subject Site to the south. Motor vehicle access to proposed underground garages is primarily provided off Baxter Road for each proposed development block to prioritize the pedestrian realm, park and urban plazas along Street One.

Section 4.6.6, Policy 1 of the Official Plan states (*emphasis added*):

*“To minimize impacts on neighbouring properties and on the public realm, transition in building heights shall be designed in accordance with applicable design guidelines. In addition, the Zoning By-law shall include transition requirements for Mid-rise and High-rise buildings, as follows:*

- a) Between existing buildings of different heights;*
- b) Where the planned context anticipates the adjacency of buildings of different heights:*
- c) Within a designation that is the target for intensification, specifically:*
  - i) Built form transition between a Hub and a surrounding Low-rise area should occur within the Hub; and*
  - ii) Built form transition between a Corridor and a surrounding Low-rise area should occur within the Corridor.”*

The Subject Site is designated Hub in the Official Plan. Policies of the Official Plan generally permit building heights up to high-rise in the Hub designation. Existing low-rise buildings are located immediately to the south of the Subject Site along Dempsey Avenue. Properties to the south are designated Neighbourhood in the Official Plan and are anticipated to remain low-rise.

A range of mid and high-rise mixed-use buildings are proposed on the Subject Site. The proposed building height, and setback from the south lot line creates a transition in the rear yard from abutting low-rise buildings to the south and mitigates potential visual impacts from the proposed development concept. Detailed plans and elevations specific to each block will be provided through separate Site Plan and Rezoning application to ensure compatibility.

Section 4.6.6, Policy 2 of the Official Plan states (*emphasis added*):

*“Transitions between Mid-rise and High-rise buildings, and adjacent properties designated as Neighbourhood on the B-series of schedules, will be achieved by providing a gradual change in height and massing, through the stepping down of buildings, and setbacks from the Low-rise properties, generally guided by the application of an angular plane as may be set in the Zoning By-law or by other means in accordance with Council-approved Plans and design guidelines.”*

Existing low-rise buildings located to the south of the Subject Site along the rear lot line are designated Neighbourhood with an Evolving Neighbourhood Overlay in the Official Plan. Properties to the south are anticipated to remain low-rise. The proposed mid and high rise buildings at the rear of the Subject Site provide a minimum 15 metre podium setback and greater than 20 metre tower setback to establish a transition from abutting low-rise buildings to the south. Detailed plans and elevations specific to each block will be provided through separate Site Plan and Rezoning application to ensure an adequate transition to abutting lots to the south.

Section 4.6.6, Policy 9 of the Official Plan states (*emphasis added*):

*“High-rise buildings shall require separation distances between towers to ensure privacy, light and sky views for residents and workers. Responsibilities for providing separation distances shall be shared equally between owners of all properties where High-rise buildings are permitted. Maximum separation distances shall be achieved through appropriate floorplate sizes and tower orientation, with a 23-metre separation distance desired, however less distance may be permitted in accordance with Council approved design guidelines.”*

The conceptual site plan ensures a minimum tower separation of 23 metres between proposed buildings on the Subject Site, consistent with policies of the Official Plan.

### 3.3.3.5 Drinking Water, Wastewater and Stormwater Infrastructure

Section 4.7 of the Official Plan provides policy direction to ensure that adequate drinking water, wastewater, and stormwater infrastructure is provided.

Section 4.7.1, Policy 7 of the Official Plan states:

*“As part of a complete application, new applications for a plan of subdivision or condominium must demonstrate, to the satisfaction of the City and based on requirements identified in the Infrastructure Master Plan, that a legal and sufficient outlet can be established to support the proposed development.”*

The Servicing and Stormwater Management Report prepared by Novatech states that stormwater drainage will be directed to an underground storage tank and existing 375mm storm sewer within Baxter Road. Additional details can be found in the report included with this submission package.

Section 4.7.1, Policy 23 of the Official Plan states:

*“Applications for new development will demonstrate, to the City’s satisfaction, that adequate services are available and can be allocated to support the proposal:*

- a) Where adequate services do not exist or cannot yet be provided to support a development, the City may use holding provisions in accordance with Zoning By-law to regulate the timing of development; and*
- b) Where adequate services or servicing capacity do not exist to support a proposed plan of subdivision, the City will not issue draft plan approval.”*

The Servicing and Stormwater Management Report prepared by Novatech dated December 22, 2025 confirms that adequate services are available to support the proposed subdivision.

### **3.3.4 Inner Urban Transect**

Section 5.2 of the Official Plan sets out general policies and guidance for proposed development within the Inner Urban Transect.

Section 5.2.1, Policy 3 of the Official Plan states:

*“The Inner Urban Transect is generally planned for mid- to high-density development, subject to:*

- a) Proximity and access to frequent street transit or rapid transit;*
- b) Limits on building heights and massing, as per the underlying functional designation, and the separation of tower elements, established through secondary plans or area-specific policy, the functional designations and urban design policies in Subsection 4.6, or as a result of the application of heritage conservation policies in Subsection 4.5; and*
- c) Resolution of any constraints in water, sewer and stormwater capacity.”*

The proposed subdivision will create blocks to facilitate future mixed use development on the Subject Site. The proposed development concept represents an appropriate example of intensification in proximity to planned rapid transit infrastructure at Queensview Station. The conceptual plan shows building heights and massing that align with the evolving built form of the neighbourhood. Policies of the Pincrest-Queensview Stations Secondary Plan apply to the Subject Site and may limit permitted uses and maximum permitted building height.

High-rise buildings are proposed nearest the north and east of the Subject Site to integrate with the future context, while mid-rise tower and podium forms positioned toward the south lot line are intended to provide a gradual transition to adjacent low-rise residential dwellings.

The Servicing and Stormwater Management Report prepared by Novatech confirms that adequate services are available to support the proposed subdivision.

The conceptual development plan supports the City’s intensification objectives by facilitating additional housing units while maintaining compatibility with the existing and planned characteristics of the area.

Section 5.2.1, Policy 4 of the Official Plan states (*emphasis added*):

*“The Inner Urban Transect shall continue to develop as a mixed-use environment, where:*

- a) Hubs and a network of Mainstreets and Minor Corridors provide residents with a full range of services within a walking distance from home, in order to support the growth of 15-minute neighbourhoods;*
- b) Small, locally oriented services may be appropriately located within Neighbourhoods;*
- c) Existing and new cultural assets are supported, including those that support music and nightlife;*
- d) Larger employment uses are directed to Hubs and Corridors; and*
- e) Increases in existing residential densities are supported to sustain the full range of services noted in Policy a).”*

Policies of the Official Plan direct areas designated “Hub” to accommodate increased residential growth and mixed-use development that support a wide variety of housing types and services for residents within walking distance from home. The proposed subdivision will create development blocks to facilitate a range of mixed-use buildings on the Subject Site, consistent with policies of the Official Plan for Hubs within the Inner Urban Transect.

Section 5.2.2, Policy 2 of the Official Plan states:

*“The transportation network for the Inner Urban Transect shall:*

- a) Prioritize walking cycling and transit; and*
- b) Accommodate motor vehicle access and movement provided doing so does not erode the public realm nor undermine the priority of pedestrians, cyclists and transit users.”*

The Subject Site is located within proximity of cycling and transit infrastructure along Iris Street with a connection to Greenbank Road to the west and Pinecrest Creek Pathway to the east. Motor vehicle access to proposed garages within the building envelope or underground is primarily provided off Baxter Road for each proposed development block to prioritize the pedestrian realm, park and urban plazas along Street One. The pathway along the south lot line provides an alternate point of access for pedestrians and cyclists to avoid potential conflict with automobiles along Baxter Road.

Section 5.2.3, Policy 1 of the Official Plan states:

*“Within Hubs, permitted building heights, are as follows:*

- a) Up to a 300 metre radius or 400 metres walking distance, whichever is greatest, of an existing or planned rapid transit station, not less than 3 storeys and up to High-rise;*
- b) High-rise 41+ where permitted by a secondary plan;*
- c) Outside the area described by Policy a), not less than 3 storeys and up to a High-rise where the parcel is of sufficient size to allow for a transition in built form massing; and*
- d) On parcels that are within a designated Hub but not covered by a local plan, High-rise buildings shall only be permitted on parcels of sufficient size to allow for a transition in built form massing, and their height shall be lowest at the*

*outer edge of the Hub and tallest at the centre of the Hub and near a rapid transit station.”*

The Subject Site is designated “Hub” and is within 400 metres walking distance from the planned Queensview O-Train station via the pedestrian bridge across Highway 417. The Official Plan defines “High-rise” as up to 40 storeys in building height. Policies of the Pinecrest-Queensview Secondary Plan apply to the Subject Site and may limit permitted uses and maximum permitted building height.

Section 5.6.1, Policy 1 of the Official Plan states:

*“The Evolving Overlay will apply to areas that are in a location or at stage of evolution that create the opportunity to achieve an urban form in terms of use, density, built form and site design. These areas are proximate to the boundaries of Hubs and Corridors as shown in the B-series of schedules of this Plan. The Evolving Overlay will be applied generally to the properties that have a lot line along a Minor Corridor; lands 150 meters from the boundary of a Hub or Mainstreet designation; and to lands within a 400-metre radius of a rapid transit station. The Overlay is intended to provide opportunities that allow the City to reach the goals of its Growth Management Framework for intensification through the Zoning By-law, by providing:*

- a) Guidance for a gradual change in character based on proximity to Hubs and Corridors,*
- b) Allowance for new building forms and typologies, such as missing middle housing;*
- c) Direction to built form and site design that support an evolution towards more urban built form patterns and applicable transportation mode share goals; and*
- d) Direction to govern the evaluation of development.*

The Subject Site is suitable for intensification being located within a Hub. The proposed development of a range of mid to high-rise, mixed-use buildings on the Subject Site is consistent with City objectives to establish a more urban community character and to create opportunities for infill and intensification within walking distance of rapid transit routes in the Inner Urban Transect.

Policy 2 of Section 5.6.1 of the Official Plan states:

*“Where an Evolving Neighbourhood Overlay is applied:*

- a) The Zoning By-law shall provide development standards for the built form and buildable envelope consistent with the planned characteristics of the overlay area, which may differ from the existing characteristics of the area to which the overlay applies; and*
- b) The Zoning By-law shall include minimum-density requirements as identified in Table 3a, and permissions to meet or exceed the density targets of Table 3b.”*

The Subject Property is designated Hub in the Official Plan. Policies of the Official Plan establish a targeted residential density of 250 dwellings per net hectare for the Pinecrest-Queensview Hub.

The density of the conceptual development proposal is approximately 711 dwelling units per net hectare, which exceeds the residential density targets for Hubs in the Official Plan.

Section 5.6.1, Policy 6 of the Official Plan states:

*“Zoning By-law development standards and development on lands with an Evolving Overlay should generally include built form and site design attributes that meet most of the urban characteristics described in Table 6 in Section 5, and where suburban attributes are retained, that these do not structurally impede the achievement of a fully urban site design over time.”*

The urban characteristics outlined in Table 6 of Section 5 of the Official Plan are as follows:

- *Shallow front yard setbacks and in some contexts zero front yards with an emphasis on built-form relationship with the public realm*
- *Principal entrances at grade with direct relationship to public realm*
- *Range of lot sizes that will include smaller lots, and higher lot coverage and floor area ratios*
- *Minimum of two functional storeys*
- *Buildings attached or with minimal functional side yard setbacks*
- *Small areas of formal landscape that should include space for soft landscape, trees and hard surfacing*
- *No automobile parking, or limited parking that is concealed from the street and not forming an integral part of a building, such as in a front facing garage*

The proposed subdivision will facilitate development of a range of mid to high-rise mixed use buildings on the Subject Site located within 400 metres radius of Queensview Station. The proposed development concept has limited front yard setbacks to establish a strong interface with the public realm along Baxter Road and Street One. To create active street frontages for each future development block, principal entrances to commercial and residential portions of each building will provide access off Street One and Baxter Road complete with considerations for soft and hard landscaping, street trees, and raised planters. The proposed development concept has off-street parking that will be primarily located within the building envelope or underground. Entrances to parking garages are proposed off Baxter Road, separated from principal entrances and active frontages located along Street One and the proposed Central Square urban plaza.

### **3.3.5 Hub Designation**

Section 6.2 of the Official Plan provides policy direction for development in the Hub designation.

*“Development within a Hub:*

- a) *Shall direct the highest density close to the transit station or stop so that transit is the most accessible means of mobility to the greatest number of people;*
- b) *Shall encourage large employment, commercial or institutional uses locate close to the transit station;*
- c) *May be required, through the Zoning By-law, to include mixed uses on sites and within buildings located within 300 metre radius or 400 metres walking distance, whichever is greatest of an existing or planned transit station, through measures including but not limited to:*

- i) Requiring commercial and service uses on the ground floor of otherwise residential, office and institutional buildings;
- ii) Requiring residential and/or office uses on the upper floors of otherwise commercial buildings; and
- iii) May require minimum building heights in terms of number of storeys to ensure multi-storey structures where uses can be mixed vertically within the building;
- d) Shall establish safe, direct and easy-to-follow public routes for pedestrians and cyclists between transit stations and all locations within the Hub;
- e) Shall create a high-quality, comfortable public realm throughout the Hub that prioritizes the needs of pedestrians, cyclists and transit users;
- f) Shall establish buildings that:
  - i. Edge, define, address and enhance the public realm through building placement, entrances, fenestration, signage and building facade design;
  - ii. Place principal entrances so as to prioritize convenient pedestrian access to the transit station and the public realm; and
  - iii. Place parking, loading, vehicle access, service entrances and similar facilities so as to minimize their impact on the public realm.
- g) Shall be subject, through the Zoning By-law, to motor vehicle parking regulations that support the Hub's prioritizing of transit, walking and cycling, including as appropriate:
  - i. Reduction or elimination of on-site minimum parking requirements;
  - ii. Maximum limits on parking supply;
  - iii. Prohibition of surface parking lots as a main or accessory use, other than publicly-operated park and-ride facilities;
  - iv. Regulation, pricing, metering and enforcement of public on- and off-street parking to balance supply and demand;
  - v. Establishment of residential on-street parking permit zones; and
  - vi. Despite the above, visitor parking shall continue to be required for high-density residential uses, in order to prevent visitor demand for parking from creating undue demand on public parking facilities; and
- h) Prohibit uses causing or likely to cause nuisance due to noise, odour, dust, fumes, vibration, radiation, glare or high levels of heavy truck traffic."

The Subject Site is designated as a Hub located near rapid transit within 400 metres of Queensview Station. The proposed subdivision includes a range of mixed-use blocks intended for mid to high-rise development, with the greatest building heights nearest Queensview Station. The future mixed-use development will provide a greater density of residential and commercial uses within the Pinecrest-Queensview Hub. A strong relationship with the public realm will be created through providing minimal front yard setbacks along Baxter Road and Street One. Active street frontages will be achieved by locating principal entrances for residential and commercial uses along the street frontages, complemented by soft and hard landscaping, street trees, and raised planters to create an inviting streetscape for pedestrians. Off-street parking will be integrated within building envelopes or provided underground, with garage access primarily from Baxter Road to maintain uninterrupted pedestrian activity along Street One and near the proposed urban plaza.

### 3.3.6 Protection of Health and Safety

Section 10 of the Official Plan provides policy direction for the protection of health and safety.

Sections 10.1.1 to 10.1.5 speak to natural hazards such as flooding, unstable soils or bedrock, and wildfires. The Subject Property is not within a floodplain and does not have unstable soils or bedrock.

Section 10.1.6 speaks to contaminated sites and requires development to only take place on sites where the environmental conditions are suitable for the proposed use. Policy 1 of Section 10.1.6 requires environmental site assessments and remedial or risk assessment/risk management activities reports to be completed as part of a development application. City staff identified the need for a Phase 1 Environmental Assessment to be completed as part of the application. A Phase 1 ESA has been completed by Paterson Group dated November 3, 2025. The report concludes:

*“Based on the existing land use, the Phase I Property is considered to be used for industrial purposes as defined by O.Reg. 153/04 and as such, an RSC is required as there is a change in use of the Phase I Property to a more sensitive land use (residential).*

*As a result of the additional historical research, records update and site visit undertaken as part of this assessment in order to meet the requirements of O.Reg. 153/04, as amended, it is our opinion that a Phase II ESA Update and remediation program are required for the Phase I Property and should be completed in conjunction with site redevelopment activities”*

Section 10.3 speaks to building resiliency to the impacts of extreme heat. Policy 1 of Section 10.3 states:

*“Trees will be retained and planted to provide shade and cooling by:*

- a) Applying the urban tree canopy policies in Subsection 4.8 and other sections of the plan;*
- b) Prioritizing them in the design, and operation of parks and the pedestrian and cycling networks and at transit stops and stations for users wherever possible; and*
- c) Encouraging and supporting maintenance and growth of the urban tree canopy on residential, commercial and private property.”*

The Conceptual Landscape Plan provided with the submission package shows that 29 of 71 existing trees on the Subject Site are proposed for retention. New trees will be proposed as part of the future development, including street trees along Baxter Road and Street One and potential planting opportunities in the public park and pathway blocks subject to detailed design and facility fit for the park. This will provide for an improved streetscape with a mix of hard and soft landscaping. As part of a future site plan application, the proposal will be designed to meet the City’s policies and objectives regarding tree canopy by providing increased canopy coverage.

Section 10.4 speaks to enhancing personal security through design. Policy 1 of Section 10.4 states:

*“When reviewing development, the City will consider measures to enhance safety and security through such means as:*

- a) Provision of outdoor lighting in spaces intended for public use after dark that is sufficient to support the activities planned for that space;*

- b) An overall pattern of design that avoids creation of enclosed areas or areas such as narrow recesses between buildings that could be used to entrap persons passing through a space;
- c) *Preservation of unobstructed sight lines for persons passing through public spaces and opportunities for public spaces to be overlooked by people in adjacent buildings or other public spaces;*
- d) Provision of a mix of uses on corridors that promotes activity and social interaction at various times of the day and night and are served by transit routes; and
- e) *Where there are overpasses and tunnels, provide alternative routes at grade, where possible; and*
- f) *Provision of pedestrian and cycling connections between neighbourhoods and across barriers shall be designed for passive supervision and wayfinding, where possible.”*

The conceptual development plan provides a mix of residential and commercial uses to promote a variety of activity at all times of the day. Opportunities for generous glazing in commercial and residential units facing the street at grade level and direct connections to the public realm could be provided to establish “eyes on the street” and enhance safety and visibility for all users. Building podiums are designed to frame Street One and are planned around a curbsless, raised intersection at the bend in Street One that functions as a pedestrian plaza at the Centre of the community, prioritizing walkability, fostering community interaction and increasing passive surveillance in the area. The public park block will serve as a focal point for the community, with connections from Baxter Road and Queensview Station via Street One and a planned multi-use pathway along the southern lot line. Further details will be provided through separate Site Plan and Rezoning applications to facilitate development for each newly created block.

**The proposed Plan of Subdivision is consistent with the policies of the City of Ottawa Official Plan.**

### **3.4 Pinecrest and Queensview Stations Secondary Plan**

The Subject Site is designated “Hub” on *Schedule A – Designation Plan*, and is located within the Iris-Baxter Sector of the Pinecrest and Queensview Stations Secondary Plan (the “Secondary Plan”) (see Figure 11). Policies of the Secondary Plan take precedence over other policies of the Official Plan.

Section 2.1 of the Pinecrest and Queensview Stations Secondary Plan establishes general policies with respect to the Hub Designation.

Section 2.1, Policy 3 of the Secondary Plan states:

*“To make most efficient use of lands with the best access to transit, minimum building heights for all new buildings shall be three storeys. Portions of a building that do not front onto a public street may be less than three storeys without an amendment to this Plan.”*

The proposed subdivision will create blocks for future mid to high-rise, mixed-use development. A minimum building height of three storeys is consistent with the proposed development concept.

A Rezoning application to address building height and separate Site Plan applications are required at some later date to facilitate development of each newly created block

Section 2.1, Policy 6 of the Secondary Plan states:

*“Buildings within the Hub designation that front onto a public street, park or publicly accessible pathway should have active frontages at grade level to frame and animate the public realm.”*

The proposed development concept has limited front yard setbacks to establish a strong interface with the public realm along Baxter Road and Street One. To create active street frontages for each future development block, principal entrances to commercial and residential portions of each building will be designed to provide entrances off Street One and Baxter Road.

Section 2.1, Policy 7 of the Secondary Plan states:

*“Shipping, receiving, loading, waste removal, and similar functions should be located on a different building façade than one with an active frontage, where possible, in order to minimize curb cuts and support an attractive streetscape on streets with active frontages”*

The proposed subdivision will establish motor vehicle access to proposed underground garages and servicing access for deliveries, loading and waste collection will be provided primarily off Baxter Road. Each proposed development block will be designed to prioritize the pedestrian realm, pathways, park area and urban plazas along Street One.

Section 2.1, Policy 8 of the Secondary Plan states:

*“Where a property within the Hub designation abuts a property within the Neighbourhood designation, redevelopment should consider opportunities to mitigate privacy impacts, such as by providing a landscape buffer”*

The proposed subdivision will create a 15-metre wide pathway block abutting properties to the south of the Subject Site designated “Neighbourhood” in the Official Plan and will be landscaped to screen the low-rise properties from future development. . The block will also provide a necessary connection

Section 3.1 of the Pinecrest and Queensview Stations Secondary Plan provides policy direction with respect to Infrastructure and Servicing capacity for the entire plan area. The preamble to infrastructure policies under Section 3.1 of the Secondary Plan states: *“there is limited sewer capacity in the Plan Area and any available sewer capacity is considered on a first-come, first-served basis through development applications”*. The intent of this plan of subdivision application is to reserve servicing capacity on the basis of “first come first-served”.

Section 3.1, Policy 2 of the Secondary Plan states:

*“Local Servicing Studies, and financial plans to support those studies, may be required during the review of development applications to determine the capacity of water, stormwater, and sanitary sewers.”*

The plan of subdivision application and supporting studies will confirm servicing capacity for future development on the Subject Site.

Section 3.2 of the Pinecrest and Queensview Stations Secondary Plan provides policy direction with respect to Connectivity, Pathways and Streets for the entire plan area.

Section 3.2, Policy 1 of the Secondary Plan states:

*“Proponents of development may be required to provide new public or private streets or active transportation connections with public access easements to improve overall local connectivity or facilitate public access to neighbourhood amenities such as parks, commercial land uses or transit, as identified on Schedule C – Maximum Building Heights, Connectivity and Parks.”*

The plan of subdivision application proposes a new private street (Street One) and a pedestrian pathway along the south lot line providing access a new public park and urban plaza located at the centre of the proposed subdivision. Specific site design and development details will be confirmed through separate Site Plan and Rezoning applications for each newly created block.

Section 3.2, Policy 2 of the Secondary Plan states:

*“Active transportation connections identified on Schedule C – Maximum Building Heights, Connectivity and Parks, shall be publicly accessible pathways that should secured through a development application.*

- a) The detailed requirements for active transportation connections will be determined through individual development applications, but are typically multi-use pathways set within a 6-metre wide walkway block (3 metres for the bi-directional pathway and 1.5 metres on each side for landscaping and snow storage).*
- b) The land for active transportation connections may be dedicated to the City or potentially held as private land with a public easement, provided the pathway and associated landscaping, lighting and fencing is designed and constructed to the satisfaction of the City.”*

The plan of subdivision application proposes a new pedestrian pathway block along the south lot line to fulfill the Proposed Active Transportation Connection shown on Schedule C. The pathway block will provide access to the public park, to the transit station through a connection to Street One, and to facilitate future connections to the broader neighbourhood as redevelopment of properties to the east occur over time.

Section 3.3 of the Pinecrest and Queensview Stations Secondary Plan provides policy direction with respect to Built Form for the entire plan area.

Section 3.3, Policy 1 of the Secondary Plan states:

*“Development shall frame public streets, and where feasible, should frame other elements of the public realm, such as parks, and private streets and active transportation connections that have public access easements.”*

The proposed subdivision will create blocks to facilitate development that represents an appropriate example of intensification in proximity to planned rapid transit infrastructure at Queensview Station. As shown on Figure 14 above, the conceptual development proposal shows building footprints that establish a street wall and urban plaza condition along Street One and Baxter Road. Proposed building heights and massing are designed to align with the evolving built form of the neighbourhood providing the greatest heights along Highway 417, transitioning to mid-rise with appropriate building setbacks along the south lot line. A separate site plan application will be submitted for each newly created block to address detailed policies of the Secondary Plan for building design on a site-specific basis.

Section 3.3, Policy 2 of the Secondary Plan states:

*“Building podiums should animate the pedestrian realm, contribute to a positive street wall condition, and relate to the adjacent buildings in massing, height and architectural rhythm”.*

The conceptual site plan shows podiums framing Street One to establish a street wall condition that complements the proposed Central Square, public park and location of adjacent buildings. Specific podium design will be determined through separate Site Plan and Rezoning applications to facilitate development on each block.

Section 3.3, Policy 3 of the Secondary Plan states:

*“Where a single building podium forms the base for more than one high-rise building, an at-grade pedestrian connection may be required through the podium to the interior of the site”*

The conceptual site plan shows potential podiums with two towers. At-grade pedestrian connections through large podiums will be considered as part of the design process to be determined through separate Site Plan and Rezoning applications.

Section 3.3, Policy 4 of the Secondary Plan states:

*“The ground floor of a building facing the public realm should be designed to be highly transparent and to animate the public realm through the incorporation of elements such as active frontages or entrances, windows, porches, and facade articulations, where appropriate”*

The conceptual site plan shows building podiums with main entrances fronting on Street One to animate the streetscape and connect with the proposed urban plaza and public park. Specific facade design and approach for articulation will be determined through separate Site Plan and Rezoning applications on a site-specific basis.

Section 3.3, Policy 5 of the Secondary Plan states:

*“The maximum tower floorplate of a high-rise residential or mixed-use building should be approximately 750 square metres in order to minimize shadow and microclimate impacts”*

The conceptual site plan shows tower plates of approximately 750 square metres. Tower floor plates will be determined through separate Site Plan and Rezoning applications to facilitate development on each block.

Section 3.3, Policy 6 of the Secondary Plan states:

*“High-rise buildings shall only be permitted on lots with a minimum area of:  
a) Approximately 1,800 square metres for interior lots; or  
b) Approximately 1,350 square metres for corner lots.”*

The proposed plan of subdivision will create future development blocks that can accommodate mid to high-rise buildings. Specific building heights will be determined through separate Site Plan and Rezoning applications for each block.

Section 3.3, Policy 7 and Policy 8 of the Secondary Plan states:

*“The minimum separation for the tower portion of high-rise buildings shall be:  
a) 23 metres for buildings up to 30 storeys;  
b) 25 metres for buildings 31 storeys or greater;  
i) Where a high-rise building 30 storeys or less abuts a building 31 storeys or greater, the minimum separation in (b) above applies.  
c) Shared between abutting lots.”*

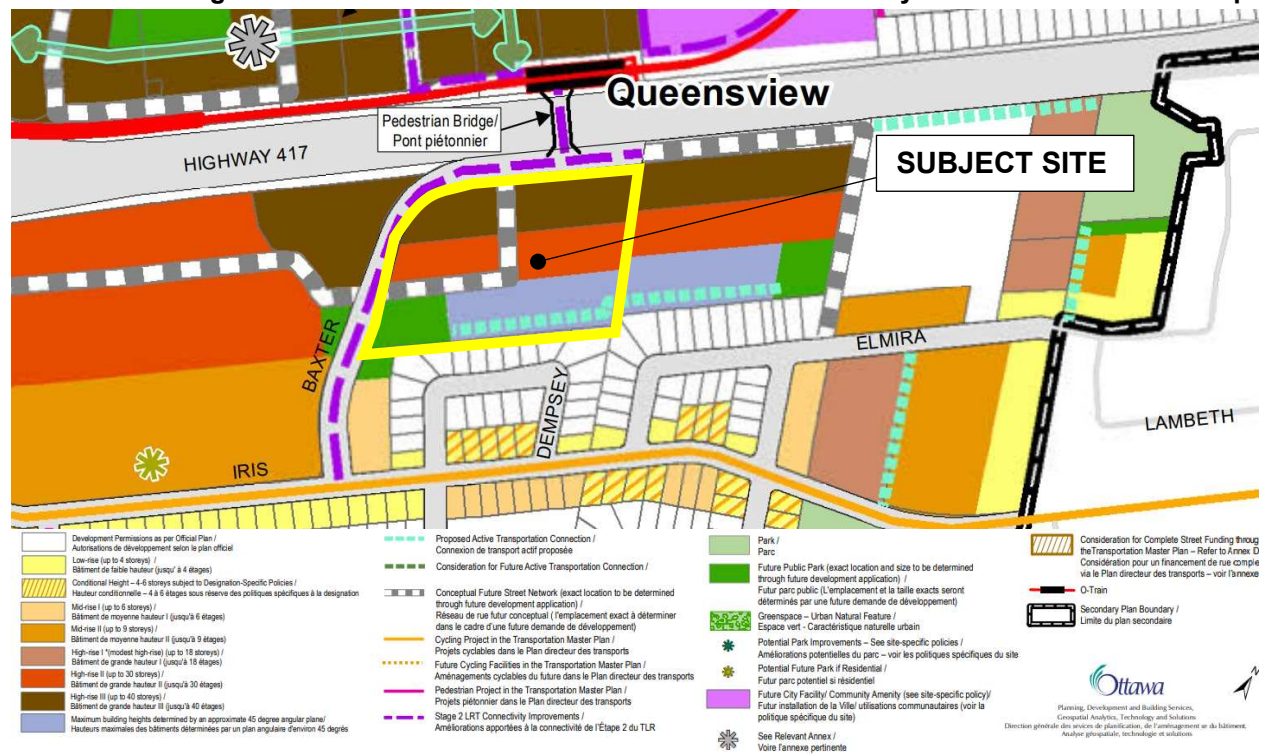
And,

*“Notwithstanding policy 7) above, on constrained sites, minimum tower separations may be reduced by up to 2 metres without an amendment to this Plan.”*

The conceptual site plan shows separation distances greater than 25 metres between each adjacent mid to high-rise tower. Specific building heights and separation distances will be determined through Site Plan and Rezoning applications for each block.

Section 3.4 of the Pinecrest and Queensview Stations Secondary Plan provides policy direction with respect to Building Heights for the entire plan area.

Figure 19: Pinecrest and Queensview Stations Secondary Plan – Schedule C Excerpt



Section 3.4, Policy 1 of the Secondary Plan states:

*“Maximum building heights are identified in Schedule C – Maximum Building Heights, Connectivity and Parks, and in some cases are further detailed in Designation-Specific Policies and Site-Specific Policies.”*

As shown on Figure 19 above, policies of the Secondary Plan permit maximum building heights up to 40 storeys abutting Highway 417, up to 30 storeys in the central portion, and as determined by an approximate 45 degree angular plane where abutting the low-rise community to the south of the Subject Site. Building heights will be determined through separate applications for Site Plan and Rezoning on a site-specific basis.

Section 3.5 of the Pinecrest and Queensview Stations Secondary Plan provides policy direction with respect to Parks, Privately-Owned Public Spaces, and the Urban Tree Canopy for the entire plan area.

Section 3.5, Policy 2 of the Secondary Plan states:

*“Parkland dedication shall be provided as part of development applications in accordance with the Parkland Dedication By-law, and in locations approximately identified on Schedule C – Maximum Building Heights, Connectivity and Parks. Adjustments to the location, size or configuration of new or expanded parks may occur through the development application process without an amendment to this Plan.”*

A 0.31 hectare public park will be provided based on 10 percent of the gross land area of the Subject Site as required by provisions of the Planning Act. As shown on Schedule C of the Secondary Plan, the proposed location of the park abutting Baxter Road is fragmented across three separate properties and divided by the roadway. To provide a more functional and cohesive public space, the park is proposed in an interior location at the centre of the subdivision. This central placement will create a larger, rectangular public park space with direct access from Street One, connections to the pedestrian pathway along the southern lot line, and connection to the planned urban plaza. The park will serve as a buffer to the low-rise residential area to the south while integrating with Citizen Plaza and planned mixed-use buildings. This approach allows the City to establish a larger, centrally located park earlier in the redevelopment process.

Section 3.5, Policy 3 of the Secondary Plan states:

*“Privately-owned public spaces (POPS) are encouraged in addition to parkland dedication where they can provide a positive contribution to the public realm, such as publicly accessible landscaped seating areas or gardens on private property along parks, pathways or transit plazas.”*

In addition to the public park, a centrally located urban plaza (Central Square) will provide landscaping and seating areas off Street One directly abutting proposed high-rise buildings, connected to the pedestrian path to the south and within a short walk of the pedestrian crossover to Queensview Station. Building podiums could be designed to frame Street One and proposed Central Square with a curbsless, raised intersection at the bend of Street One designed to prioritize walkability, fostering community interaction and increasing opportunities for activation of the area through special events. The public park block will serve as a focal point for the community, with connections from Baxter Road and Queensview Station via Street One and a planned multi-use pathway along the southern lot line.

Section 3.5, Policy 4 of the Secondary Plan states:

*“Where a POPS is identified in a development application, it shall be implemented through a Site Plan Control agreement and should meet the following objectives:*

- a) Be publicly accessible and connected to pedestrian facilities in the public realm.*
- b) Contribute to the animation of the right-of-way.*
- c) Consider opportunities to coordinate the location of POPS on abutting properties in order to create linked or consolidated POPS.*
- d) Ensure development fronting a POPS has active entrances or transparent glazing to allow for passive supervision of POPS users.”*

Specific details related to the proposed urban plaza will be determined through individual Site Plan applications for each newly created block.

Section 3.5, Policy 5 of the Secondary Plan states:

*“Development should minimize impacts to the urban tree canopy and seek opportunities to retain and plant trees that will provide a positive contribution to the urban tree canopy, towards the target in Volume 1 of the Official Plan of 40% canopy coverage within the Plan area”*

Specific details related to proposed landscaping and tree plantings will be determined through individual Site Plan applications for each newly created block. A conceptual landscape plan is included with the plan of subdivision application to provide the general landscaping design intent. Section 7.1 of the Pinecrest and Queensview Stations Secondary Plan provides sector-specific policy direction for the Baxter-Iris Sector.

Section 7.1, Policy 4 of the Secondary Plan states (*emphasis added*):

*“The future renewal or reconstruction of Baxter Road should explore the feasibility of the following:*

- a) Additional vehicular passenger pick-up and drop-off areas;*
- b) Cycle tracks to replace on-street bike lanes;*
- c) Cycling ramps at the north and south sides of the pedestrian bridge to Queensview Station in order to allow for continuous cycling access over Highway 417 without entering a fare-paid zone; and*
- d) Opportunities for pedestrian crossings, such as pedestrian crossovers.”*

The conceptual development plan considers a direct connection from Building A to the pedestrian crossover. Specific details related to the proposed pedestrian crossover to the Subject Site will be determined through an individual Site Plan application for the relevant block.

Section 7.2 of the Pinecrest and Queensview Stations Secondary Plan provides site-specific policy direction with respect to the Baxter-Iris Sector. Section 7.2, Policies 18 through 23 provide specific reference to 1101 Baxter Road.

Section 7.2, Policy 18 of the Secondary Plan states:

*“Upon redevelopment, a sidewalk shall be required in the right-of-way along the east and south side of Baxter Road, adjacent to 1101 Baxter Road.”*

As part of the Stage 2 LRT Station Connectivity Enhancement Study, the City has developed a functional design of improved active transportation facilities on Baxter Road, to connect pedestrians and cyclists to the new Queensview Station pedestrian bridge. A sidewalk is not shown as part of the City’s functional design. Adequate space for a sidewalk is included in the conceptual development proposal included with this plan of subdivision application. Further details are included with the TIA report included with the submission package.

Section 7.2, Policy 19 of the Secondary Plan states:

*“Upon redevelopment, cycling facilities may be required in the right-of-way along the east and south side of Baxter Road, adjacent to 1101 Baxter Road.”*

As part of the Stage 2 LRT Station Connectivity Enhancement Study, the City has developed a functional design of improved active transportation facilities on Baxter Road, to connect pedestrians and cyclists to the new Queensview Station pedestrian bridge. Further details are included with the TIA report included with the submission package.

Section 7.2, Policy 20 of the Secondary Plan states:

*“Redevelopment shall include a new public or private street(s) that demonstrate all of the following:*

- a) Improved overall connectivity through the site;*
- b) Direct access to Queensview Station;*
- c) Prioritized safety for people using active transportation; and*
- d) Connections to the broader street network.”*

The proposed plan of subdivision includes a new private street (Street One). Street One is designed to improve connections from Baxter Road and the surrounding area through the Subject Site and connections to the pedestrian bridge. Access to off-street parking, service traffic and loading is primarily from Baxter Road. Street One will prioritize pedestrian and active transportation traffic, while accommodating low speed automobile traffic for accessibility and convenience.

Section 7.2, Policy 22 of the Secondary Plan states:

*“Notwithstanding policy 2.1(6), development in the north-west part of the site fronting Baxter Road may provide active frontages along the new public or private street(s) internal to the site instead of along Baxter Road in order to mitigate noise impacts from Highway 417, provided the following conditions are met:*

- a) The new public or private street conforms to policy 20) above;*
- b) Building façades at grade level fronting Baxter Road, and landscaping between buildings and Baxter Road, should support pedestrian comfort and safety, such as by minimizing blank walls and providing transparent glazing; and*
- c) Shipping, receiving, loading, waste removal, and similar functions should be located to minimize impacts to the public realm.”*

Refer to comments on Section 7.2, Policy 20 above.

Section 7.2, Policy 23 of the Secondary Plan states:

*“Development at the north part of the site may include a pedestrian bridge over Baxter Road to connect directly to the pedestrian bridge over Highway 417 to Queensview Station without an amendment to this Plan.”*

The conceptual development plan considers a direct connection from Building A to the pedestrian crossover. Specific details related to the proposed pedestrian crossover to the Subject Site will be determined through individual Site Plan application for the relevant block.

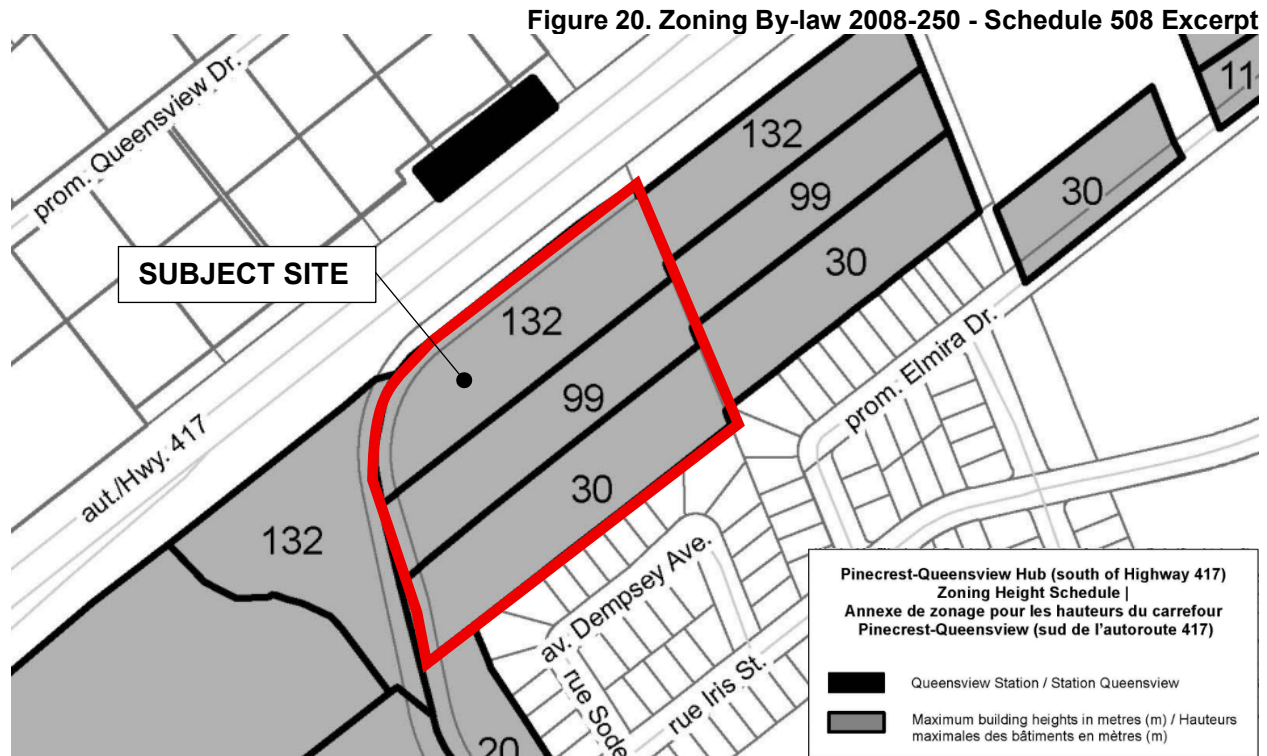
**The proposed Plan of Subdivision application is consistent with the policies of the Pinecrest and Queensview Stations Secondary Plan.**

### 3.5 City of Ottawa Zoning By-law 2008-250

The Subject Site is zoned Mixed Use Centre, Subzone 20, Schedule 508 (MC20 S508) in the City of Ottawa Zoning By-law 2008-250. The Mixed Use Centre zone permits the proposed land uses. Specific land use permissions will be determined through Site Plan and Rezoning applications for each proposed block.

#### 3.5.1 Zoning Review

Policies of the City of Ottawa Official Plan support a mix of transit-supportive land uses with building heights of up to 40 storeys on the Subject Site.



Schedule 508 implements policies of the Pinecrest and Queensview Secondary Plan (see Figure 20). Schedule 508 limits maximum permitted building height on the Subject Site based on proximity to Queensview Station and to provide a height transition to abutting low-rise residential uses to the south.

The draft City of Ottawa Zoning By-law is anticipated to be in-force by mid-2026. The final draft Zoning By-law zones the Subject Site Hub Zone 2, Subzone E, Schedule 508 (H2E S508), consistent with policies of the applicable Secondary Plan.

#### 3.5.2 MC Zoning

The purpose of the MC zone is to:

- (1) ensure that the areas designated Mixed-Use Centres in the Official Plan, or a similar designation in a Secondary Plan, accommodate a combination of transit-supportive uses such as offices, secondary and post secondary schools, hotels, hospitals, large institutional buildings, community recreation and leisure centres, day care centres, retail

uses, entertainment uses, service uses such as restaurants and personal service businesses, and high- and medium-density residential uses;

(2) allow the permitted uses in a compact and pedestrian-oriented built form in mixed-use buildings or side by side in separate buildings; and

(3) impose development standards that ensure medium to high profile development while minimizing its impact on surrounding residential areas

The proposed subdivision will establish blocks to facilitate future development of mid to high-rise, mixed use buildings that support a full range of potential transit-supportive land uses based on proximity and direct pedestrian connection to Queensview Station. The proposed subdivision will create blocks that are consistent with the purpose of the MC zone.

Table 2 provides the Zoning provisions that apply to the MC20 Subzone.

**Table 2: Zoning Provisions for the MC20 Zone**

<b>MC Zoning Provisions (Table 191)</b>	<b>Required</b>	<b>Provided</b>
Minimum Lot Width (m)	No minimum	varies
Minimum Lot Area (m <sup>2</sup> )	No minimum	varies
Minimum Front Yard and Corner Side Yard Setback (m)	<ul style="list-style-type: none"> <li>• abutting a lot in a residential zone – <b>3m</b></li> <li>• abutting the rapid transit corridor – <b>2m</b></li> <li>• other cases – no minimum</li> </ul>	<b>N/A – To be confirmed through Site Plan/Rezoning for each block</b>
Minimum Interior Side Yard Setback (m)	<ul style="list-style-type: none"> <li>• abutting a lot in a residential zone – <b>3m</b></li> <li>• abutting the rapid transit corridor – <b>2m</b></li> <li>other cases – no minimum</li> </ul>	<b>N/A – To be confirmed through Site Plan/Rezoning for each block</b>
Minimum Rear Yard Setback (m)	<ul style="list-style-type: none"> <li>• abutting a lot in a residential zone – <b>6m</b></li> <li>• abutting the rapid transit corridor – <b>2m</b></li> <li>other cases – no minimum</li> </ul>	<b>Minimum 11m setback from south lot line (servicing block) – To be confirmed through Site Plan/Rezoning for each block</b>
Maximum Floor Space Index	No maximum	<b>N/A</b>
Minimum Building Height (m)	18 m	<b>N/A – To be confirmed through Site Plan/Rezoning for each block</b>
Maximum Building Height (m)	as shown by the suffix “H”, <u>on a zoning map</u> , or specified in a subzone or exception where applicable	<b>See Schedule 508 – To be confirmed through Site Plan/ Site Specific Rezoning for each block</b>
Minimum Width of Landscaped Area	No minimum, except that where a yard is provided and not used for required driveways, aisles, parking, loading spaces or outdoor commercial patio, the whole yard must be landscaped	<b>N/A – To be confirmed through Site Plan/Rezoning for each block</b>

The plan of subdivision application is intended to create blocks in order to facilitate future development of the Subject Site and fully conforms with provisions of the applicable MC20 zone. Detailed plans for each newly block created through the subdivision process will be submitted for City review through separate Site Plan applications. Should building heights, setbacks or any other zoning provision exceed the requirements of the MC20 zone, a site-specific Zoning By-law Amendment application will be required in order to facilitate proposed development.

**The proposed Plan of Subdivision application is consistent with the purpose of the Mixed Use Centre Zone and is appropriate for the Subject Site.**

#### **4.0 REVIEW OF SUPPORTING STUDIES**

##### **Phase 1 Environmental Site Assessment**

A Phase 1 ESA has been completed by Paterson Group dated November 3, 2025. The report concludes:

*“Based on the existing land use, the Phase I Property is considered to be used for industrial purposes as defined by O.Reg. 153/04 and as such, an RSC is required as there is a change in use of the Phase I Property to a more sensitive land use (residential).*

*As a result of the additional historical research, records update and site visit undertaken as part of this assessment in order to meet the requirements of O.Reg. 153/04, as amended, it is our opinion that a Phase II ESA Update and remediation program are required for the Phase I Property and should be completed in conjunction with site redevelopment activities”.*

A Record of Site Condition and Phase 2 ESA study will be required as part of the future development applications on the Subject Site.

##### **Geotechnical Investigation**

A Geotechnical Investigation has been completed by Paterson Group, dated September 26, 2025. The report concludes that *“From a geotechnical perspective, the subject site is suitable for the proposed development.”* The report notes that foundation support methods based on the number of underground parking levels proposed. More details on the conditions and recommendations can be found in the Geotechnical Report included with the submission.

##### **Servicing and Stormwater Management Report**

A Servicing and Stormwater Management Report was prepared by Novatech, dated December 22, 2025.

The report states that the proposed development will be serviced with:

- Zurn area drains located within the site to convey runoff from building rooftops, private street, and landscaped areas to the underground storage tank.
- Approximately 15m of storm sewers within the site to convey controlled runoff from the underground storage tank to the existing 375mm storm sewer within Baxter Road.
- Approximately 194m of storm sewer to collect and convey uncontrolled runoff from the public park to the existing 375mm storm sewer within Baxter Road.

The sanitary servicing for the Subject Site is expected to be through connections to the sanitary sewer within Baxter Road, which will outlet to the Pinecrest Creek Collector Sewer. The report concludes that there is adequate servicing to support the proposed development. More details can be found in the Servicing and Stormwater Management Report included with the submission.

### **Transportation Impact Assessment**

A Transportation Impact Assessment was prepared by Novatech, dated December 22, 2025. The report concludes that *“the proposed conceptual development is recommended from a transportation perspective. All building footprints and unit statistics will be confirmed via separate Site Plan Control and Zoning By-Law Amendment applications.”* More details can be found in the Transportation Impact Assessment that is included with the submission package.

## **5.0 PUBLIC CONSULTATION STRATEGY**

### **Prior to Submission**

A pre-consultation meeting was held with City staff on February 20, 2025.

### **Upon Submission**

The public will be consulted regarding the proposed Plan of Subdivision through the legislated public consultation requirements. This includes a sign posted on the site and the posting of the application on the City’s ‘DevApps’ website. At this time, neighbours will have the opportunity to comment on the proposal.

### **Community Heads Up**

A “Community Heads Up” notice will be circulated to the local Community Association, where applicable. The notice would be completed by the City of Ottawa during the application review process.

### **Delegated Authority Review Process**

Under City procedures, once the complete application submission is “deemed complete” by the City, it would be sent out on circulation under the Delegated Authority review process. Subject to resolving technical comments from the City and review agencies, the City would then prepare a set of draft plan conditions for review. Following review and comment by the applicant, the City would send a copy of the final draft plan conditions to the client and ward Councillor for formal concurrence. Unless the ward Councillor specifically requests that the matter go before Planning and Housing Committee, following concurrence with the Draft Plan conditions, a “Delegated Authority” approval report would be prepared for review and signature by the Director of Planning.

Following a decision by the Director of Planning, the City would issue a “notice of approval”. The issuance of Draft Plan of subdivision conditions would then be subject to a 20-day appeal period under the Planning Act. Once the appeal period has passed and if no appeals are filed, Draft Plan Approval is issued

## 6.0 CONCLUSION

This Planning Rationale has been prepared in support of a Plan of Subdivision application to facilitate the future development of a mixed-use subdivision on the property municipally known as 1101 Baxter Road (the "Subject Site"). The Subdivision will create six blocks to facilitate a future mixed-use development on the property. The residential component will consist of approximately 1,400 dwelling units, along with retail and commercial uses. A park block would be accessed off Baxter Road via a private street. Specific land use permissions will be determined through Site Plan and Rezoning applications for each proposed block.

The Subject Site is designated Hub in the Inner Urban Transect of the City of Ottawa Official Plan (2022). The Subject Site is designated Hub, located within the Iris-Baxter Sector of the Pinecrest and Queensview Stations Secondary Plan. The Subject Site is zoned Mixed Use Centre, Subzone 20, Schedule 508 (MC20 S508) in the City of Ottawa Zoning By-law 2008-250.

The draft City of Ottawa Zoning By-law is anticipated to be in-force by mid-2026. The final draft Zoning By-law zones the Subject Site Hub Zone 2, Subzone E, Schedule 508 (H2E S508), consistent with policies of the applicable Secondary Plan.

The proposed development is appropriate to support the growth and development of a settlement area within the City of Ottawa located near rapid transit. The proposed development will provide additional residential units within the urban area and will contribute to a greater diversity of dwelling types in the neighbourhood. The proposed subdivision will include a mix of uses on each proposed block which will support the proposed residential uses and contribute to creating a complete community. The proposed subdivision will support active transportation by providing sidewalks, a pedestrian pathway and cycling connections to Baxter Road and Iris Street. The proposed subdivision is consistent with the policies of the Provincial Planning Statement.

The proposed plan of subdivision conforms to the policies of the City of Ottawa Official Plan (2022). The proposed subdivision supports the City's Growth Management Framework by contributing approximately 1,400 dwelling units to the urban area. The proposed subdivision will include a range of mid to high-rise, mixed-use buildings that will provide for a greater range of transit-supportive land uses and increased housing options for residents while fitting into the neighbourhood context. A Rezoning application to address building height and separate Site Plan applications are required at a later date to facilitate development of each newly created block.

The proposed Plan of Subdivision application is appropriate for the development of the Subject Site and represents good land use planning.

Yours truly,

**NOVATECH**



Jeffrey Kelly, MCIP, RPP  
Project Manager | Planning & Development

**Appendix A:**

**DRAFT PLAN OF SUBDIVISION**

