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# PLANNING JUSTIFICATION REPORT

MINOR ZONING BY-LAW AMENDMENT &  
SITE PLAN CONTROL APPLICATIONS  
SUBJECT SITE: 71 RUSSELL AVENUE



LAWRENCE  
ARCHITECTS

REPORT DATE: FEBRUARY 9, 2026  
PREPARED FOR: JERSEY DEVELOPMENTS INC.  
PREPARED BY: Q9 PLANNING + DESIGN INC.

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This Zoning Confirmation Report is prepared in support of a Minor Zoning By-law Amendment & Site Plan Control Application for the proposed rezoning of the site at 71 Russell Ave.

Q9 Planning + Design have been retained by Jersey Developments Inc. to prepare a Planning Rationale regarding the minor Zoning By-law Amendment & Site Plan Control applications in order to construct a new low-rise apartment building on the subject site at 71 Russell Avenue.

The following represents the Planning Rationale required as part of the submission requirements for a minor Zoning By-law Amendment & Site Plan Control applications.

## 1.0 EXECUTIVE SUMMARY

The subject property at 71 Russell Avenue is designated *Neighbourhood* within the Downtown Core Transect in the new Official Plan. The property is located within the Central and East Downtown Core Secondary Plan. The property is zoned R4UD – Residential Fourth Density Zone, Subzone UD, in the City of Ottawa Zoning By-law 2008-250.

The property is located within the Sandy Hill neighbourhood in Ward 12 – Rideau-Vanier. Sandy Hill is a historic neighbourhood just east of Ottawa’s downtown core, roughly 1.5 km from Parliament Hill. The neighbourhood is located in Ottawa’s central and downtown core and the property is bordered by Osgoode Street to the north, Somerset St. East to the south, the Russell Avenue to the west and the Chapel Street to the east. Its central location gives Sandy Hill both excellent transit access and proximity to commercial, civic and cultural designations.

The proposed development is construct a 4-storey, 28-unit low-rise apartment dwelling. The proposed development proposes 28 bicycle parking spaces, zero vehicular parking spaces, a 1.8 m walkway from the sidewalk directly to the front door. The property is currently vacant.

The proposed development requires minor relief from the Zoning By-law as detailed below. As this report concludes, the proposed minor rezoning is consistent with Provincial Planning policy, the Official Plan and is considered good land use planning.

### Minor Zoning By-law Amendment

The minor amendments to the Zoning By-law are identified below:

Low-rise Apartment Dwelling (9+ units) (71 Russell Avenue)

- To permit a low-rise apartment dwelling with a rear yard setback of 7.52 m whereas 11.28 m is required (S.144, Table 144A)
- To permit a low-rise apartment dwelling with a rear yard area of 151.6 m<sup>2</sup> whereas 226.9 m<sup>2</sup> is required (S.144, 3(a))
- To permit a minimum front façade recess of 0.61 m for 13.4% and 0.56 m for 29.9% Section 161(15)(h)

- To permit 0 resident and visitor parking spaces whereas 8 resident and 2 visitor parking spaces are required. (Section 110 and 102)

## 2.0 SITE & CONTEXT

### 2.1 Site

The subject site is a rectangular interior lot located along the east side of Russell Avenue in Sandy Hill. The property is currently developed with an existing 1-storey dwelling along with the existing 2 accessory structures in the rear.

- [ Lot frontage: 20.12 m
- [ Lot depth: 37.59 m
- [ Lot area: 756.32 m<sup>2</sup>
- [ Legal Description: Lot 14, Plan 58319, PIN 042080265

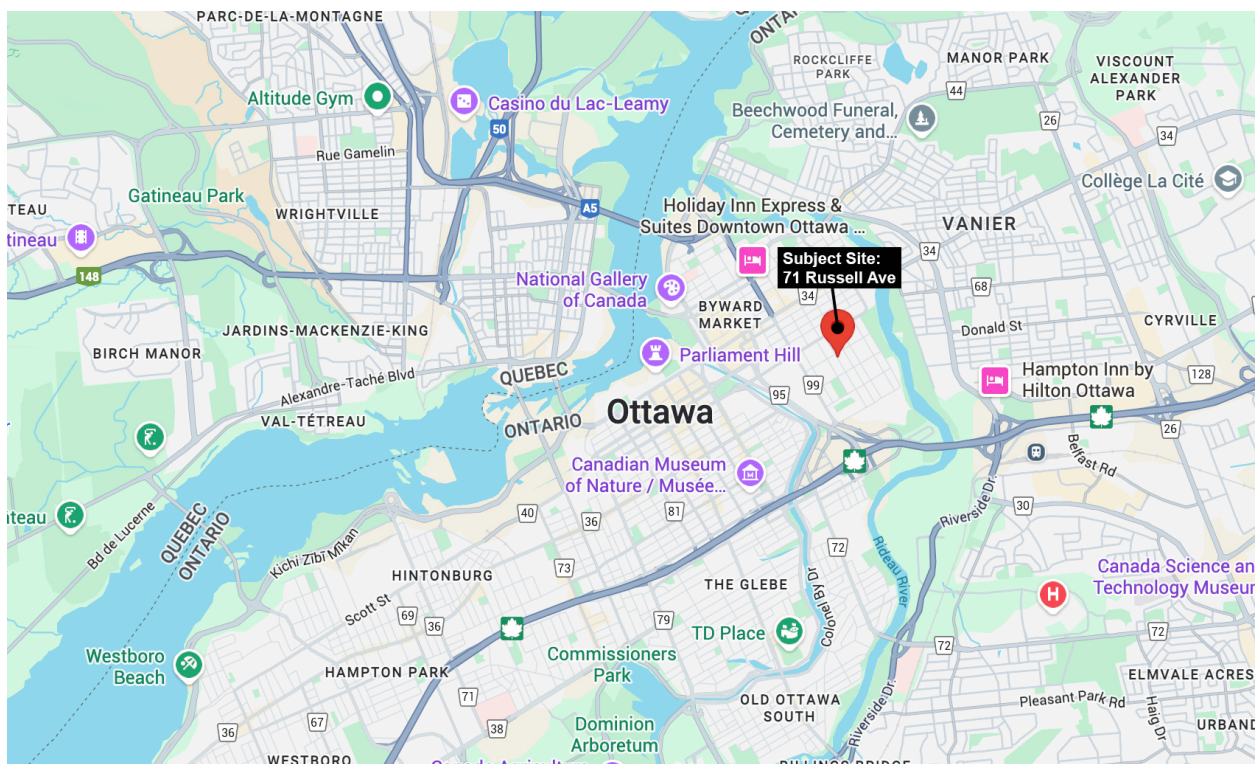


Figure 1: Location Map (Source: Google Maps)

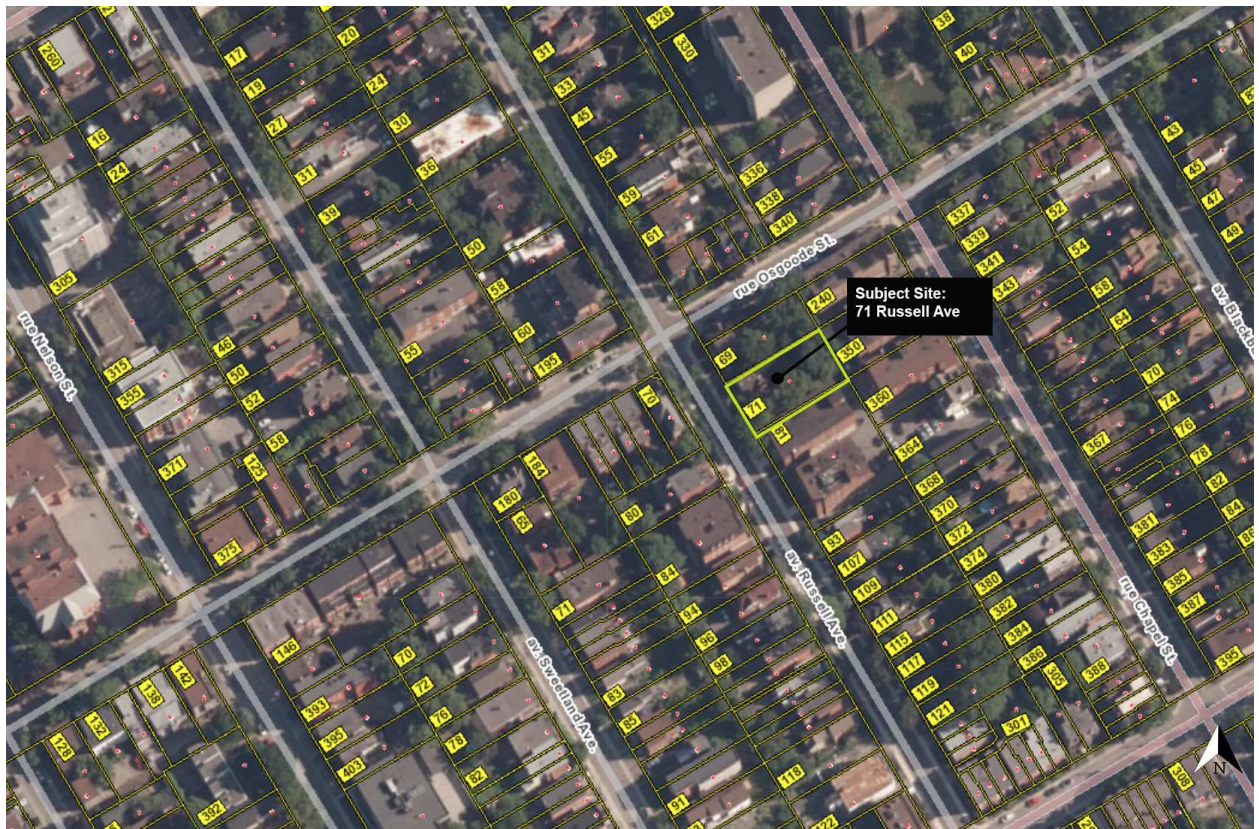


Figure 2: Site Map (Source: GeoOttawa)

## 2.2 Context

The property is located within the Sandy Hill neighbourhood in Ward 12 – Rideau-Vanier. Sandy Hill is a historic neighbourhood just east of Ottawa’s downtown core, roughly 1.5 km from Parliament Hill. The neighbourhood is located in Ottawa’s central and downtown core and the property is bordered by Osgoode Street to the north, Somerset St. East to the south, the Russell Avenue to the west and the Chapel Street to the east. Its central location gives Sandy Hill both excellent transit access and proximity to commercial, civic and cultural designations. Lindenlea-New Edinburgh and Lowertown, Rockcliffe Park and Manor Park are located to the north of Sandy Hill with Overbrook-McArthur to the east.

The neighbourhood is characterized by mostly residential development (in many forms, low-rise single detached, duplex, semis, as well as taller apartment dwellings). The housing typologies are mainly mid-20th-century housing, with some older homes retained. The residential uses are mainly located along local roads, with the commercial and mixed uses concentrated along Laurier Avenue. The subject site is located approximately 200 m east of the University of Ottawa campus which contains many institutional buildings.

The site is south of the Laurier Avenue East commercial mainstreet and east of the University of Ottawa Campus. There also exist other institutional or public uses in the surrounding area including Churches, fire stations and schools. The University of Ottawa campus lines Sandy Hill’s western boundary (along the Rideau Canal), drawing heavy student presence and influencing local land use patterns. Strathcona Park lies along the eastern edge by the Rideau River. It includes recreational facilities, historic playgrounds, and open-air theatre through Odyssey Theatre. The area adjacent to Strathcona Park, especially along Range Road and Laurier Avenue, is home to numerous embassies and dignitaries’ residences.

## 2.2 PEDESTRIAN AND TRANSIT NETWORK

Given that the site is located adjacent to the commercial main streets of Laurier Avenue East and King Edward Avenue, the site is well served by commercial uses that are within walking distance. The local streets are organized in a modified grid format. Sidewalks are provided on both sides of Russell Avenue and Osgoode Street to the north. Sidewalks are continuous along Russell, Somerset, Laurier, and Rideau. These streets intersect many shops, transit, and green spaces. The walkable grid connects to green spaces like Sandy Hill Park, St Germain Park, Rideau Canal Pathway, as well as Major’s Hill Park via ByWard Market.

The site is within an approximate 5-minute walk to the nearest community centre, is a 6-to-12-minute walk from various schools, including the University of Ottawa and is within a 20 minute walking distance to the nearest Ottawa Public Library branch.

The proposed development is well-located near public transportation service, with immediate access to OC Transpo routes 5 & 11. The site is within close walking distance to transit stops located along Laurier Avenue. Overall, the site has strong public transportation access, with a variety of frequent service routes running within walking distance to the site.

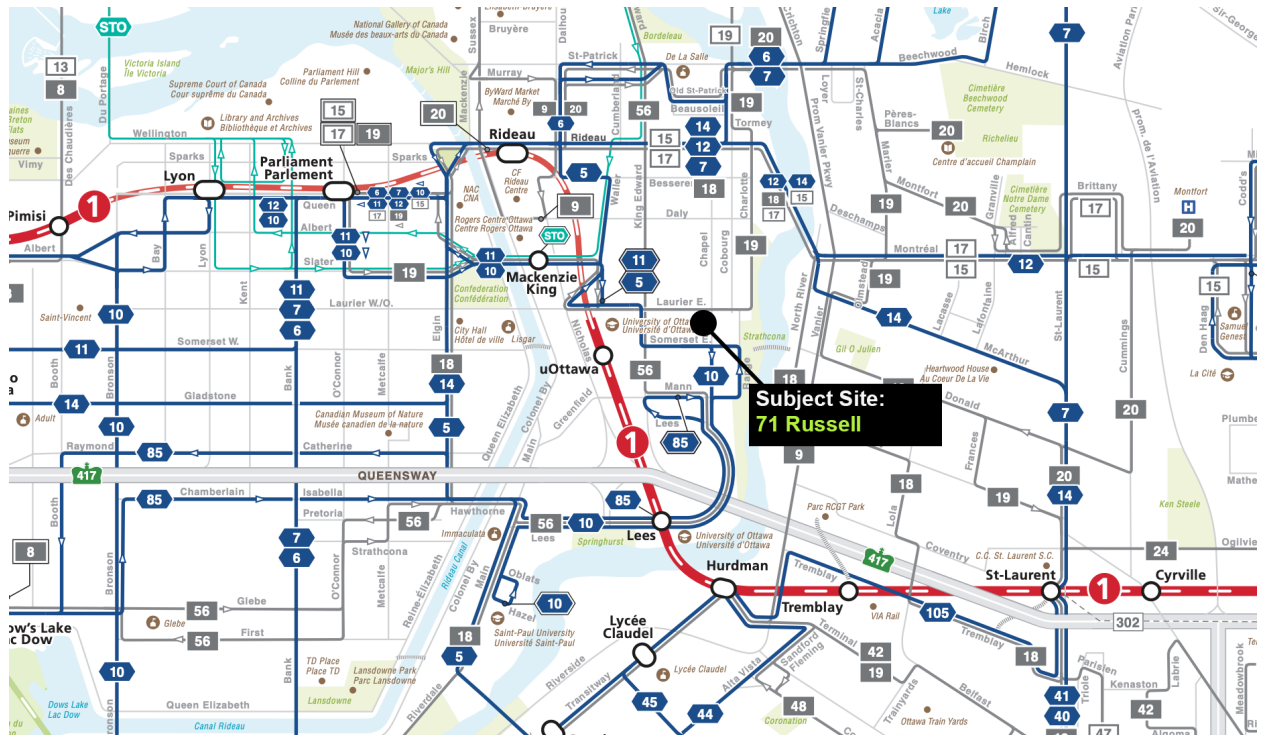


Figure 3: Transit Map (Source: OC Transpo)

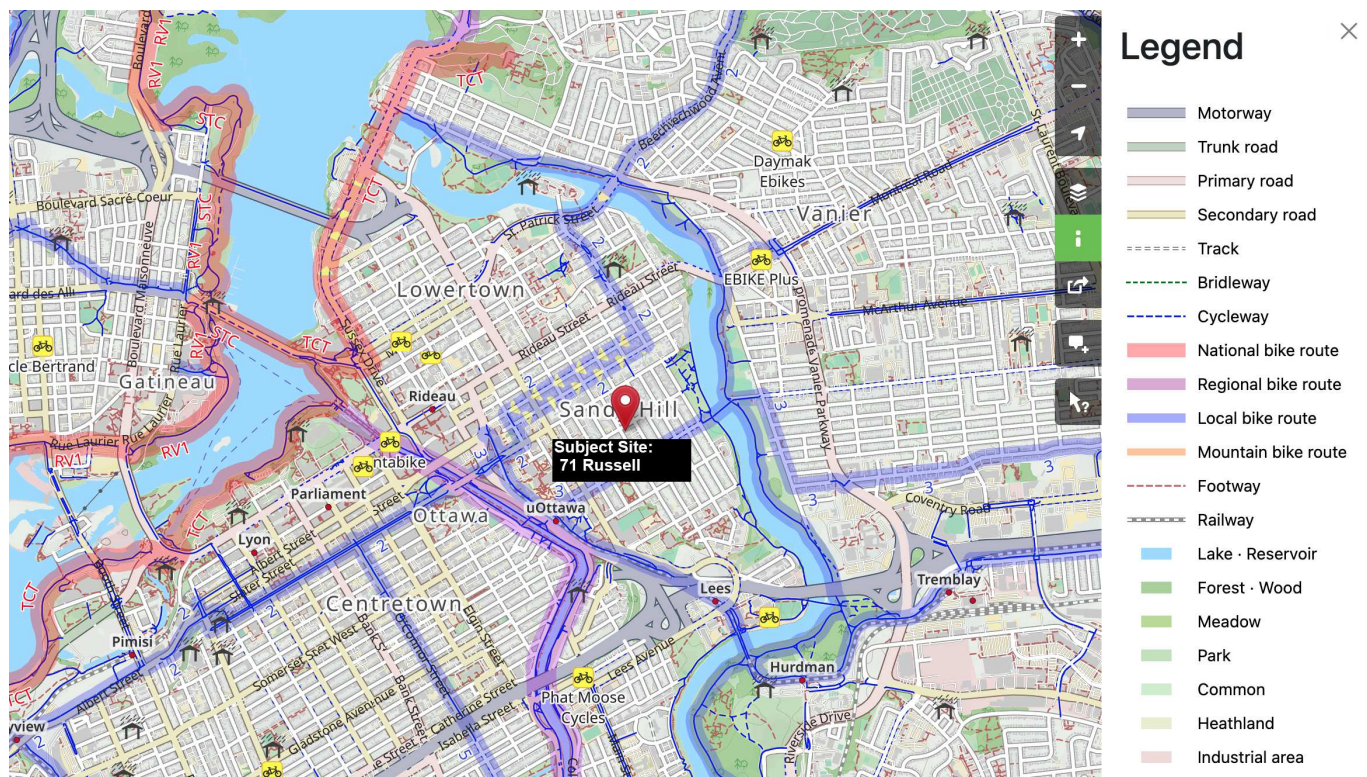


Figure 4: Active Transportation Map (Source: Open Streets Map)

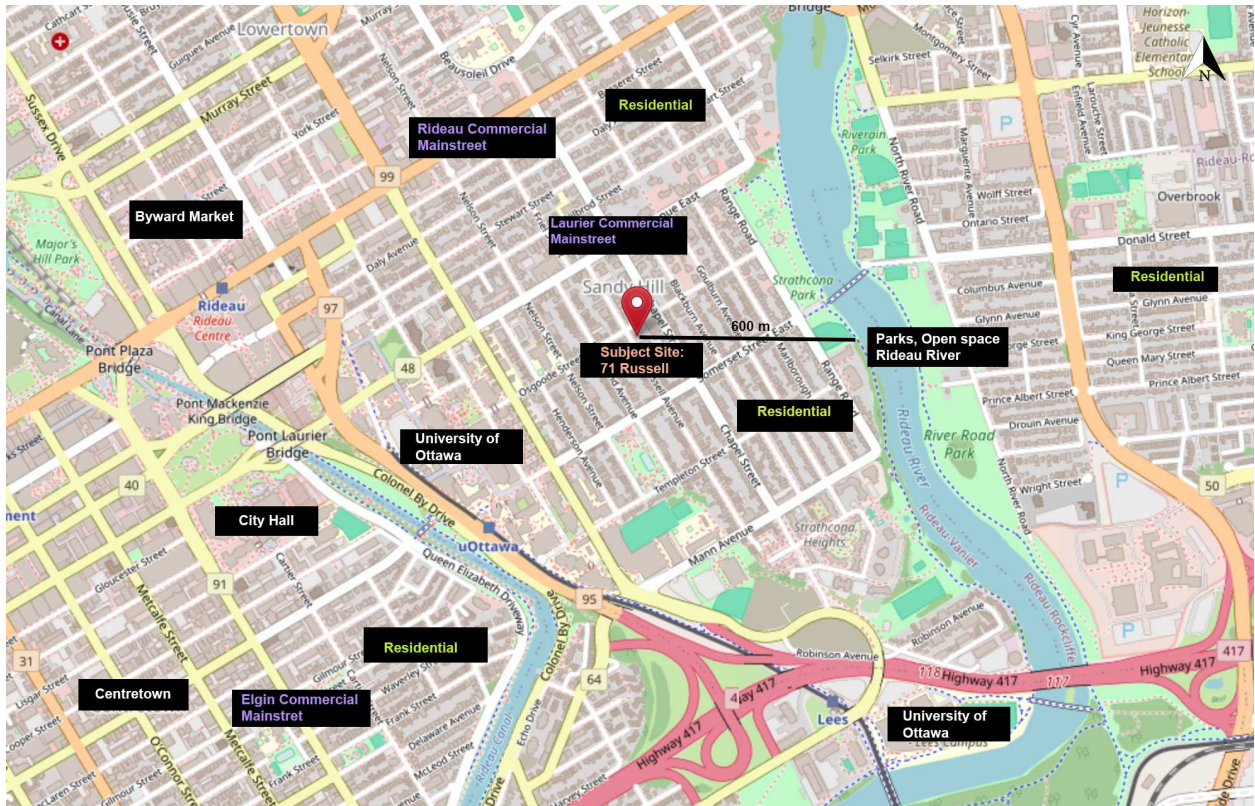


Figure 5: Context Map (Source: Open Streets Map)

### 3.0 PROPOSED DEVELOPMENT

The proposed development is to construct a 4-storey, 28-unit low-rise apartment dwelling. The proposed development proposes 28 bicycle parking spaces, zero vehicular parking spaces, a 1.8 m walkway from the sidewalk directly to the front door.

The built form features a 4.5 m front yard setback, a 1.5 m south interior yard setback, a 1.5 m north interior yard setback, and a 7.5 m rear yard setback. The entirety of the rear yard is soft landscaped which will provide an area greater than 25 m<sup>2</sup> for tree planting.



Figure 6: Rendering of Proposed Building (Source: Lawrence Architects)



*Figure 7: Proposed Development (Source: Lawrence Architects)*



1 FRONT PERSPECTIVE 1  
A4.3 SCALE



3 FRONT PERSPECTIVE 2  
A4.3 SCALE



2 REAR PERSPECTIVE 1  
A4.3 SCALE



4 REAR PERSPECTIVE 2  
A4.3 SCALE

Figure 8: 3D Elevation Renderings (Source: Lawrence Architects)

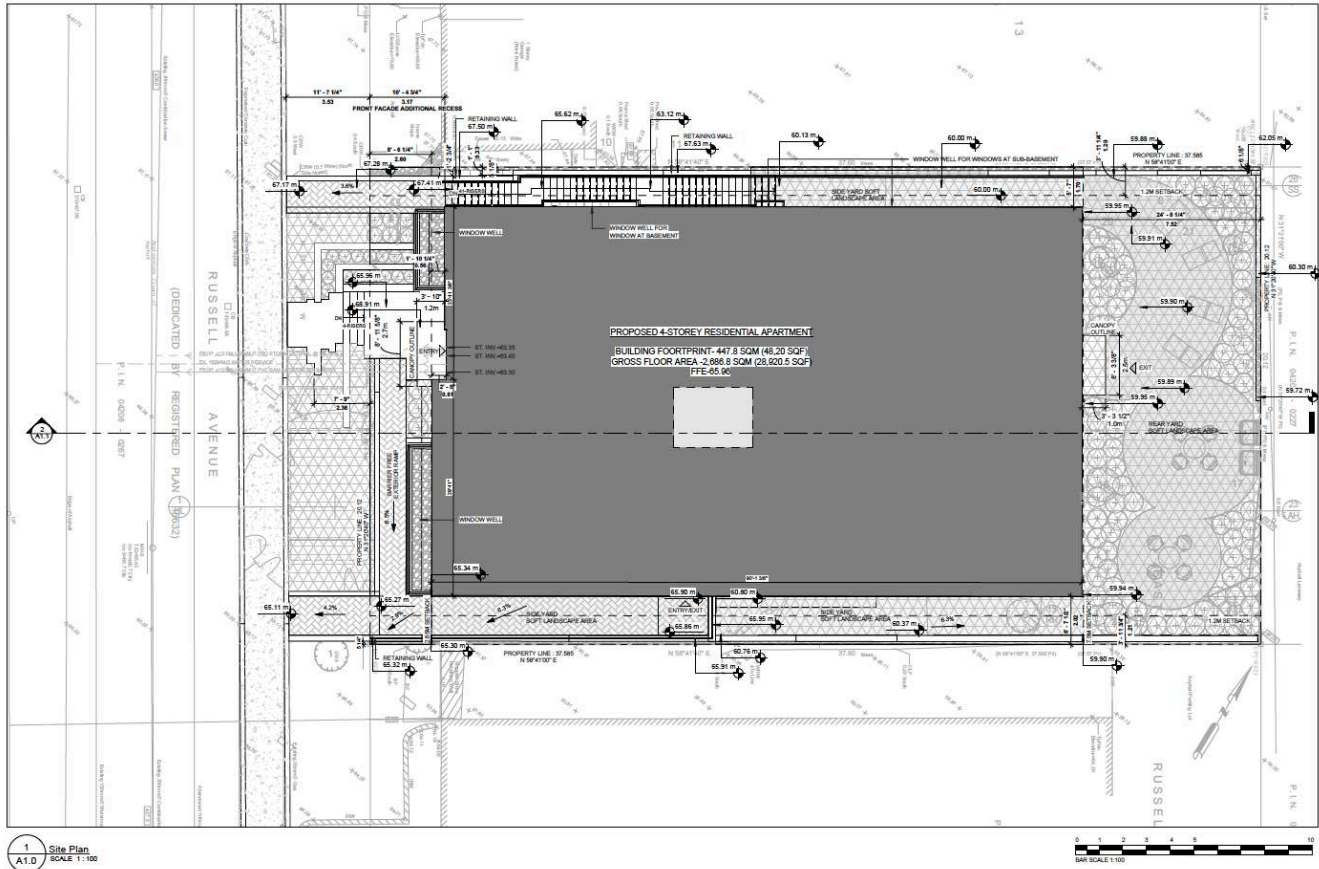


Figure 9: Site Plan (Source: Lawrence Architects)

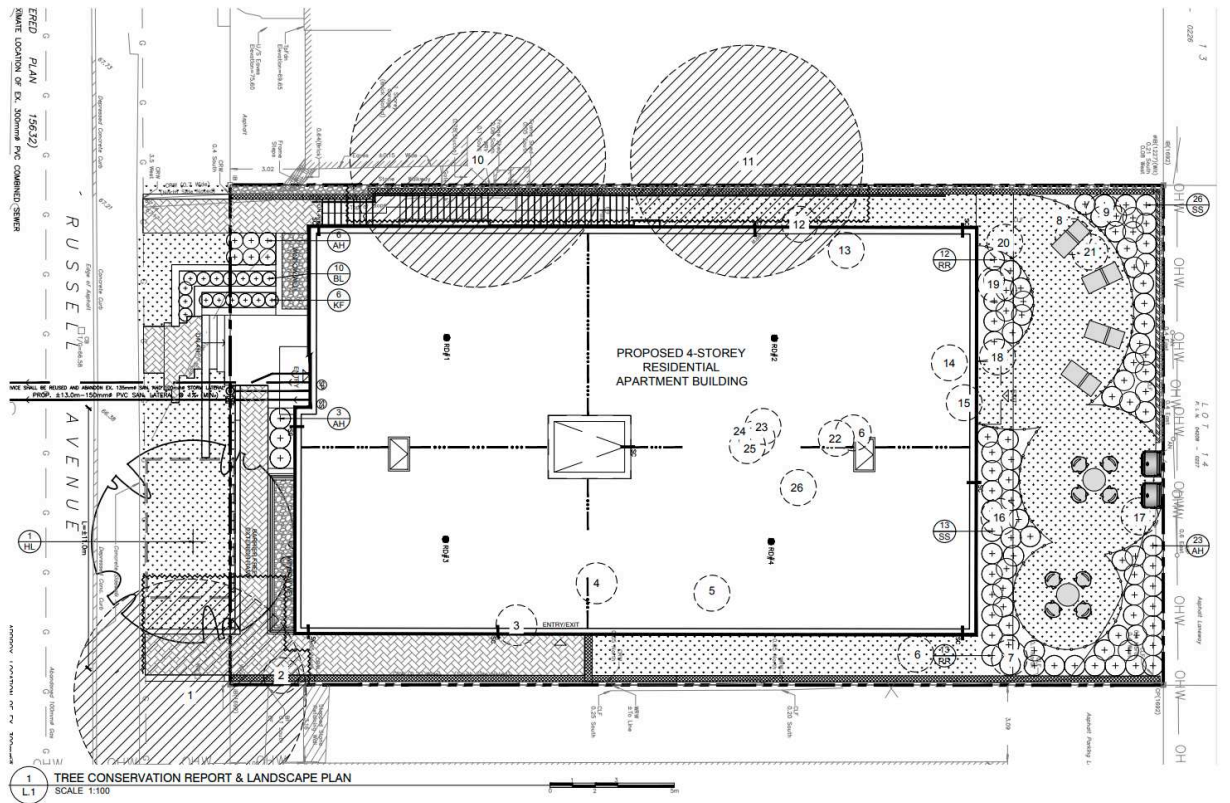


Figure 10: Except from Landscape Plan (Source: Lawrence Architects)

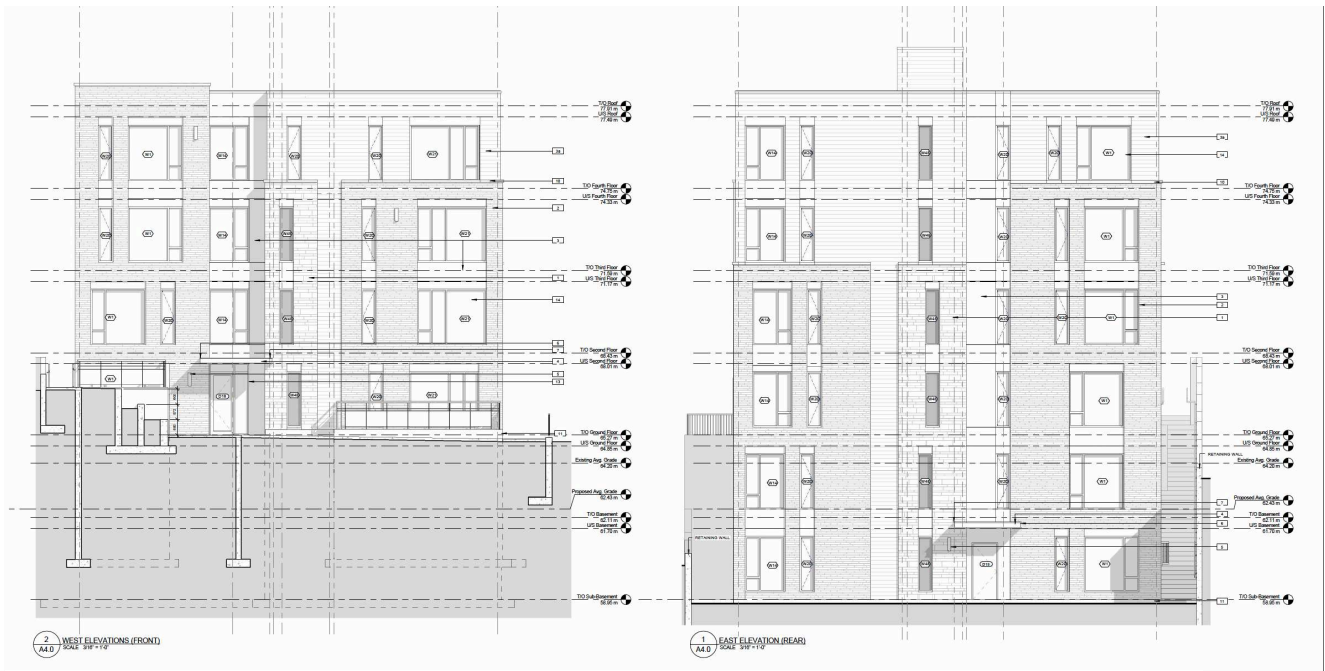


Figure 11: West & East 2D Elevations (Source: Lawrence Architects)

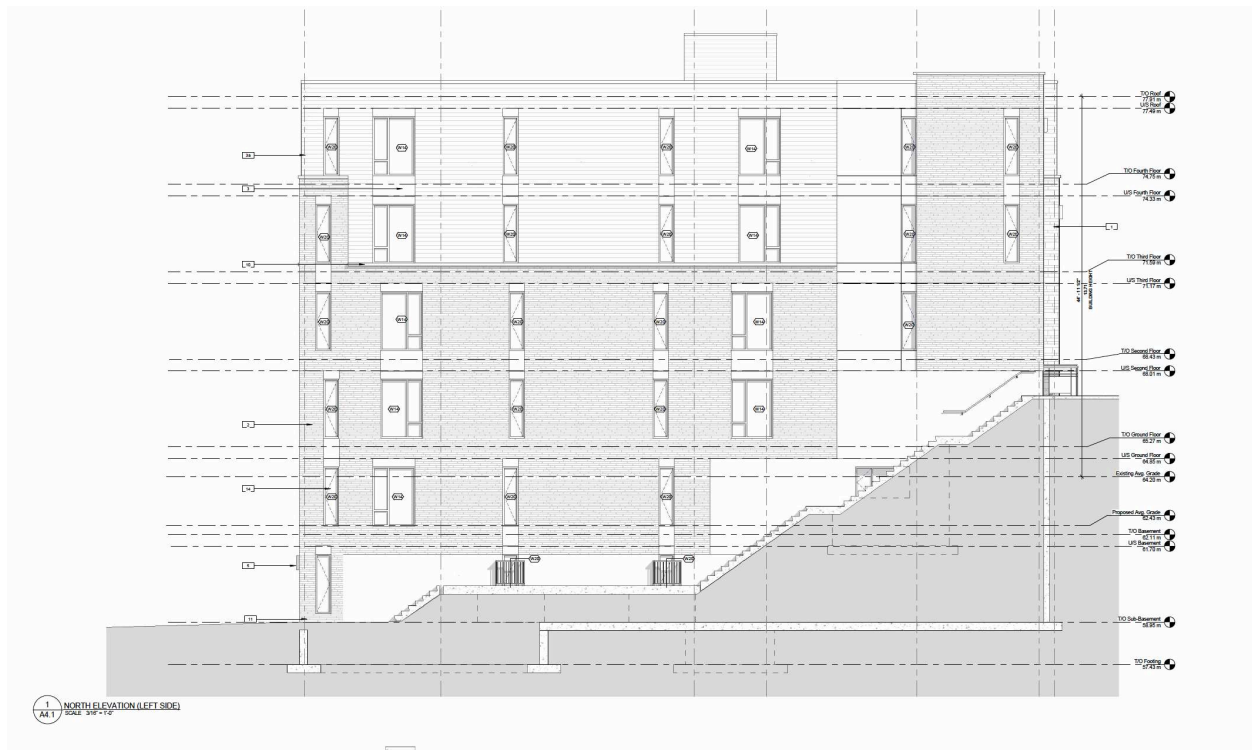
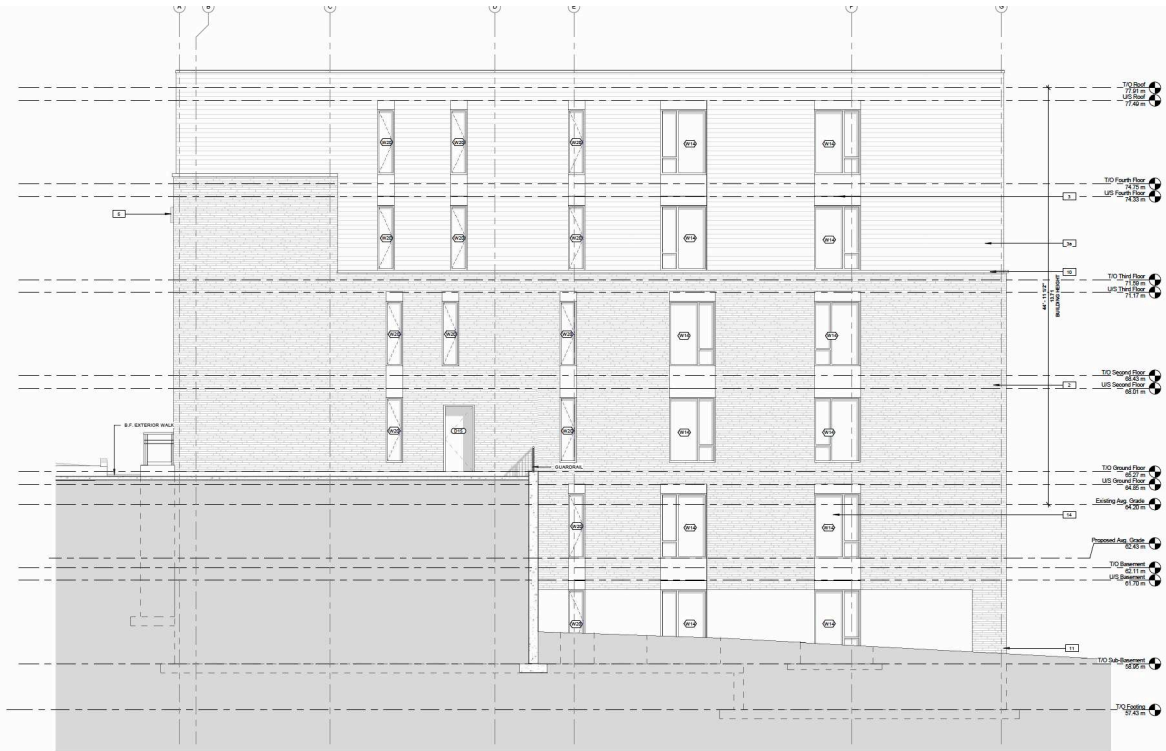


Figure 12: North 2D Elevations (Source: Lawrence Architects)



1 SOUTH ELEVATION (RIGHT SIDE)  
A1.2 SCALE: 3/8" = 1'-0"

Figure 13: South 2D Elevations (Source: Lawrence Architects)

## 4.0 POLICY REVIEW

### 4.1 PROVINCIAL PLANNING STATEMENT, 2024

The Provincial Planning Statement, 2024 (PPS) came into effect on October 20, 2024, and merges the previous “A Place to Grow: Growth Plan for the Greater Golden Horseshoe” and the “PPS (2020)”. It provides broad policy direction on land use planning and development, emphasizing intensification to reach a target of 1.5 million homes by 2031.

These policies must be integrated with other provincial and municipal plans, including local Official Plans and Secondary Plans, and all planning decisions must be consistent with the PPS. Relevant policies from the PPS are outlined below, with the specific policies provided in italics. Section 2.0 provides policies to ensure that planning authorities prepare for long-term growth by using provincial forecasts, maintaining adequate land for residential and other uses, and incorporating any additional growth from zoning orders into future plans. It emphasizes the creation of complete, accessible, and equitable communities through a diverse mix of land uses.

#### Section 2.1 - Planning for People and Homes

*2.1.6 Planning authorities should support the achievement of complete communities by:*

- a. accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses (including schools and associated child care facilities, long-term care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs;*
- b. improving accessibility for people of all ages and abilities by addressing land use barriers which restrict their full participation in society; and*
- c. improving social equity and overall quality of life for people of all ages, abilities, and incomes, including equity-deserving groups.*

#### Section 2.2 - Housing

*Policy 2.2.1. Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by:*

- a. establishing and implementing minimum targets for the provision of housing that is affordable to low and moderate-income households, and coordinating land use planning and planning for housing with Service Managers to address the full range of housing options including affordable housing needs;*
- b. permitting and facilitating:*
  - 1. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including additional needs housing and needs arising from demographic changes and employment opportunities; and*
  - 2. all types of residential intensification, including the development and redevelopment of underutilized commercial and institutional sites (e.g., shopping malls and plazas) for residential use, development and introduction of new housing options within*

*previously developed areas, and redevelopment, which results in a net increase in residential units in accordance with policy 2.3.1.3;*

- c. promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation; and*
- d. requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations.*

**Comment** | The proposed addition of twenty-eight (28) units in a 4-storey, low rise apartment for will provide critical housing rental units to the area by effectively utilizing the available lot, resulting in a more efficient lot that maintains the prevailing low-rise context of the area and supports transit-supportive development and active transportation development through the provision of 28 bicycle parking spaces and 0 vehicle parking spaces.

### Section 2.3 - Settlement Areas and Settlement Area Boundary

Section 2.3 directs growth in Ontario's settlement area, particularly near strategic growth areas and major transit stations. It states that planning authorities shall establish minimum intensification and redevelopment targets to create complete communities within designated growth areas to ensure orderly development and sufficient infrastructure provision.

2.3.1.1. Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.

2.3.1.2 Land use patterns within settlement areas should be based on densities and a mix of land uses which:

- a. efficiently use land and resources;*
- b. optimize existing and planned infrastructure and public service facilities;*
- c. support active transportation;*
- d. are transit-supportive, as appropriate; and*
- e. are freight-supportive.*

2.3.2.1 States that planning authorities shall consider the following for new settlement areas and boundary expansions:

- a. the need to designate and plan for additional land to accommodate an appropriate range and mix of land uses;*
- b. if there is sufficient capacity in existing or planned infrastructure and public service facilities;*
- c. whether the applicable lands comprise specialty crop areas;*
- d. the evaluation of alternative locations which avoid prime agricultural areas and, where avoidance is not possible, consider reasonable alternatives on lower priority agricultural lands in prime agricultural areas;*
- e. whether the new or expanded settlement area complies with the minimum distance separation formulae;*

- f. *whether impacts on the agricultural system are avoided, or where avoidance is not possible, minimized and mitigated to the extent feasible as determined through an agricultural impact assessment or equivalent analysis, based on provincial guidance; and*
- g. *the new or expanded settlement area provides for the phased progression of urban development.*

**Comment** | The subject site is located within the urban boundary, within walking distance to transit stops, supports intensification, and is an efficient use of land and infrastructure. By developing within an existing serviced area, in an area identified for intensification and growth, the proposal supports optimal use of municipal infrastructure and public transportation services. The proposed development represents a contextual form of intensification, maintaining a typical and appropriate interior yard setbacks, four storeys in height comparable to the low-rise apartments in the neighbourhood and is a suitable location for intensification and new housing as there is appropriate levels of infrastructure and where active transportation and transit can be supported. The rental apartment building will contribute to the rental units available in the area, help achieve housing targets, and support existing transit and infrastructure investments in the area.

Section 4.0 of the PPS provides policies aimed at protecting Ontario's natural heritage, water, agricultural, mineral, cultural heritage, and archeological resources in order to preserve the province's long-term prosperity, environmental health, and social wellbeing.

Section 5.0 of the PPS contains policies to protect the health and safety of Ontarians, reducing risk from natural and human-made hazards by directing development away from hazard areas.

**Based on our review, it is our professional planning opinion that the proposed development is consistent with the policies of the Provincial Planning Statement (PPS), 2024.**

## 4.2 City of Ottawa Official Plan

**Designation:** *Neighbourhood, Downtown Core Transect*

The City of Ottawa Official Plan was adopted by City Council on November 24th, 2021 was approved by the MMAH on November 4th, 2022. The Plan is intended to manage growth and change in Ottawa to the year 2046.

Section 2 contains the overall strategic direction of the new Official Plan and is based around the Five Big Policy Moves, which are intended to make Ottawa the most liveable mid-sized City in North America. The Five Big Moves call for increased growth through intensification, sustainable transportation, context-based urban and community design, environmental, climate, and health resiliency embedded into planning policy, and planning policies based on economic development. Six cross-cutting issues have also been identified as essential to the achievement of liveable cities, which are related to

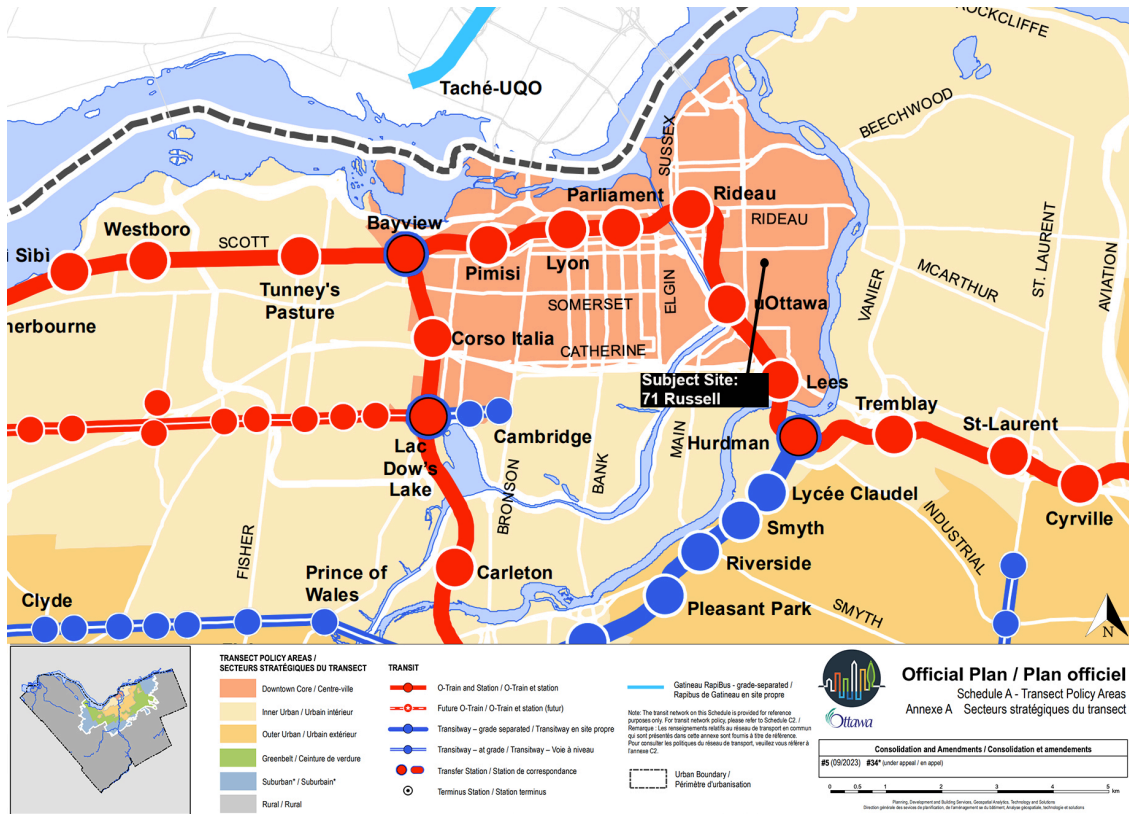


Figure 14: City of Ottawa Official Plan, Schedule A

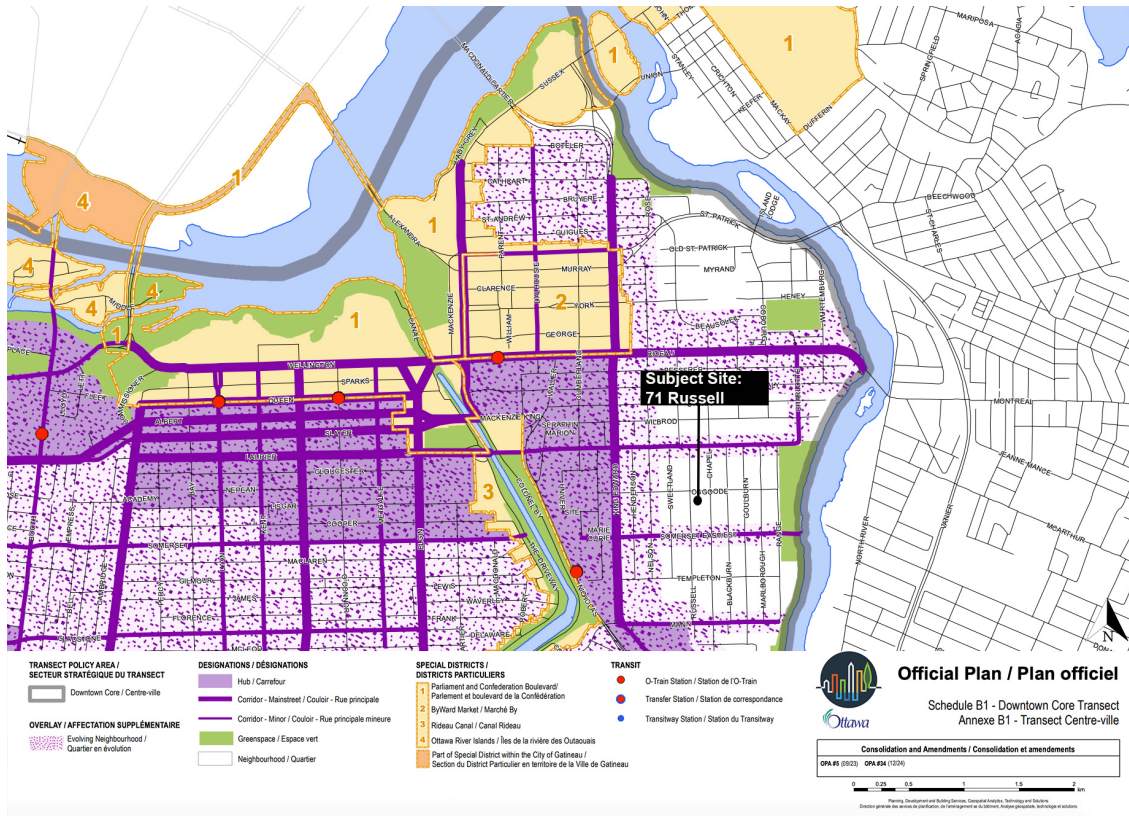


Figure 15: City of Ottawa Official Plan, Schedule B1

intensification, economic development, energy and climate change, healthy and inclusive communities, gender equity, and culture.

**Comment:** The proposed development results in an increase of 28 dwelling units through intensification of an existing site, contributing towards intensification that is planned for areas within proximity to transit and within proximity of Mainstreet Corridors.

Section 3 provides a growth management framework that directs various types of growth appropriately in order to accommodate projected population and employment growth. Most growth is anticipated to occur in the urban area, which is the built-up area where existing development is located and the greenfield area of vacant lands at the periphery of the urban area. Within the urban area, there are six different transect areas representing different contexts. Growth is expected to be accommodated in these transect policy areas in a manner that supports 15-minute communities. The balance of growth is to occur in the rural area. In order to accommodate anticipated population and job growth, an increasing amount of growth is to be absorbed through intensification.

Section 3.2 provides specific goals for the overall amount of growth in the urban area to be accommodated through intensification. The plan allocates 47% of growth to occur within the built-up portion and 46% of growth within the greenfield portions of the urban area. Growth through intensification is expected to support the creation of 15-minute neighbourhoods. Intensification may also occur through various built form and height profiles, based around Hubs, Corridors, and Neighbourhoods and housing choice with a variety of dwelling types and sizes are planned.

**Comment:** The proposed development is located within the urban area and is designated as '*Neighbourhood*' within the Downtown Transect, which plans for the area to redevelop as a 15-minute neighbourhood at a density that supports additional public and active transportation use. The proposed development of a new 28-unit, 4-storey low-rise apartment through infill of an existing lot contributes to the strategic direction of accommodating new rental housing units through intensification.

Section 4 of the Official Plan provides land use and transportation policies that apply to the entire city.

Section 4.1.2 plans for the promotion of healthy, 15-minute neighbourhoods, with safe walkable pedestrian networks and access to active and public transportation infrastructure.

Section 4.1.4 provides policies aimed at facilitating the shift towards sustainable modes of transportation, including through managing the supply of parking close to transit stations and regulating the design and location of parking garage entrances and surface parking areas.

Section 4.2 provides policies aimed at providing adequate, safe, and affordable housing by enabling a greater supply and diversity of housing options throughout the City. Housing should meet the needs of all ages, incomes, and backgrounds to create healthier communities.

Section 4.2.1 enables more flexibility to provide an adequate supply and diversity of housing options throughout the city, which shall primarily be provided through the zoning by-law. It provides for a variation in densities, building types, and tenure types, including the provision of more rental units.

**Comment:** The proposed development provides for compatible infill by creating additional density through the construction of a low-rise apartment. The proposed four storey height is compatible with the area where two-storey heights are common, and three-storey heights already exist. It is noted that four-storey heights are permitted in the applicable designation. The building contributes quality design to the area and constitute compatible infill of the subject site. The site focuses on green space, and active modal access. The design integrates a combination of materials, glazing and articulation to provide interest facing the public realm.

Section 4.6.5 of the Official Plan provides for effective site planning that supports the objectives of the designations. Effective site planning includes providing appropriate setbacks, designs that minimize conflict between vehicles and pedestrians, and universal accessibility.

Section 4.6.6 includes policies aimed at integrating low-rise, mid-rise, and high-rise buildings to ensure that intensification targets are met while ensuring liveability. It provides for appropriate transitions in building heights and the use of angular planes to step-back the upper storeys of mid-rise and high-rise buildings.

Policy 6 states that low-rise buildings shall be designed to respond to context, and transect area policies, and shall include areas for soft landscaping, main entrances at-grade, front porches or balconies, where appropriate. Buildings shall integrate architecturally to complement the surrounding context.

Section 5 provides policy direction for the six identified transect policy areas. The subject site is located within the **Downtown Core Transect** policy area. Within the Downtown Core Area, the site is designated as **Neighbourhood**. Low-rise buildings at least two storeys in height and up to four storeys are generally permitted.

Section 5.1 provides policy direction for the Downtown Core Transect. The Official Plan states that the Downtown Core is the most significant focus area in which these objectives can be demonstrated. The Downtown Core is a mature built environment whose urban characteristics of high-density, mixed uses and sustainable transportation orientation are to be maintained and enhanced.

**Comment** | The proposed development supports the overall objectives of the Official Plan, which plans for an urban pattern of development, transit-supportive densities, and a strong mix of uses. The existing building addresses the street frontage, fits contextually with surrounding development and provides a positive pedestrian experience from all directions.

Section 5.1.2 states that the Downtown Core shall continue to develop as healthy 15-minute neighbourhoods within a highly mixed-use environment, where: a) Hubs and a dense network of Corridors provide a full range of services; b) A high concentration of

employment is maintained and increased; c) Existing and new cultural assets are supported, including those that support music and nightlife; and d) Residential densities are sufficient to support the full range of services noted in Policy a).

Section 5.1.5.1 provides direction to the Neighbourhoods located within the Downtown Core Transect.

*Neighbourhoods located in the Downtown Core shall accommodate residential growth to meet the Growth Management Framework as outlined in Subsection 3.2, Table 3b. The Zoning By-law shall implement the density thresholds in a manner which adheres to the following: a) Allows and supports a wide variety of housing types with a focus on missing-middle housing, which may include new housing types that are currently not contemplated in this Plan; b) The application, as appropriate, of Zoning By-law development standards to be applied as one lot for zoning purposes to support missing middle housing; c) Provides for a Low-rise built form, by requiring in Zoning a minimum built height of 2 storeys, generally permitting 3 storeys, and where appropriate, will allow a built height of up to 4 storeys to permit higher density Low-rise residential development; d) Building on Table 6, provides an emphasis on regulating the maximum built form envelope that frames the public right of way; and e) In appropriate locations, to support the production of missing middle housing, prohibit lower-density typologies.*

Section 6.0 contains policies specific to designations within the urban settlement area.

Section 6.3 contains policies that pertain to Neighbourhoods. These are contiguous urban areas that form the heart of communities and consist of a mix of densities and built forms. Neighbourhoods are noted as being at different types and stages of development, maturity, and evolution. A variety of dwelling types and densities are permitted in Neighbourhoods, with the intent of creating and reinforcing 15-minute communities through gradual, context-sensitive development. Permitted building heights are generally 2-4 storeys, which transition in height and density from the neighbourhood interior towards Corridors and Hubs.

**Comment:** The proposed development at 71 Russell Avenue is consistent with the intent and direction of Sections 5.1.2, 5.1.5.1 and 6.3 of the City of Ottawa Official Plan. The subject site is located within the Downtown Core Transect and designated Neighbourhood, an area identified to accommodate residential growth through sensitive intensification that supports the creation of healthy 15-minute neighbourhoods. The proposal contributes to a complete, mixed-use downtown environment by introducing 28 new rental dwelling units within walking distance of commercial services, institutional uses, cultural amenities, parks, and frequent transit routes. Its central location near Laurier Avenue East, the University of Ottawa campus, and nearby mainstreets ensures residents have convenient access to daily needs, employment opportunities, and cultural and social assets, thereby reinforcing the objectives of a 15-minute neighbourhood as set out in Section 5.1.2.

In accordance with Section 5.1.5.1, the proposed development represents an appropriate form of low-rise residential intensification within a Downtown Core Neighbourhood. The four-storey low-rise apartment building provides a missing-middle housing form that is specifically encouraged by the Official Plan and contributes to meeting the Growth Management Framework targets through redevelopment of an under-utilized lot. The built form falls within the generally permitted 2–4 storey height range, supports increased

residential density, and maintains a scale and massing that is compatible with the surrounding context.

The proposal also reflects the intent of Section 6.3 by accommodating gradual, context-sensitive change within an established neighbourhood. The transition from a single detached dwelling to a low-rise apartment building represents an evolution of the area that maintains neighbourhood character while providing a greater diversity of housing options. The building’s massing, setbacks, and pedestrian-oriented design frame the public right-of-way appropriately and contribute positively to the streetscape, while the absence of vehicular parking and the provision of generous bicycle parking reinforce sustainable transportation objectives.

Overall, the proposed development supports residential densities sufficient to sustain nearby services and transit, advances the provision of missing-middle housing, and aligns with the Official Plan’s vision for healthy, complete, and evolving neighbourhoods within the Downtown Core.

**Based on our review, it is our professional planning opinion that the proposed development conforms with the City of Ottawa Official Plan.**

### 4.3 Central & East Downtown Core Secondary Plan

**Designation:** Local Neighbourhood Designation, Sandy Hill Character Area

The subject site is located within the Sandy Hill Character Area and is designated Local Neighbourhood. As per Schedule C - Maximum Building Height, the maximum permitted building height is 4 storeys.

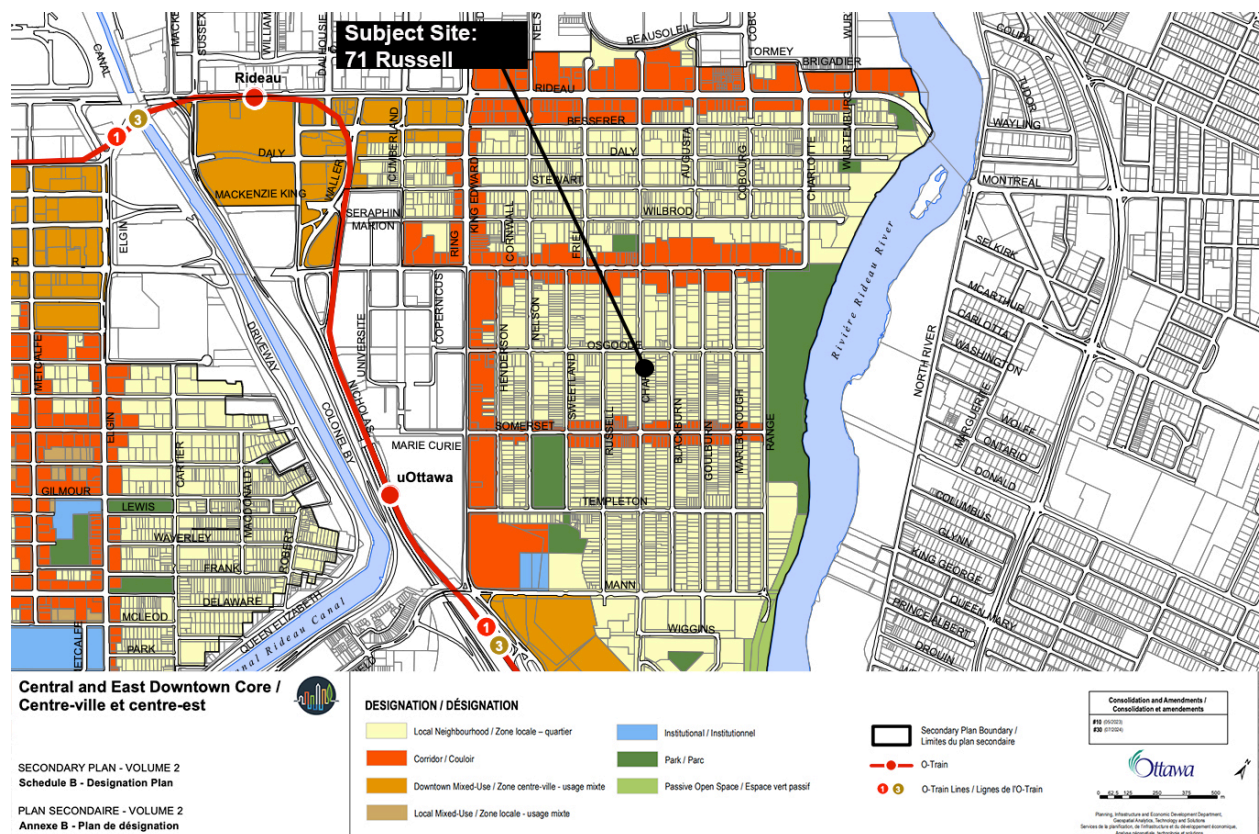


Figure 16: Schedule B, Central and East Downtown Core, City of Ottawa

The Central and East Downtown Core Secondary Plan, Centretown Character Area in Policy 4.7.2 outlines core principles that provide a vision for Sandy Hill and the foundation for this Character Area. They establish overarching goals for Sandy Hill, and each provides the basis for the more specific objectives:

- (1) *To preserve and enhance Sandy Hill as an attractive residential neighbourhood, especially for family living.*
- (2) *To provide for a broad range of socio-economic groups.*
- (3) *To accept a modest increase in population, primarily as a way of housing some of the growth in the Central Area labour force.*
- (4) *To maintain and coordinate both the local functions of Sandy Hill (primarily as a residential neighbourhood) and the functions that serve a wider area (e.g., the mainstreet mixed-use area along Rideau Street and the University of Ottawa)*

Section 3.4 speaks to the cultural heritage of the area:

21) The Central and East Downtown Core is distinguished by its high concentration of heritage buildings, districts and landscapes, including those designated under Part IV and Part V of the Ontario Heritage Act, the Federal Heritage Buildings Review Office, or listed on the City's Heritage Register. Development will respect the area's heritage character and where located on or adjacent to a built heritage resource, will be in accordance with the policies found in Section 4.5 – Cultural Heritage and Archaeology, of Volume 1 of the Official Plan.

22) Development on properties designated under Part V of the Ontario Heritage Act, including alterations to existing properties and new construction, shall be consistent with the policies and guidelines of the following Heritage Conservation District Plans, as applicable: Bank Street, Besserer-Wurtemberg, Cathedral Hill, Centretown, Daly Avenue, King Edward Avenue, Minto Park, Russell-Range, Sandy Hill West, Sparks Street, Stewart-Wilbrod and Sweetland Avenue. Heritage applications will be required for any development on properties located within a Heritage Conservation District.

**Comment |** The proposal introduces a low-rise, four-storey apartment building that is consistent with the permitted built form for Neighbourhoods within the Downtown Core. The scale, height, and massing are appropriate to the surrounding context, which includes a mix of low-rise dwellings and apartment buildings. The design emphasizes a pedestrian-oriented frontage, generous glazing, high-quality materials, and landscaping, contributing positively to the public realm and reinforcing Sandy Hill's character as an attractive, livable residential neighbourhood. While compact in form, the building provides functional, well-designed dwelling units suitable for a range of household types, including families seeking centrally located rental housing.

The subject property is not designated under Part IV or Part V of the *Ontario Heritage Act* and is not located within a Heritage Conservation District. As such, no heritage applications are required. Nevertheless, the proposed development respects the broader

heritage character of Sandy Hill through its low-rise form, articulated façades, and contextual design approach. The scale and massing are compatible with the surrounding built environment, ensuring that the development does not detract from nearby heritage buildings, districts, or landscapes. The proposal is consistent with the intent of Section 4.5 of the Official Plan by demonstrating sensitivity to context and reinforcing the established urban fabric of the area.

**Based on our review, it is our professional planning opinion that the proposed development conforms with the City of Ottawa Central and East Downtown Core Secondary Plan.**

## 4.4 City of Ottawa Zoning By-law

The City of Ottawa zones this site as R4UD - Residential Fourth Density, Subzone UD, Urban Exception 480 in the City of Ottawa Zoning By-law 2008-250. The intent of the R4UD Zone is to allow for a wide mix of residential building forms. The performance standards in the zone seek to regulate development in a manner that is compatible with existing land use patterns so that the mixed building form, residential character of a neighbourhood is maintained or enhanced. As the property is located within the Greenbelt, it is subject to the alternative provisions of Section 139 and 144. The table below provides an overview of the required provisions for this zone and the proposed development's compliance.

<b>Lot Details:</b>	<ul style="list-style-type: none"> <li>[ Lot frontage: 20.12 m</li> <li>[ Lot depth: 37.59 m</li> <li>[ Lot area: 756.32 m<sup>2</sup></li> <li>[ Rear Yard Area: 151.6 m<sup>2</sup></li> </ul>		
<b>Unit Details:</b>	<ul style="list-style-type: none"> <li>[ 1-Bedrooms: 16</li> <li>[ 2-Bedrooms: 10</li> <li>[ 2-Bedroom + Office: 2</li> <li>[ Total: 28</li> </ul>		
<b>EXISTING ZONING BY-LAW</b> <b>R4UD[480] Low-rise</b> <b>Apartment Dwelling (9+</b> <b>units)</b>	<b>Requirement</b>	<b>Provided</b>	<b>Section</b>
<b>Minimum No. of 2-Bedroom Units</b>	25%	43% 2 bedroom	Section 161(16)(b)(i)
<b>Minimum Lot Width</b>	15 m	20.12 m	Section 162, Table 162A
<b>Minimum Lot Area</b>	450 m <sup>2</sup>	756.5 m <sup>2</sup>	Section 162, Table 162A
<b>Max Building Height</b>	14.5 m	14.4 m	Section 162, Table 162A
<b>Minimum Front Yard Setback</b>	Lesser of the average of abutting lots' corresponding FYS or 4.5 m (1.4 m average of abutting, but not less than 1.5 m)	2.65 m	Section 144(1)(a); Section 162, Table 162A
<b>Minimum Rear Yard Setback</b>	30% of lot area (11.28 m <sup>2</sup> )	<b>7.52 m (20.02% of lot depth)</b>	Section 144, Table 144A

<b>Minimum Interior Yard Setback</b>	1.5 m	1.7 m & 2.02 m	Section 162, Table 162A
<b>Minimum Area of Soft Landscaping in Rear Yard</b>	On a lot greater than 450 m <sup>2</sup> in area: 50% of rear yard  Minimum aggregate area of 25 m <sup>2</sup> , with a longer dimension $\leq 2 \times$ shorter dimension	100%	Section 161(15)(b)(i)  Section 161(15)(b)(iv)
<b>Minimum Rear Yard Area</b>	25% of lot area (189.12 m <sup>2</sup> )	<b>151.6 m<sup>2</sup> (20%)</b>	Section 144(3)(a)
<b>Minimum Aggregate Front Yard Soft Landscaped Area</b>	For lot where FYS is 1.5 m - 3m is 20% (10.46 m <sup>2</sup> )	29.8 m <sup>2</sup> (56.9%)	Section 161, Table 161
<b>Minimum Fenestration requirement</b>	Front facade: at least 25% windows	29.4% facade = 61.73 m <sup>2</sup>	Section 161(15)(g)
<b>Minimum Front Facade Additional Recess</b>	20% an additional 0.6 m from the front setback line (except where balconies or porches are provided for each storey at or above the first storey)	<b>0.61 m for 13.4% and 0.56 m for 29.9%</b>	Section 161(15)(h)
<b>Parking: Area X</b>			
<b>Minimum Number of Parking Spaces (Resident)</b>	S101(3)(a) Parking only required for units > 12: 16 units @ 0.5 spaces per d/u = <b>8 spaces</b>	<b>0 spaces</b>	Section 110
<b>Minimum Number of Parking Spaces (Visitor)</b>	S.102(2) 0 spaces $\leq 12$ spaces Table 102: 0.1 spaces per d/u @ 16 units = 1.6 <b>(2 spaces)</b>	<b>0 spaces</b>	Section 102
<b>Maximum Walkway</b>	For a low-rise apartment: 1.8 m	1.8 m	Section 139(4)(c)(i)
<b>Minimum Bicycle Parking</b>	0.5 per dwelling unit : 14 spaces	28 parking spaces	Section 111, Table 111A(b)
<b>Encroachments</b>			
<b>Encroachments: Canopies (low-rise, multiple residential)</b>	A distance of 1/2 the depth of the front, rear or corner side yard, but not closer than 0.6 m to the lot line	Rear & front yard complies	Section 65(4)(b)(i)

<b>Encroachments: Canopies (low-rise, multiple residential)</b>	1.8 m into an interior side yard, but not closer than 0.6 m to a side lot line	N/A	Section 65(4)(b)(ii)
<b>Encroachments: Open Stairways</b>	Where at or below the floor level of the first floor: 1. in the case of the interior side yard or rear yard: no limit, and 2. in the case of the front yard or corner side yard: no closer than 0.6m to a lot line, and	Front 1.2 m and Rear 1.0 m, complies	Section 65(5)(b)(i)
<b>Encroachments: Open Stairways</b>	Other cases: In the case of any yard: 1.5 m, but not closer than 1 m to a lot line; except that, switchback stairs and landings may project 2.2 m into the rear yard where these are intended to provide a means of egress for dwelling units located on the second and higher storeys.		Section 65(4)(b)(ii)

<b>NEW ZONING BY-LAW N4B Low-rise Apartment Dwelling (Approved by Council January 28, 2026 - Not in effect)</b>	<b>Requirement</b>	<b>Provided</b>	<b>Section</b>
<b>Max Number of Dwelling Units</b>	N/A	28	Section 801, Table 801A
<b>Minimum Lot Width</b>	7.5 m	20.12 m	Section 801, Table 801A
<b>Minimum Lot Area</b>	450 m <sup>2</sup>	756.5 m <sup>2</sup>	
<b>Max Building Height</b>	14.5 m	14.4 m	Section 801, Table 801A
<b>Minimum Front Yard Setback</b>	Lesser of the average of abutting lots' corresponding FYS or 4.5 m (1.4 m average of abutting, but not less than 1.5 m)	2.65 m	Section 801, Table 801A, Section 802(2)(c)
<b>Minimum Rear Yard Setback</b>	25% of lot depth but not more than 7.5 m (9.4 m)	7.52 m	Section 801, Table 801A

<b>Minimum Rear Yard Area</b>	25% of lot area (189.12 m <sup>2</sup> )	<b>150.9 m<sup>2</sup> (19%)</b>	
<b>Minimum Interior Yard Setback</b>	Total 1.8 m	1.7 m & 2.02 m	Section 801, Table 801A
<b>Minimum Area of Soft Landscaping in Rear Yard</b>	On a lot greater than 450 m <sup>2</sup> in area: 50% of rear yard  Minimum aggregate area of 25 m <sup>2</sup> , with a longer dimension <= 2 x shorter dimension	100%	Section 802(12)
<b>Minimum Landscaped Area</b>	30% of lot area (226.95m <sup>2</sup> )	<b>188.8 m<sup>2</sup></b>	
<b>Minimum Aggregate Front Yard Soft Landscaped Area</b>	For lot where FYS is 1.5 m - 3m is 20% (10.46 m <sup>2</sup> )	29.8 m <sup>2</sup> (56.9%)	Section 802(10)
<b>Minimum Fenestration requirement</b>	Front facade: at least 25% windows	29.4% facade = 61.73 m <sup>2</sup>	Section 802(15)
<b>Minimum Front Facade Additional Recess</b>	At least 20% of front facade minimum of 0.6 m from front setback line	<b>0.61 m for 13.4% and 0.56 m for 29.9%</b>	
<b>Minimum Parking Space</b>	2.6 m (width) x 5.2 m (length)  Maximum width of 3.1 m	N/A	
<b>Unit Breakdown</b>	25% 2 bedroom units	43% 2 bedroom	
<b>Maximum Driveway Width</b>	3 m, no double driveway permitted	N/A	
<b>Driveway Separation from Interior Lot line</b>	0.15 m (can be landscaped or decorative brick)	N/A	
<b>Maximum Walkway</b>	For a low-rise apartment: 1.5 m	1.8 m	Section 802(8)
<b>Minimum Bicycle Parking (Short Term)</b>	minimum 2 spaces with an additional 0.1 spaces per unit above 20 residential units = 3 space	<b>3 spaces</b>	Section 613, Table 613B
<b>Minimum Bicycle Parking (Long Term)</b>	1 per unit = 28 spaces	1 per unit = 28 spaces	Section 613, Table 613B
<b>Minimum Number of Parking Spaces (Resident)</b>	N/A	<b>0 spaces</b>	

<b>Minimum Number of Parking Spaces (Visitor)</b>	N/A	<b>0 spaces</b>	Section 603
<b>Encroachments: Canopies (low-rise, multiple residential)</b>	A distance of 1/2 the depth of the front, rear or corner side yard, but not closer than 0.6 m to the lot line	Rear & front yard complies	Section 204(7)
<b>Encroachments: Canopies (low-rise, multiple residential)</b>	2.0 m into an interior side yard, but not closer than 0.6 m to a side lot line	N/A	Section 204(7)
<b>Encroachments: Open Stairways</b>	Where at or below the floor level of the first floor: in the front or exterior side yards the minimum setback from a lot line is 0.6 metres; and in the rear or interior side yards, no minimum setback.	Front 1.2 m and Rear 1.0 m, complies	Section 204(12)
<b>Encroachments: Open Stairways</b>	Other cases: 1.0 m and maximum projection is 1.5 m		Section 204(12)
<b>Functional Path of Travel</b>	To rear yard 1.5 m by 1.5 m	Provided	Section 802(7)(8)

## 5.0 PUBLIC CONSULTATION STRATEGY

Public Consultation for the proposed development occurs through the following means:

- [ Open line of communication where any community member is welcome to contact Q9 Planning + Design and provide comments and feedback throughout the process
- [ Required signage on site with City file lead contact details (comments provided are shared with the proponent)
- [ Updates shared with the Community via the Councillor's office and the Community Association for an efficient line of communication moving forward.

## 6.0 INTEGRATED ENVIRONMENTAL REVIEW STATEMENT (IERS)

### 6.1 Summary of Reports and Studies

#### 6.1.1 ENVIRONMENTAL SITE ASSESSMENT

A Phase I - Environmental Site Assessment was prepared by Paterson Group dated October 22, 2025. The objective of this Phase I ESA was to research the past and current use of the site (Phase I Property) and a 250m radius (Phase I Study Area) to identify any environmental concerns with the potential to have impacted the subject property.

According to the historical research, the Phase I Property was developed with a residential dwelling prior to 1912, which was demolished after 2022. No potential environmental concerns were identified with respect to the historical use of the Phase I Property.

Historically, neighbouring properties were used for residential purposes, with minimal commercial and institutional land uses further away from the Phase I Property, but within the Phase I Study Area. Some historical off-site potentially contaminating activities were identified on properties situated within the Phase I Study Area, however, due to their separation distances and/or their cross-gradient orientation with respect to the Phase I Property, none of these historical off-site activities are considered to have had the potential to impact the Phase I Property.

Following the historical research, a site visit was conducted. The Phase I ESA Property is currently vacant. The ground surface is lightly vegetated. No potential environmental concerns were identified with respect to the current use of the Phase I Property.

Neighbouring land use consists predominately of residential developments, with minimal commercial, community, and institutional land uses further away from the Phase I Property, but within the Phase I Study Area. No potential environmental concerns were identified with respect to the current uses of the neighbouring properties.

Based on the findings of this assessment, it is our opinion that a Phase II – Environmental Site Assessment will not be required for the Phase I Property.

#### 6.1.2 GEOTECHNICAL ASSESSMENT & SLOPE STABILITY ANALYSIS

A Geotechnical Assessment was prepared by Paterson Group dated January 26, 2026. The report evaluates subsurface and groundwater conditions at 71 Russell Avenue in Ottawa to support a proposed multi-storey residential development. Based on a review of prior borehole data, laboratory testing, and site conditions, the report concludes that the site is suitable for the proposed development, with conventional spread footings founded on undisturbed, compact glacial till. Subsurface conditions generally consist of fill and silty clay overlying glacial till and limestone/shale bedrock, with groundwater encountered at approximately 5.4 m below grade. The report provides detailed recommendations for site preparation, foundation and basement design, seismic considerations, pavement structures, excavation support, drainage, frost protection, and construction precautions,

including the need for temporary shoring and groundwater control. Overall, the findings support feasible construction provided the outlined geotechnical recommendations and monitoring requirements are followed.

The slope stability assessment was performed because the subject site slopes downward from west to east, with a drop of about 6 to 7 meters across the site. The analysis was conducted for the proposed site conditions using the computer program SLIDE2, which performs a two-dimensional slope stability analysis with limit equilibrium methods, such as the Bishop's Method. The program calculates a factor of safety, which is the ratio of the forces resisting failure to those favouring failure.

The minimum recommended factors of safety are generally 1.5 for static conditions and 1.1 for seismic conditions, where slope failure would endanger permanent structures. For conservative modelling, the native subsoil along the slope was assumed to be fully saturated.

The results of the analysis under proposed conditions indicated the following for Section A-A':

- Static Loading Analysis: The factor of safety was found to be greater than 1.5, meaning the slope is considered stable under static loading conditions.
- Seismic Loading Analysis: A horizontal acceleration of  $0.186 \text{ g}$  was used, and the factor of safety was found to be greater than 1.1, indicating the slope is considered stable under seismic loading.

In conclusion, because the factors of safety exceed the minimum recommended values for both static (1.5) and seismic (1.1) conditions, the slope at the site is considered stable for the proposed development.

### 6.1.3 GRADING & DRAINAGE

A Stormwater Management Report was prepared by T.L. Engineering Consultants dated December, 2025.

At this proposed residential site and to develop this lot to house a 28 unit apartment building on a 0.0757 ha. parcel of land, the estimated allowable flow off-site is calculated at 6.17 L/s based on City of Ottawa Drainage and Stormwater Management (SWM) criteria of 2-Year pre-development flow at  $C_{pre} = 0.38$ . For on-site SWM attenuation, the flat roof top of the proposed apartment building will be utilized and (4) controlled roof drains are incorporated each with a controlled maximum release rate of 0.95 L/s (15.0 U.S. gal/min.) under a maximum head of 150 mm. The controlled flow from this site at the 5-Year event totals to 3.16 L/s and 3.80 L/s for the 100-Year event for the post development condition. The uncontrolled 5-Year post development flow from the remainder of the site is estimated at 4.78 L/s and 9.42 L/s for the 100-year event respectively.

During the five (5)-year storm event for the flat rooftop storage, the ponding depth of rooftop area 1, 2, 3 and 4 is estimated at 100 mm at the drain and 0mm at the roof perimeter, assuming a 1.7% minimum roof pitch to the drain. The rooftop storage available

at Roof Area 1 is 1.49 m<sup>3</sup>, at Roof Area 2 is 2.01 m<sup>3</sup>, at Roof Area 3 is 1.17 m<sup>3</sup> and the rooftop storage available at Roof Area 4 is 1.70 m<sup>3</sup>, for a total of 6.37 m<sup>3</sup>, which is greater than the required volume of 5.68 m<sup>3</sup>.

During the 100-year storm event for the flat rooftop storage, the ponding depth of Roof Area 1, 2, 3 and 4 is estimated at 150 mm at the drain and 0mm at the roof perimeter, assuming a 1.7% minimum roof pitch to the drain. The rooftop storage available at Roof Area 1 is 4.55 m<sup>3</sup>, at Roof Area 2 is 6.59 m<sup>3</sup>, at Roof Area 3 is 4.15 m<sup>3</sup> and the rooftop storage available at Roof Area 4 is 5.31 m<sup>3</sup>, for a total of 20.60 m<sup>3</sup>, which is greater than the required volume of 13.82 m<sup>3</sup>.

Therefore, by means of flat building rooftop storage and grading the site to the proposed grades as shown on the Proposed Grading and Servicing Plan and Proposed Rooftop Stormwater Management Plan Dwg. 825-96 G-1 and 825-96 SWM-1 respectively, the desirable five (5)-Year storm and 100-Year storm event detention volume of 6.37 m<sup>3</sup> and 20.60 m<sup>3</sup> respectively will be available on site. Refer to Appendix D for detailed calculations of available storage volumes.

Thus for this development site, the 5-Year maximum post development flow draining off-site is the controlled roof top flow plus the uncontrolled flow from the remainder of the site totals to 7.94 L/s (4.78 L/s + 3.16 L/s) which is slightly 1.77 L/s above the allowable 6.17 L/s. For storm events up to and including 100-Year, the estimated maximum post development flow draining off-site is 13.22 L/s (9.42 L/s + 3.80 L/s) which exceeds the site allowable of 6.17 L/s by 7.05 L/s for this site.

However, in comparing the pre-development flow of the current site conditions to the post development flow, the SWM regulated flow plus uncontrolled flow from the proposed site under the post development conditions at the 5-Year event = 7.94 L/s and the 100 year event = 13.22 L/s where both of the post development flow events are less than the current pre-development flow estimate for the site at 5-Year Pre = 8.33 L/s and 100-Year Pre = 16.91 L/s. Therefore with this proposed development, stormwater flow is improved from that of the existing condition for the 5-year event at the 100-year event under the proposed post-development conditions.

### 6.1.3 TRAFFIC NOISE ASSESSMENT

A Traffic Noise Assessment was prepared by Gradient Wind dated January 26, 2025.

This report describes a traffic noise assessment undertaken for a proposed four-storey low-rise apartment building located at 71 Russell Avenue in Ottawa, Ontario. The primary source of environmental noise affecting the site is Chapel Street, a 2-lane urban collector roadway near the property. The assessment is based on (i) theoretical noise prediction methods that conform to the Ministry of the Environment, Conservation and Parks (MECP) and City of Ottawa requirements; (ii) noise level criteria as specified by the City of Ottawa's Environmental Noise Control Guidelines (ENCG); (iii) future vehicular traffic volumes based on the City of Ottawa's Official Plan roadway classifications; and (iv) site plan drawings prepared by Lawrence Architects.

The results of the current analysis indicate that exterior noise levels at the building façade will reach 51 dBA at the worst-case location in the daytime period (07:00-23:00) and 49

dBA in the nighttime period (23:00-07:00). The traffic noise was found to be in compliance with the ENCG objective sound level of 55 dBA, therefore no noise control measures are required for the development.

The building is surrounded by residential low-rise buildings. There are no significant sources of stationary noise impacting the development.

The development is expected to have small internal heating and cooling equipment; therefore, no significant sources of noise are associated with the development, and it will not have a significant impact of the surroundings.

#### 6.1.4 TREE CONSERVATION REPORT

A Stormwater Management Report was prepared by J.B Lennox & Associates dated December, 2025.

The Tree Conservation Report prepared by Lennox Landscape Architects has outlined that 23 existing trees will be removed and 3 existing trees will be protected on-site. The proposed plant list will include 1 tree, 86 shrubs and 16 ornamental grasses. The following table is a summary of the Tree Conservation Report:

PROPOSED PLANT LIST						
KEY	QTY.	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	REMARKS
<b>TREES</b>						
*HL	1	Gleditsia triacanthos 'Draves'	Street Keeper Honey Locust	50mm ø	B&B	
<b>SHRUBS</b>						
AH	32	Hydrangea arborescens 'Annabelle'	Annabelle Hydrangea	1 gallon pot	Potted	Space 1000mm o.c.
RR	25	Rosa rugosa	Rugosa Rose	800mm ht.	Potted	Space 1000mm o.c.
SS	29	Sorbaria sorbifolia 'Sem'	Sem False Spirea	600mm ht.	Potted	Space 1000mm o.c.
<b>ORNAMENTAL GRASSES</b>						
BL	10	Leymus arenarius 'Blue Dune'	Blue Lyme Grass	250mm pot	Potted	Space 800mm o.c.
KF	6	Calamagrostis 'Karl Foerster'	Feather Reed Grass	250mm pot	Potted	Space 800mm o.c.

\*Indicates that species is native to Ontario

EXISTING TREE LIST #								
KEY	QTY.	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	REMARKS	OWNERSHIP	COMMENTS
1	1	<i>Acer platanoides</i>	Norway Maple	490mm DBH	GOOD	To remain	City of Ottawa	
2	1	<i>Acer negundo</i>	Manitoba Maple	520mm DBH	GOOD	To be removed	71 Russell Avenue	
3	1	<i>Picea pungens</i>	Colorado Spruce	390mm DBH	POOR	To be removed	71 Russell Avenue	
4	1	<i>Picea pungens</i>	Colorado Spruce	310mm DBH	POOR	To be removed	71 Russell Avenue	
5	1	<i>Acer negundo</i>	Manitoba Maple	700mm DBH	POOR	To be removed	71 Russell Avenue	
6	1	<i>Acer platanoides</i>	Norway Maple	300mm DBH	POOR	To be removed	71 Russell Avenue	
7	1	<i>Acer negundo</i>	Norway Maple	400mm DBH	GOOD	To be removed	71 Russell Avenue	
8	1	<i>Acer negundo</i>	Manitoba Maple	500mm DBH	POOR	To be removed	71 Russell Avenue	
9	1	<i>Quercus macrocarpa</i>	Bur Oak	200mm DBH	GOOD	To be removed	71 Russell Avenue	
10	1	<i>Ulmus americana</i>	American Elm	600mm DBH	POOR	To remain	69 Russell Avenue	
11	1	<i>Acer negundo</i>	Manitoba Maple	500.650mm DBH	POOR	To remain	69 Russell Avenue	
12	1	<i>Acer negundo</i>	Manitoba Maple	500mm DBH	DEAD	To be removed	71 Russell Avenue	
13	1	<i>Acer saccharum</i>	Sugar Maple	300mm DBH	GOOD	To be removed	71 Russell Avenue	
14	1	<i>Acer saccharum</i>	Sugar Maple	100mm DBH	GOOD	To be removed	71 Russell Avenue	
15	1	<i>Acer platanoides</i>	Norway Maple	600mm DBH	GOOD	To be removed	71 Russell Avenue	
16	1	<i>Acer negundo</i>	Manitoba Maple	600mm DBH	GOOD	To be removed	71 Russell Avenue	
17	1	<i>Acer saccharum</i>	Sugar Maple	210mm DBH	GOOD	To be removed	71 Russell Avenue	
18	1	<i>Acer platanoides</i>	Norway Maple	700mm DBH	GOOD	To be removed	71 Russell Avenue	
19	1	<i>Acer saccharum</i>	Sugar Maple	500mm DBH	GOOD	To be removed	71 Russell Avenue	
20	1	<i>Acer saccharum</i>	Sugar Maple	100mm DBH	GOOD	To be removed	71 Russell Avenue	
21	1	<i>Fraxinus spp.</i>	Ash	200mm DBH	DEAD	To be removed	71 Russell Avenue	
22	1	<i>Acer platanoides</i>	Norway Maple	200mm DBH	GOOD	To be removed	71 Russell Avenue	
23	1	<i>Acer platanoides</i>	Norway Maple	100mm DBH	GOOD	To be removed	71 Russell Avenue	
24	1	<i>Acer platanoides</i>	Norway Maple	210mm DBH	GOOD	To be removed	71 Russell Avenue	
25	1	<i>Acer platanoides</i>	Norway Maple	100mm DBH	GOOD	To be removed	71 Russell Avenue	
26	1	<i>Acer platanoides</i>	Norway Maple	400mm DBH	GOOD	To be removed	71 Russell Avenue	

Figure 18: Excerpt from Tree Conservation Report

### 6.1.5 STORMWATER MANAGEMENT REPORT

A Stormwater Management Report was prepared by T.L. Engineering Consultants dated December, 2025.

At this proposed residential site and to develop this lot to house a 28 unit apartment building on a 0.0757 ha. parcel of land, the estimated allowable flow off-site is calculated at 6.17 L/s based on City of Ottawa Drainage and Stormwater Management (SWM) criteria of 2-Year pre-development flow at  $C_{pre} = 0.38$ . For on-site SWM attenuation, the flat roof top of the proposed apartment building will be utilized and (4) controlled roof drains are incorporated each with a controlled maximum release rate of 0.95 L/s (15.0 U.S. gal/min.) under a maximum head of 150 mm. The controlled flow from this site at the 5-Year event totals to 3.16 L/s and 3.80 L/s for the 100-Year event for the post development condition.

The uncontrolled 5-Year post development flow from the remainder of the site is estimated at 4.78 L/s and 9.42 L/s for the 100-year event respectively.

During the five (5)-year storm event for the flat rooftop storage, the ponding depth of rooftop area 1, 2, 3 and 4 is estimated at 100 mm at the drain and 0mm at the roof perimeter, assuming a 1.7% minimum roof pitch to the drain. The rooftop storage available at Roof Area 1 is 1.49 m<sup>3</sup>, at Roof Area 2 is 2.01 m<sup>3</sup>, at Roof Area 3 is 1.17 m<sup>3</sup> and the rooftop storage available at Roof Area 4 is 1.70 m<sup>3</sup>, for a total of 6.37 m<sup>3</sup>, which is greater than the required volume of 5.68 m<sup>3</sup>.

During the 100-year storm event for the flat rooftop storage, the ponding depth of Roof Area 1, 2, 3 and 4 is estimated at 150 mm at the drain and 0mm at the roof perimeter, assuming a 1.7% minimum roof pitch to the drain. The rooftop storage available at Roof Area 1 is 4.55 m<sup>3</sup>, at Roof Area 2 is 6.59 m<sup>3</sup>, at Roof Area 3 is 4.15 m<sup>3</sup> and the rooftop storage available at Roof Area 4 is 5.31 m<sup>3</sup>, for a total of 20.60 m<sup>3</sup>, which is greater than the required volume of 13.82 m<sup>3</sup>.

Therefore, by means of flat building rooftop storage and grading the site to the proposed grades as shown on the Proposed Grading and Servicing Plan and Proposed Rooftop Stormwater Management Plan Dwg. 825-96 G-1 and 825-96 SWM-1 respectively, the

desirable five (5)-Year storm and 100-Year storm event detention volume of 6.37 m<sup>3</sup> and 20.60 m<sup>3</sup> respectively will be available on site. Refer to Appendix D for detailed calculations of available storage volumes.

Thus for this development site, the 5-Year maximum post development flow draining off-site is the controlled roof top flow plus the uncontrolled flow from the remainder of the site totals to 7.94 L/s (4.78 L/s + 3.16 L /s) which is slightly 1.77 L/s above the allowable 6.17 L/s. For storm events up to and including 100-Year, the estimated maximum post development flow draining off-site is 13.22 L/s (9.42 L/s + 3.80 L/s) which exceeds the site allowable of 6.17 L/s by 7.05 L/s for this site.

In comparing the pre-development flow of the current site conditions to the post development flow, the SWM regulated flow plus uncontrolled flow from the proposed site under the post development conditions at the 5-Year event = 7.94 L/s and the 100 year event = 13.22 L/s where both of the post development flow events are less than the current pre-development flow estimate for the site at 5-Year Pre = 8.33 L/s and 100-Year Pre = 16.91 L/s. Therefore with this proposed development, stormwater flow is improved from that of the existing condition for the 5-Year event and at the 100-Year event under the proposed post-development conditions.

## 7.0 PLANNING ANALYSIS & RATIONALE

The proposed Minor Zoning By-law Amendment and Site Plan Control application to permit a low-rise apartment dwelling at 71 Russell Avenue is appropriate, represents good land use planning, and is consistent with provincial and municipal planning policy. The proposal has been evaluated against the Provincial Planning Statement (2024), the City of Ottawa Official Plan, and the City of Ottawa Zoning By-law, as summarized below.

### 7.1 Provincial Planning Statement, 2024

The Provincial Planning Statement (PPS), 2024 establishes a clear policy direction to accommodate growth through intensification within existing settlement areas, support the delivery of housing, and promote complete, transit-supportive communities.

The subject site is located within an existing urban settlement area that is fully serviced and well-connected to transit, active transportation networks, and community amenities. The proposed redevelopment efficiently utilizes an underdeveloped urban lot and results in a net increase in residential units, directly supporting PPS policies that encourage residential intensification and the efficient use of land and infrastructure.

The proposal contributes to the provision of a broader range and mix of housing options in a central location, including purpose-built rental units, which supports housing affordability and choice for a range of household types and incomes. The development is compact in form, supports active transportation through the provision of bicycle parking and pedestrian-oriented design, and minimizes reliance on private automobiles in an area with excellent transit access.

The PPS also promotes development that improves overall quality of life and social equity by locating housing close to employment, education, and services. Given the site's proximity to downtown Ottawa, the University of Ottawa, commercial corridors, and frequent transit routes, the proposal aligns with these objectives. No natural heritage features or hazard lands are affected by the proposal.

Accordingly, the proposed development is consistent with the policy direction of the Provincial Planning Statement, 2024.

### 7.2 City of Ottawa Official Plan

The subject property is designated **Neighbourhood** within the **Downtown Core Transect** in the City of Ottawa Official Plan. This designation anticipates residential intensification in the form of low-rise buildings, generally between two and four storeys, in a manner that is context-sensitive and supportive of 15-minute neighbourhoods.

The proposed low-rise apartment dwelling conforms to the planned function and scale of development contemplated for Neighbourhoods within the Downtown Core. The proposal provides additional residential density in a central location while maintaining a low-rise built form that is compatible with the surrounding context. The building height, massing, and setbacks are consistent with Official Plan policies that emphasize transitions, human-scale design, and positive contributions to the public realm.

The Official Plan places a strong emphasis on intensification in areas that are well served by transit and active transportation. The site's proximity to frequent transit routes, cycling infrastructure, and daily amenities supports the proposed reduction in vehicle parking and reinforces the Plan's direction to shift travel behaviour toward more sustainable modes. The provision of generous bicycle parking and direct pedestrian access further aligns with these policies.

The proposal also supports Official Plan housing objectives by contributing to the supply of rental housing and enabling a more efficient use of land within the built-up area. The development respects neighbourhood character while accommodating modest population growth, consistent with the Downtown Core and Sandy Hill policy framework.

Overall, the proposed development conforms with the City of Ottawa Official Plan.

### 7.3 City of Ottawa Zoning By-law

The subject property is zoned R4UD – Residential Fourth Density Zone (Urban Exception 480) in Zoning By-law 2008-250, which permits a range of low-rise residential building forms, including low-rise apartment dwellings. The intent of the zone is to support residential intensification while ensuring compatibility with existing neighbourhood character through appropriate performance standards.

The proposed development generally complies with the applicable zoning provisions related to use, height, lot width, lot area, interior side yard setbacks, landscaping, bicycle parking, and building design. The requested zoning relief relates to rear yard setback and area, front façade recess requirements, and the provision of no vehicular parking.

#### **Rear-Yard Setback & Rear Yard Area Rationale:**

The proposed reduction in the rear yard setback and rear yard area maintains the intent of the Zoning By-law by continuing to provide adequate outdoor amenity space, landscaped open space, and appropriate separation between buildings. Although a reduced setback is proposed, the resulting rear yard remains usable, contiguous, and fully landscaped, ensuring that it continues to function as a meaningful outdoor amenity for residents rather than residual or fragmented space.

The proposed rear yard setback of 7.52 m is consistent with the minimum rear yard setback endorsed by Council through the adoption of the new Comprehensive Zoning By-law, which reflects updated planning policy and contemporary urban development patterns in transit-supportive and walkable neighbourhoods. This alignment demonstrates that the proposed setback is appropriate in principle and consistent with the City's evolving approach to intensification in established urban areas, particularly within the Downtown Core.

The proposal incorporates a high-quality landscape design that prioritizes soft landscaping and tree planting opportunities in the rear yard. One hundred percent (100%) of the rear yard area will be soft landscaped, exceeding minimum requirements and ensuring sufficient soil volume to support new tree planting and long-term canopy growth. The consolidated rear yard configuration provides passive amenity space, contributing to urban canopy objectives, stormwater infiltration, and resident well-being.

While the removal of four (4) existing trees is required to accommodate the proposed building envelope and reduced setback, this impact is mitigated through the provision of enhanced landscaping within the rear yard. The net effect is a functional, landscaped outdoor space that supports environmental objectives while accommodating a more efficient and compact building form.

Importantly, the proposed rear yard configuration enables the delivery of additional rental housing in a highly walkable, transit-rich location where reliance on private vehicles is not required. Maximizing the developable area of the site allows for the efficient use of serviced urban land and supports broader municipal and provincial objectives related to housing supply, intensification, and sustainable transportation. In this context, the modest reduction in rear yard setback represents an appropriate planning trade-off that balances housing delivery, neighbourhood compatibility, and environmental considerations.

### **Reduction in Façade Recess Rationale:**

The minor reduction in the required front façade recess does not undermine the intent of the regulation, which is to ensure building articulation, visual interest, and a pedestrian-scaled streetscape along public rights-of-way. Despite the limited reduction, the proposed building achieves a high degree of façade articulation through a combination of architectural strategies that effectively break down the massing and enhance the pedestrian experience along Russell Avenue.

The façade facing Russell Avenue incorporates generous glazing and exceeds the minimum façade opening requirement of 25 percent, providing approximately 29 percent glazing. This level of transparency contributes to an active and engaging street presence, improves natural surveillance, and reinforces a human-scale relationship between the building and the sidewalk. Large window openings animate the façade and ensure that residential units have a direct visual connection to the public realm, consistent with the intent of the Zoning By-law and Official Plan urban design policies.

In addition to glazing, the building employs deliberate variation in materials, colour, and depth along both the vertical and horizontal axes of the façade. These material transitions and subtle recesses create a visual rhythm that reduce the perceived width and height of the building, thereby minimizing massing impacts and enhancing compatibility with the surrounding low-rise context. The articulation strategies provide the visual relief that the front façade recess regulation is intended to achieve, even where the numerical recess requirement is marginally reduced.

### **Relief from Resident & Visitor Parking Rationale:**

The request for relief from resident and visitor parking requirements is appropriate and supportable given the subject site's downtown location, proximity to frequent public transit, and strong active transportation connections. The site is located within a highly walkable urban area where daily needs, employment, education, and community services are readily accessible on foot, by bicycle, or by transit, significantly reducing the necessity for private vehicle ownership.

The proposal is consistent with the direction of Council as reflected in the recently approved Comprehensive Zoning By-law, which removes minimum parking requirements in

recognition of changing travel behaviour, climate objectives, and the need to support transit-oriented development. In this context, the absence of on-site vehicular parking does not represent an under-provision, but rather aligns with contemporary planning practice and the City's broader policy shift away from automobile-dependent development patterns. The proposed parking supply is therefore consistent in principle with Council-approved zoning standards.

The site benefits from immediate access to OC Transpo bus routes and is located within walking distance of higher-order transit corridors, commercial mainstreets, institutional uses, and cycling infrastructure. These locational attributes support a reduced reliance on private vehicles and make the site particularly well-suited to a low- or no-parking residential development. Residents are able to meet daily needs without the requirement to own or regularly use a vehicle, thereby reducing traffic impacts on local streets.

To further support sustainable transportation, the proposal exceeds the minimum bicycle parking requirements, providing secure, long-term bicycle parking for residents. This level of bicycle parking encourages active transportation, supports mode shift, and aligns with the City's objectives to increase cycling as a viable and convenient transportation option in the downtown core.

From a land-use perspective, eliminating on-site vehicular parking allows the site to be used more efficiently by prioritizing housing, landscaping, and pedestrian-oriented design over space-intensive parking infrastructure. This approach supports intensification goals, reduces impervious surface coverage, and contributes to a more attractive and functional urban environment.

Taken together, the requested zoning relief maintains the general intent and purpose of the Zoning By-law and facilitates an appropriate form of development that is compatible with the surrounding area.

## 8.0 CONCLUSION

The proposed development, to construct a 28-unit, 4-storey rental apartment building is considered to be an appropriate development, built form, and level of intensification suitable for the subject location.

The proposed zoning relief to permit reduced rear yard setback and rear yard area, reduced front façade recess, and relief from the provision of vehicular parking spaces is appropriate and supportable in the context of the subject site. The requested amendments are minor in nature, result in no undue adverse impacts on surrounding properties or the public realm, and maintain the general intent and purpose of both the City of Ottawa Official Plan and Zoning By-law. The proposed amendments facilitate a form of low-rise, transit-supportive residential development that is desirable for the appropriate use and development of the land.

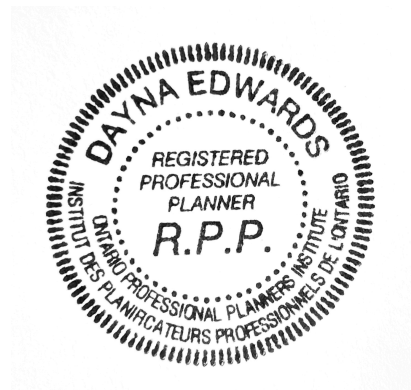
This project will contribute to the provision of rental housing in the area without compromising green space, while supporting public and active transportation modes.

The project is considered good land use planning and the proposed amendments to performance standards in the By-law are recommended for approval for land use planning perspective.

Yours truly,



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CC: Dave Aston, Jersey Developments Inc.

## LIMITATIONS OF REPORT

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