

**Peter Mansfield**  
A r c h i t e c t  
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122 Bridge Street, Almonte, Ontario K0A 1A0

## Urban Design Brief

**For:**

Job: P2403

### Proposed 39 Unit Residential Development

391 Hilversum Way, Ottawa, ON

Inverness Homes  
c/o Robin Daigle P.Eng.  
(343) 997-6755  
Email: [robin@invernesshomes.ca](mailto:robin@invernesshomes.ca)

November 25, 2025

The following report is formatted to address each individual under section Part 3 'Content' of the City of Ottawa's 'Urban Design Brief, Terms of Reference'

#### Project Description:

- Brief description of the design intent behind the development proposal. This description should be more design detailed, and not replicate the description within the Planning Rationale.*

**Response:** The design of 27 Hilversum Way has focused on creating a compatible architectural language of gable roofs, stone veneer with siding, bracketed awnings and individual balconies/terraces for all units. The palette of materials (and colours) are intended to reference a rural vernacular suitable to its location within the village of Carp. It is important to note that extensive effort has been put into creating a site that is both 'car free' and with full pedestrian circulation around its entire perimeter (Access to the below grade parking level has been located such that there is no 'cross circulation' between vehicles and pedestrians whatsoever).

- Project statistics, including gross floor area, the breakdown of floor area for different uses, total number and detailed breakdown of units, total number and detailed breakdown of vehicle and bike parking, building heights, lot coverage, etc. Project statistics should be illustrated in a table.*

**Response:** The proposed 39 Unit apartment building is an 'all ages' rental building complete with fully underground parking and several common interior and exterior amenities. Units offering consists of: 17 2-Bedroom Units, 3 1-Bedroom + Den Units, 17 1-Bedroom Units and 2 Bachelor Units. Barrier Free Units are provided for all four Unit types. A portion of the total number of units will be provided as affordable as defined under the City of Ottawa guidelines. Interior amenities include: Lounge Area, Party Room (with Kitchenette), Gymnasium and Lounge (with fireplace) as well as Tenant Lockers. A 'stand-alone' Clubhouse is located at the east end of site for use by the tenants of 391 Hilversum Way as well as neighbours from the adjacent townhomes. The Clubhouse amenities include a large Gathering Room (with expansive views into the north ravine), a full kitchen and Universal Washroom. Exterior common amenities include an expansive street-facing terrace adjacent the Party Room & Gymnasium along Hilversum Way, a large garden with bench seating in the centre of the site, a large covered canopy adjacent to the Clubhouse (with BBQ area) as well as a network of pedestrian pathways. Refer to the 'Site Statistics' table below for additional information.

### Site Statistics

Lot Area: 3,086 sm<sup>2</sup>  
Building Area (Apartment Bldg.): 1,257 sm<sup>2</sup>  
Building Area (Clubhouse Bldg.): 68 sm<sup>2</sup>

Coverage: 42.9%

Asphalt Area: 88 sm<sup>2</sup>  
Asphalt Coverage: 2.8%

Landscaped Area: 1,673 sm<sup>2</sup>  
Landscaped Coverage: 54.3%

Front Yard Setback: 6.0 m  
Exterior Yard Setback: 6.0 m  
Interior Yard Setback: 4.5 m  
Rear Yard Setback: 6.0 m  
Landscaped Boundary (along Street): 6.0 m

Parking Required:  
39 Units: 39 x 1.2(\*) = 47 Spaces  
\* As per Table 101, Area 'D' (Mid Rise Apartment)  
= 1 Space/Unit + 0.2Visitor = 1.2 [City of Ottawa]

Parking Provided: 47 Spaces  
Handicapped Parking Required: 1 Space  
Handicapped Parking Provided: 2 Spaces  
Bicycle Parking Provided: 20 Spaces



*Rendering of the proposal.*

**Response:** Please see rendering on right.

## Design Directives:

- *A concise summary and response to the applicable City's design policies, including from the Official Plan, and City urban design guidelines. A more 2 detailed responses shall be provided for any applicable urban design criteria that are not being met by the proposal.*

**Response:** Please refer to information provided in Planning Consultant's submission.

- *A response to urban design directions provided at the various pre-consultation meetings with City staff.*

**Response:** Please see email correspondence from Robin Daigle of Inverness Homes and Jerrica Gilbert from the City of Ottawa, dated August 12, 2025.

---

**From:** Robin Daigle  
**Sent:** August 12, 2025 2:08 PM  
**To:** 'Gilbert, Jerrica' <jerrica.gilbert@ottawa.ca>  
**Cc:** McCormick, Sarah <sarah.mccormick@ottawa.ca>; Kadri, Nader <nader.kadri@ottawa.ca>; Sclauzero, Cass <cass.sclauzero@ottawa.ca>; Gervais, Josiane <josiane.gervais@ottawa.ca>  
**Subject:** RE: PC2025-0124 391 Hilversum Pre-consultation Feedback

Thank you Jerrica for the additional feedback,

Please see attached revised Site Plan. After further revisions to the overall building interior and site, we feel we have effectively addressed the concerns that were raised. The specific changes to the design and responses to comments received are as follows:

- (1) The Vehicular Drop off at the south edge of the site has been eliminated. This now results in a solely pedestrian interface with Hilversum Way leading to the Primary Entrance of the building.
  - a. Note that we still meet the 15 meter maximum distance from the curb to the Front Entrance as required under Part 3.2.5 of the OBC for firefighting purposes.
- (2) The primary entrance has been re-located to the SE corner of the building immediately adjacent the main sidewalk. There is now significantly more soft landscaping in the vicinity of the building entrance and it is entirely free of vehicular traffic or crosswalks etc.
- (3) The Ground Level interior plan has been fully re-worked in order to place common amenities at the south face of the building adjacent Hilversum Way. These areas include the Exercise Room and one Amenity Room (both of which have direct access to an exterior terrace). We feel this will add greatly to the 'social realm' substantially.
- (4) We feel internalizing the Ramp into the underground parking area within the main building volume is not a beneficial idea for a several reasons:
  - a. Should the Ramp be internalized into the building rather than be placed at the exterior, the amount of parking provided on the Parking Level itself would be reduced significantly (by as much as 12-15 spots) This would necessitate placing these spots back at grade as surface parking. This in effect would be a significant 'step backward' in the site design.

- b. The space within the first floor volume taken up by the ramp would likely result in the loss of 2 or more living units.
  - c. By having the vehicular ramp externalized and offset from the face of the building, the amount of asphalt (i.e. access to an inboard parking ramp access) is reduced by approximately 75sm. As the site is currently configured, the total amount of asphalt is less than 3% of the site area.
  - d. A detached external vehicular ramp gives back the same amount of area (75sm) as landscaped amenity space. In this case a common garden. It is important to note that with the ramp situated in this manner, we have provided fully unbroken pedestrian circulation around the entire perimeter of the apartment building. This is enhanced further by pedestrian linkages to the ravine and adjacent Community Playground to the west.
- (5) No "guest suites" are currently being considered. The small single storey Clubhouse at the east corner of the site is intended to have a small kitchenette and dining/living space intended to be used as a dinner/party room for tenants. As the program spaces get more developed we will send along updated plans of the Clubhouse as soon as they become available.
  - (6) Environmental setbacks established during the Subdivision Approvals process including the Turtle Habitat have been respected and will be further detailed in subsequent submissions along with the applicable Environmental Plans and Reports required.

Trusting this addresses the high level concerns you had identified but please let me know if you have any questions. We are intending to proceed to a detailed design stage based on this concept, so please let me know if you have any further feedback as soon as possible.

Thank you,

Robin Daigle P.Eng.  
(343) 997-6755  
**Inverness Homes**  
Email: robin@invernesshomes.ca

**Site, Context and Analysis:** Photographs, maps, diagrams, and images may be utilized along with brief explanatory text to document and analyze condition and context of the site. The requested information should cover area within a 100 metre radius of a development site. A larger radius may be requested for larger / more complex projects.

- Photographs of existing site conditions and surrounding area, including a numbered map pinpointing where each photo is taken. Correspond these numbers with the site photos and include arrows illustrating the direction of the photograph. / Perspective images to and / or from the site.

**Response:** Please refer to site photographs below.



**Photo 1: Aerial Photo (Development Site shown in Red)**



**Photo 2: Looking South rear of site toward Cul-de-Sac**



**Photo 3: Looking NE toward Ravine**



**Photo 4: Looking SE toward Village**



**Photo 5: Looking North toward Inverness Townhomes**

- *Protected view corridors or views of interest that may be impacted by the proposed development.*

**Response:** There are no known protected view corridors on the development site.

- *Built and natural heritage assets on site and adjacent area.*

**Response:** There are no known natural heritage assets on the immediate development site. As the existing wooded ravine could be considered a natural asset, it should be noted that, with the exception of some walking paths, the ravine will be left undisturbed. It should also be noted that significant measures have been taken to mitigate any disturbances to the ravine (i.e. water flow and influx of sediments etc.).

- *Microclimate conditions of the site.*

**Response:** The overall development, including current adjacent townhomes, are all relatively low density and from low to moderate height (2 and 3 storeys). As such, it should be assumed that existing wind and patterns and solar arrays will be minimally impacted.

- *Key uses, destinations, and spatial elements in the surrounding area such as focal points/nodes, gateways, parks/open spaces, and public arts.*

**Response:** The surrounding area to the NW, North and NE is predominantly residential consisting of a mixture of detached single-family homes and new townhomes. Immediately to the south is a mix of commercial and some institutional along two of Carp's main streets: Donald B. Munroe Drive and Carp Road. A medical centre and elementary school are less than a kilometer to the NE. The most significant focal points within the immediate area is the carp Farmer's Market to the NW which is bordered along its north and NW edges with several recreational facilities. A pedestrian pathway from the SW portion of the Cul-de-Sac creates a direct linkage from the overall Inverness development to Carp Road (essentially to the centre of the Village).

- *Urban pattern (streets, blocks) / Characteristics of adjacent streets and public realm*

**Response:** The existing network of streets within the immediate area have evolved over several years and can be described as predominantly 'organic' in nature (i.e. not orthogonal in nature). The street, as well as pedestrian pathway, of the new development is very much in keeping with the existing 'fabric' of the surrounding area. The 'public realm' can be considered as low density and 'quaint' due to its mixture of older frame buildings (some of which are industrial in nature), small brick churches and older traditional storefronts etc. The single large multi-unit residential within the core of the village, to the south of Carp Road, is sited at the bottom of a small hill and effectively maintains a relatively low density feeling. As stated above, the Carp Framers Market (and Fairground) adds a significant amount of positive vitality to the surrounding

neighbourhood.

- *Mobility networks, such as transit stations, street networks, cycling facilities, pedestrian routes and connections, and parking.*

**Response:** There does not exist any transit stations in the immediate area. The area could be considered ‘bicycle friendly’ although there does not exist any designated cycling lanes within the village. Apart from the Farmers Market there are no designated parking lots etc. within the immediate area.

- *Future and current development proposals on adjacent properties.*

**Response:** Please refer to information provided in Planning Consultant’s submission.

- *The planned functions of the adjacent properties, such as the permitted building envelope under current zoning.*

**Response:** Please refer to information provided in Planning Consultant’s submission.

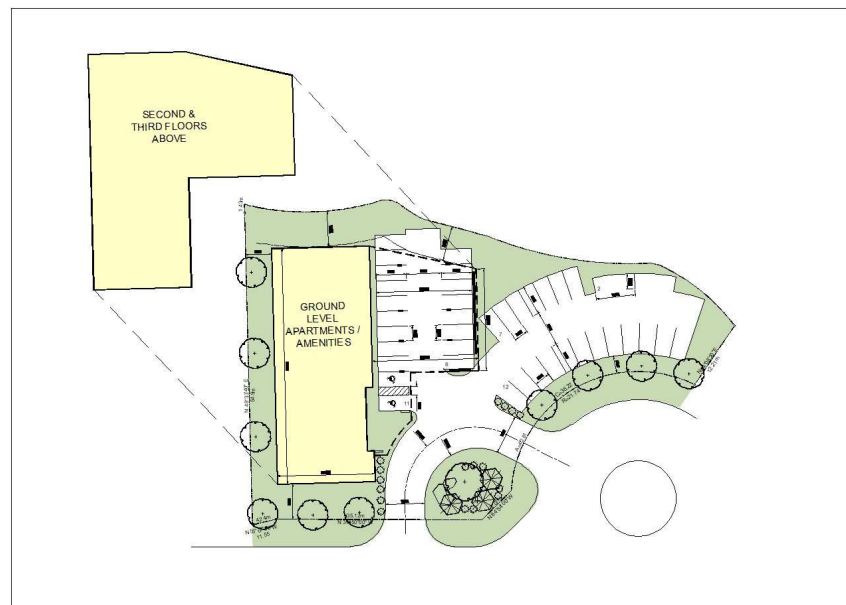
***Design Research: Diagrams, 3D images and other tools may be utilized to explain and illustrate design aspirations, alternatives and proposed outcomes.***

- *Parti diagrams, sketches, and precedent images.*

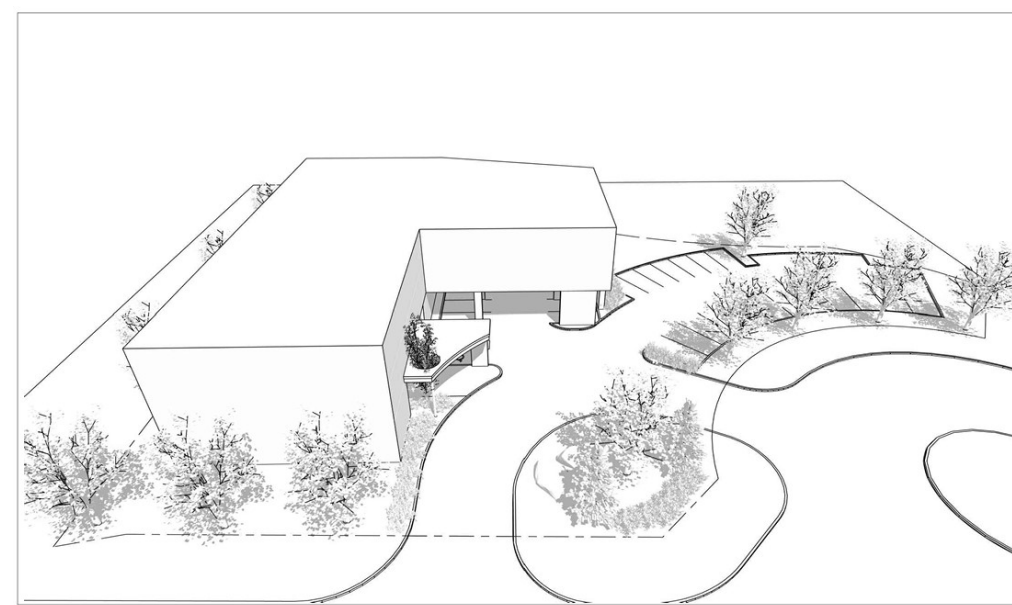
**Response:** Early parti diagrams & sketches demonstrated the challenges of this very irregularly shaped site. Once all the necessary setbacks (including those associated with the ravine as well as turtle habitat) were determined the bulk of the building mass was required to be at the NW corner of the site. Design development focused primarily on preserving street connections, views to the surrounding natural landscape as well as accommodating vehicular parking & circulation with minimal impact on the site – especially pedestrian circulation.

- *Alternative site plan options / Alternative massing options / Design evolution*

**Response:** Early conceptual plans accommodated all required parking at grade. The upper two levels were elevated above a significant portion of the on-grade parking in an attempt to maximize landscaped area within the site. Eventually it was decided to place all parking below grade as will be shown in the subsequent images of the design in its current form. A brief exploration of a ‘flat-roofed’ building was explored and was abandoned in favor of a more ‘rural’ vernacular which utilizes peaked roofs which we feel is more sympathetic to the context of surrounding village of Carp.



**Image 1: Early Site Plan showing at-grade parking**



**Image 2: Massing of Early Concept**



**Image 3: Earlier Concept showing flat roof and surface parking. This was abandoned in favor of a more sympathetic architectural style with below grade parking.**

- Massing of the proposed development in the existing.*

**Response:** Please refer to responses regarding comparative massing cited above as well as Perspective Drawings below.

- Block Plan illustrating potential future development in the area in which the proposed site is situated.*

**Response:** Please refer to the ‘Plan of Subdivision’ in the ‘Additional Materials – Appendix’ section of this document.

- Built form transition between the proposed development and the surrounding area / Response to abutting public realm conditions beyond the boundaries of the site.*

**Response:** A pedestrian pathway located at the SW corner of the Cul-de-Sac leading directly unto the Carp Road (in the ‘centre’ of the village) will link the overall Inverness Development to the greater ‘public realm’. Please also refer to information provided in Planning Consultant’s submission.

- Street cross sections that show the building wall to building wall conditions of the adjacent streets.*

**Response:** Given the non-urban context of this proposed development, we feel this will be effectively shown in the 3D images to follow.

- Approach to sustainable design as it relates to the City’s High-performance Development Standards or any other accredited system such as LEED.*

**Response:** Pending City of Ottawa approvals, it is Inverness's intent to construct a building envelope which will meet or exceed all current requirements under the MMA Supplementary Standard 'SB-10' Energy Efficiency Requirements of the current Ontario Building Code. As an additional note, sun shading awnings have been incorporated into the design which will contribute to the mitigation of solar gain. The fact that all parking (and the associated hard surfaces) have been removed from the site will enhance the storm water management significantly. The abundant gardens will also add many environmental as well as psychological benefits to the proposed development.

- *Approach to bird-safe design as it relates to the City's Bird-Safe Design Guidelines*

**Response:** Care will be taken to the extent possible to avoid design elements that could endanger birds such as overly large expanses of un-marked glass etc.

**Additional Materials – Appendix:**

- *Plan of Subdivision*

**Response:** Please refer to the Plan of Subdivision Drawing below.



*Plan of Subdivision by Vanderberg & Wildeboer Architects, dated June 2019.*

□ *Architectural Site Plan*

**Response:** Please refer to the Architectural Site Plan in Appendix A attached.

□ *Landscape Plan*

**Response:** Please refer to the Landscape Plan from Rudy Levstek Landscape Consultants Ltd. (not part of this document).

□ *Grading and Drainage Plan / Site Servicing Plan*

**Response:** Please refer to Grading / Drainage Plan and Site Servicing Plan as provided by RCI Robinson Land Development Ltd. (not part of this document).

□ *Building elevation of the proposed building / Perspective Drawings*

**Response:** Please refer to Elevation Drawings below.



**Image 1: South Elevation (as Seen From Hilversum Way)**



**Image 2: West Elevation**



**Image 3: East Elevation**



**Image 5: North Elevation (from Ravine)**



**Image 6: Main Entrance (from Hilversum Way)**



**Image 7: Looking NW into Courtyard Garden**



*Image 8: Looking West at Access Ramp to Underground Parking*



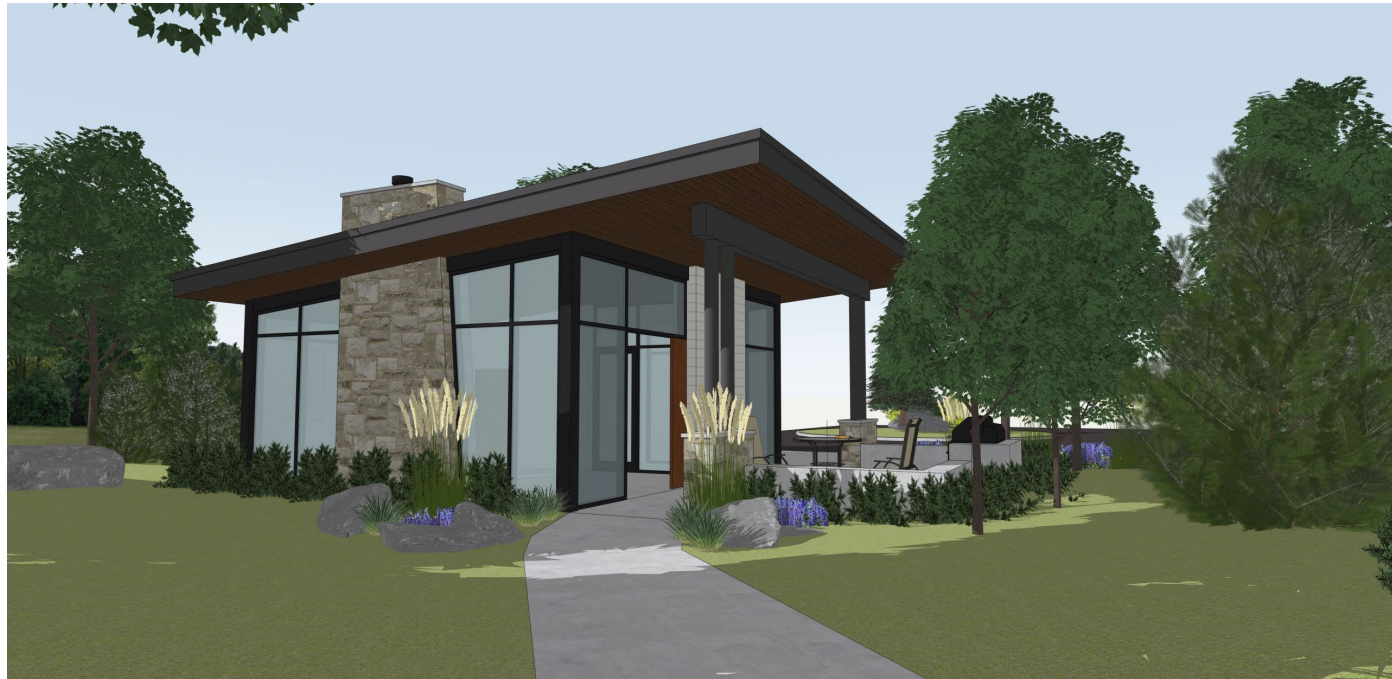
*Image 9: Looking East from Community Park*



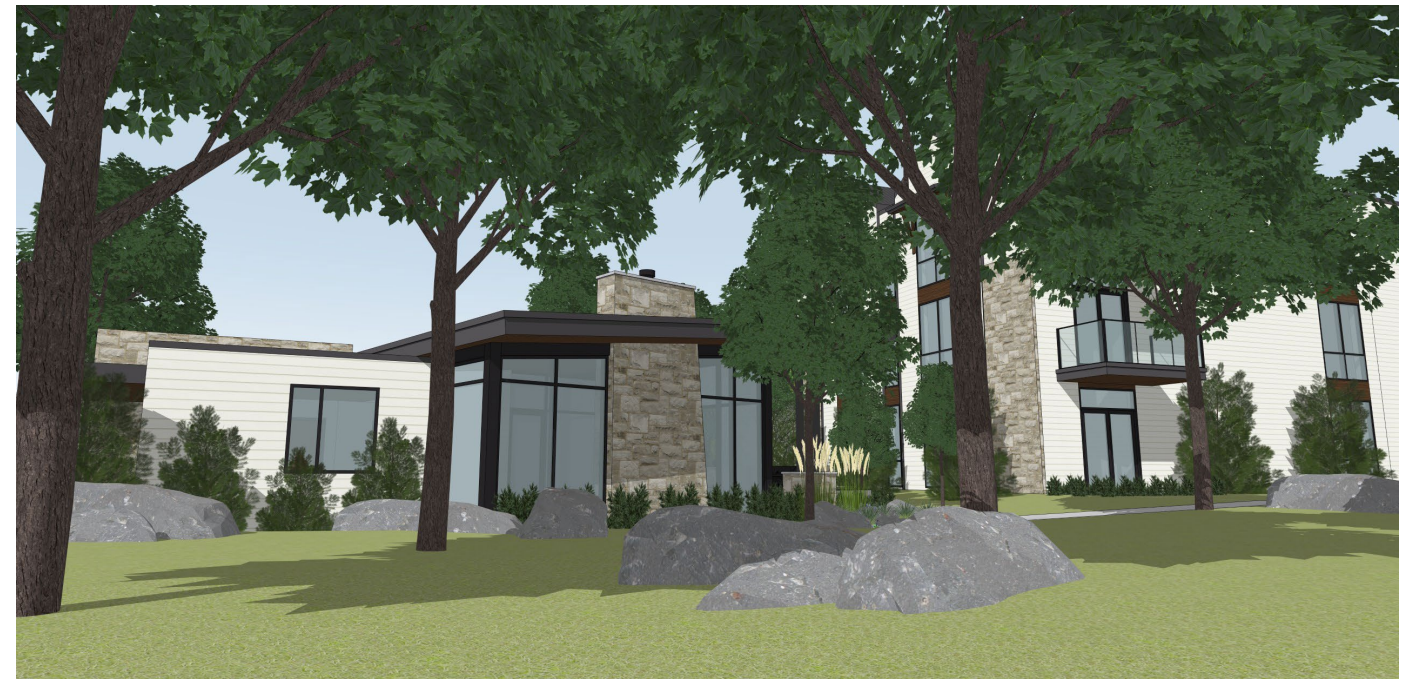
*Image 10: Aerial View Looking Southwest*



*Image 11: Approach to Clubhouse (Looking North)*



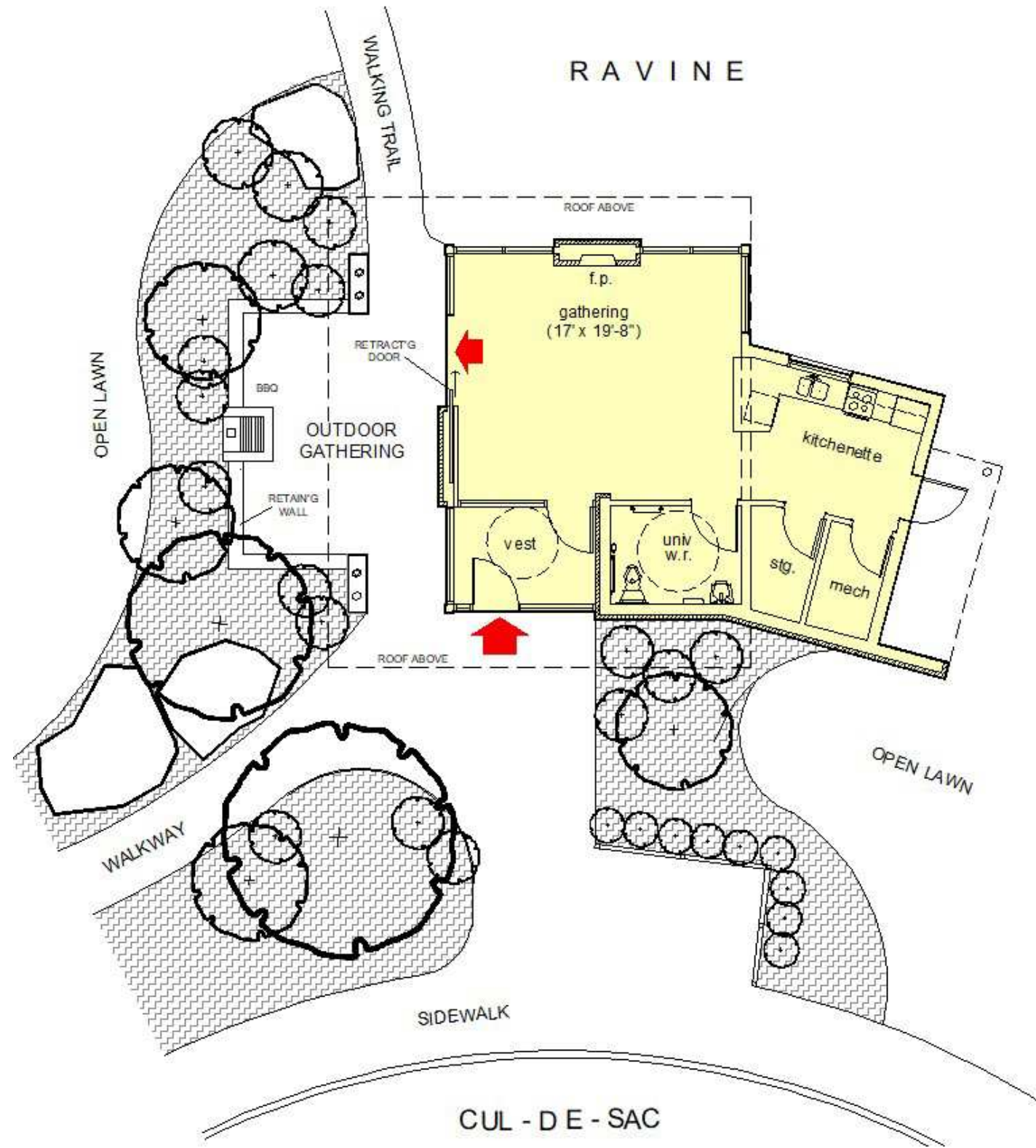
*Image 12: Approach to Clubhouse from Ravine Walking Path (Looking South)*



*Image 13: Clubhouse from Ravine (Looking Southwest)*



*Image 14: Looking North from Cul-de-Sac*



*Image 16: Aerial View of Site showing Apartment and Adjacent Clubhouse Building*

*Image 15: Schematic Site Plan of Shared Clubhouse Building*

*Floor Plan(s) of the proposed building(s).*

**Response:** Please refer to the Architectural Site Plan in Appendix A attached.

*Wind Analysis*

**Response:** Due to the relatively modest height of the building (at 3 storeys) and surrounding open space, a formal Wind Study was not undertaken.

*Shadow Analysis*

**Response:** Due to the relatively modest height of the building (at 3 storeys) and surrounding open space, a formal Shadow Study was not undertaken.

*Heritage Impact Statement*

**Response:** Due to the site's undeveloped surrounding area a Heritage Impact Statement was not undertaken.

Respectfully submitted,

A handwritten signature in blue ink, appearing to read "Peter Mansfield".

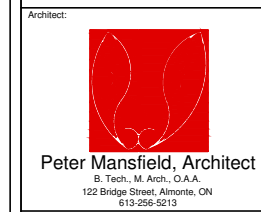
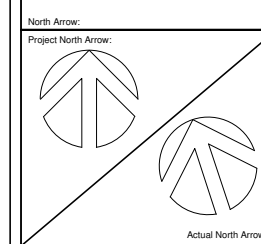
Peter Mansfield, Architect

Attached: Appendix A:

- Architectural Site Plan (Peter Mansfield Architect)
- Floor Plans (Peter Mansfield Architect)

It is the responsibility of the appropriate Contractor to verify all dimensions on site and report all errors and/or omissions to the Architect.  
 All Contractors must comply with pertinent codes & by-laws.  
 Do not scale drawings. This drawing may not be used for construction until signed. Architect's copyright reserved.  
 Metric Scale Drawing: All measurements are in millimeters (mm) unless otherwise noted.

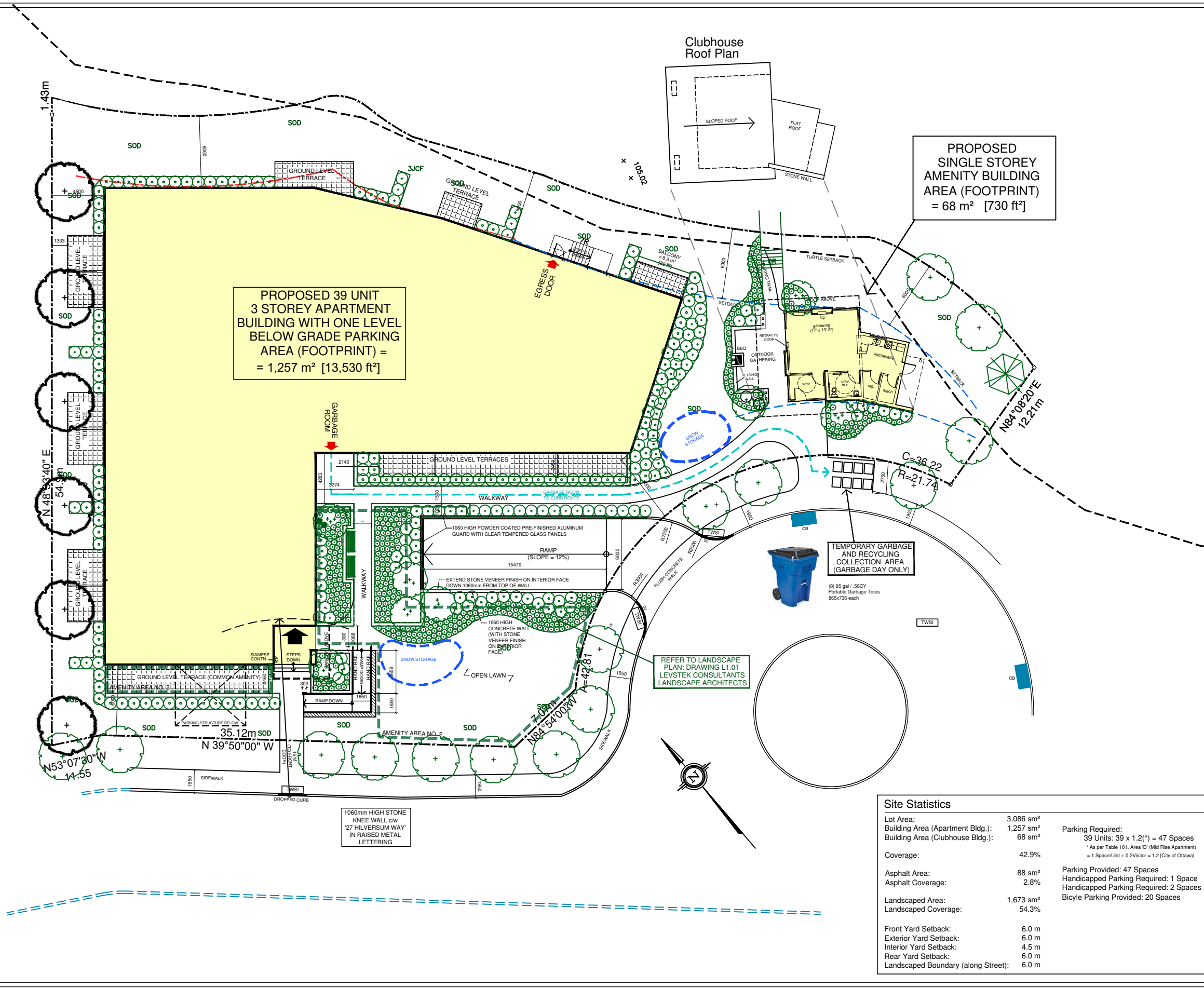
Symbol Legend:



Project Title:  
 Proposed 39 Unit Apartment Building  
 391 Hilversum Way, Carp, ON

Drawing List:  
 Site Plan

Job No.:	2404	Drawing No.:	<b>A0.0</b>
Scale:	As Noted		
Date:	Nov 03-2025		
Drawn By:	PM	Reviewed By:	
	PM		



Site Statistics		
Lot Area:	3,086 m <sup>2</sup>	Parking Required: 39 Units: 39 x 1.2(*) = 47 Spaces * As per Table 101, Area 'D' (Mid Rise Apartment) = 1 Space/Unit + 0.2/Visitor = 1.2 (City of Ottawa)
Building Area (Apartment Bldg.):	1,257 m <sup>2</sup>	
Building Area (Clubhouse Bldg.):	68 m <sup>2</sup>	
Coverage:	42.9%	Parking Provided: 47 Spaces Handicapped Parking Required: 1 Space Handicapped Parking Required: 2 Spaces Bicycle Parking Provided: 20 Spaces
Asphalt Area:	88 m <sup>2</sup>	
Asphalt Coverage:	2.8%	
Landscaped Area:	1,673 m <sup>2</sup>	
Landscaped Coverage:	54.3%	
Front Yard Setback:	6.0 m	
Exterior Yard Setback:	6.0 m	
Interior Yard Setback:	4.5 m	
Rear Yard Setback:	6.0 m	
Landscaped Boundary (along Street):	6.0 m	

No.:	Issued For:	Date:
1	For SPA	Nov 04-2025

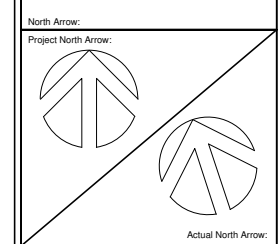
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Symbol Legend:



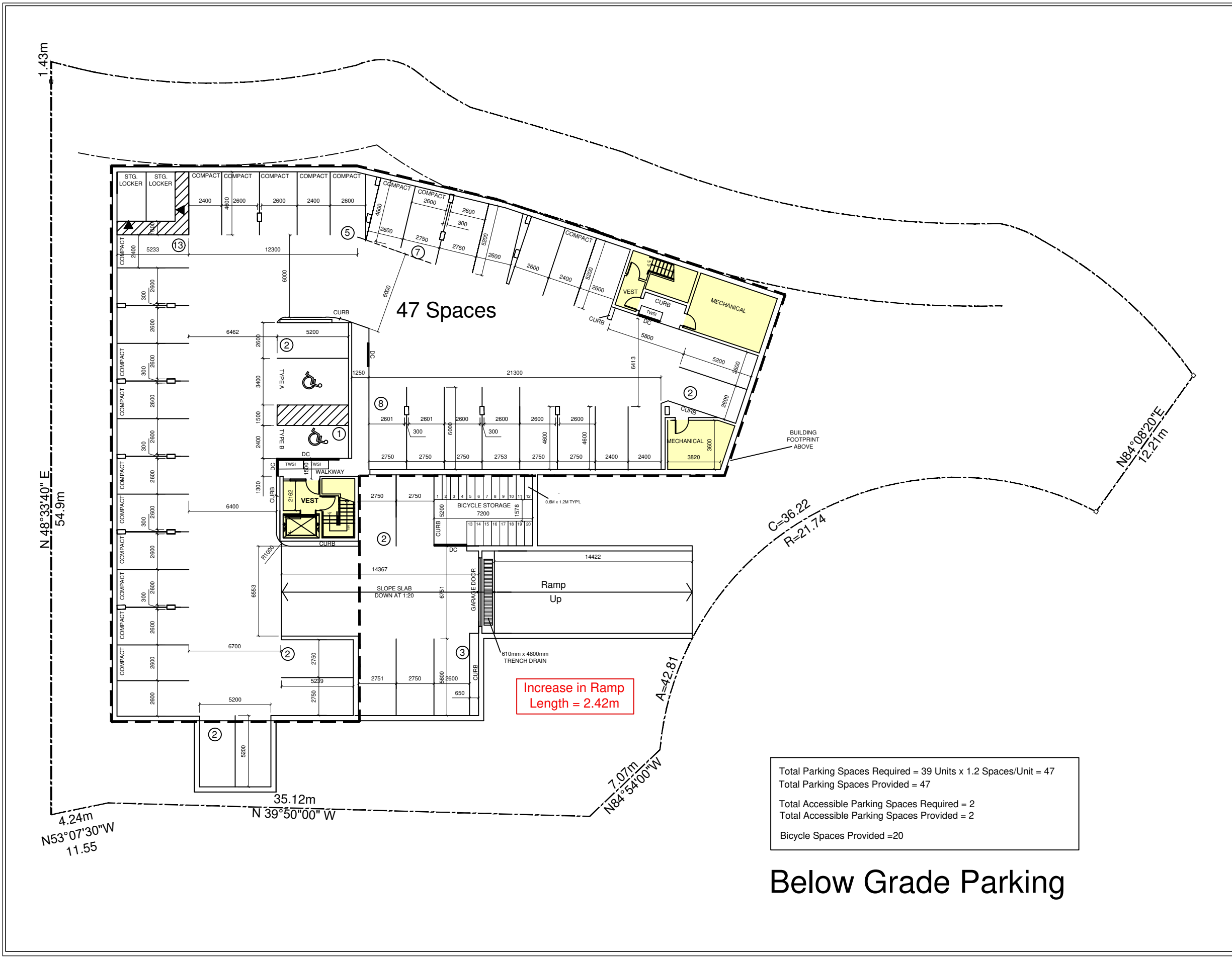
Architect:

**Peter Mansfield, Architect**  
 B. Tech., M. Arch., O.A.A.  
 122 Bridge Street, Almonte, ON  
 613-256-5213

Project Title:  
 Proposed Apartment Building  
 391 Hilversum Way, Carp, ON

Drawing List:  
 Lower Level Plan

Job No.:	2404	Drawing No.:	<b>A0.1</b>
Scale:	1:125		
Date:	Nov 03 2025		
Drawn By:	PM	Reviewed By:	



Increase in Ramp Length = 2.42m

Total Parking Spaces Required = 39 Units x 1.2 Spaces/Unit = 47  
 Total Parking Spaces Provided = 47

Total Accessible Parking Spaces Required = 2  
 Total Accessible Parking Spaces Provided = 2

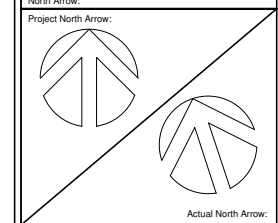
Bicycle Spaces Provided = 20

## Below Grade Parking

No.:	Issued For:	Date:
1	For SPA	Nov 03-2025

It is the responsibility of the appropriate Contractor to verify all dimensions on site and report all errors and/or omissions to the Architect.  
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Symbol Legend:



Architect:  
**Peter Mansfield, Architect**  
 B. Tech., M. Arch., O.A.A.  
 122 Bridge Street, Almonte, ON  
 613-256-5213

Project Title:  
 Proposed Apartment Building  
 391 Hilversum Way, Carp, ON

Drawing List:  
 Ground Floor Plan

Job No.:	2404	Drawing No.:	<b>A1</b>
Scale:	As Noted		
Date:	Nov 03-2025		
Drawn By:	Reviewed By:		
	PM	PM	



Denotes Shared Amenity Space of 128 m<sup>2</sup> [1,378 ft<sup>2</sup>]

# Ground Floor Plan

No.:	Issued For:	Date:
1	For SPA	Nov 03-2025

It is the responsibility of the appropriate Contractor to verify all dimensions on site and report all errors and/or omissions to the Architect.

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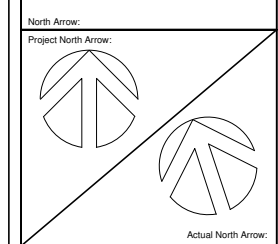
Metric Scale Drawing: All measurements are in millimeters (mm) unless otherwise noted.

Symbol Legend:



## Second Floor Plan

Denotes Accessible Unit



Architect:  
  
**Peter Mansfield, Architect**  
B. Tech., M. Arch., O.A.A.  
122 Bridge Street, Almonte, ON  
613-256-5213

Project Title:  
Proposed Apartment Building  
391 Hilversum Way, Carp, ON

Drawing List:  
Second Floor Plan

Job No.:	2404	Drawing No.:	<b>A2</b>
Scale:	As Noted		
Date:	Sept 03-2025		
Drawn By:	Reviewed By:		
	PM	PM	

No.:	Issued For:	Date:
1	For SPA	Nov 03-2025

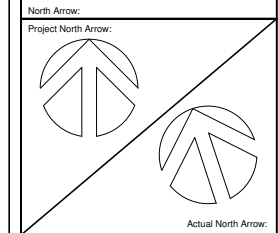
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Symbol Legend:



Architect:

**Peter Mansfield, Architect**  
 B. Tech., M. Arch., O.A.A.  
 122 Bridge Street, Almonte, ON  
 613-256-5213

Project Title:  
 Proposed Apartment Building  
 391 Hiversum Way, Carp, ON

Drawing List:  
 Third Floor Plan

Job No.:	2404	Drawing No.:	<b>A3</b>
Scale:	As Noted		
Date:	Nov 03-2025		
Drawn By:	Reviewed By:		
	PM	PM	



# Third Floor Plan