



## **DRAFT PLANNING RATIONALE**

**Proposal:**

Site Plan Control Application to Permit a Mid-rise Mixed-use Development

**Location:**

91 & 93 Holland Avenue

**Prepared for:**

Nicholson Gluckstein Lawyers  
249 McLeod Street  
Ottawa, ON K2P 1A1

**Prepared by:**

Stantec Consulting Ltd.  
400-1331 Clyde Avenue  
Ottawa, ON K2C 3G4

**File: 160410335**

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# 1. OVERVIEW

This planning rationale has been prepared in support of a site plan control application on behalf of Nicholson Gluckstein Lawyers (owner) for a proposed redevelopment of the properties described municipally as 91 and 93 Holland Avenue (the site).

The above application is in support of a proposed mixed-use development consisting of a single mid-rise building and exterior landscaping elements. The majority of the proposed building will be for residential use, with the ground floor to provide a single commercial tenancy, with the intent for it to accommodate one of the site's existing restaurant operations. Limited parking for the development will be provided below-grade, with direct garage access to Holland Avenue. The intended full build-out of the site is summarized below:

- Building Height 19.3 m (6 storeys plus mechanical penthouse/rooftop access)
- Land Use(s) Mixed Use (residential and one ground-floor commercial unit)
- Residential Units 32 dwelling units (rental units of varying size)
- Residential GFA 1,990.88 m<sup>2</sup>
- Non-residential GFA 101.62 m<sup>2</sup>
- Vehicle Parking 4 visitor stalls
- Bicycle Parking 28 stalls



FIGURE 1. ARCHITECTURAL BUILDING RENDER (CHMIEL ARCHITECTS)

It is our professional opinion that the proposed development represents good land use planning that is in the public interest, is consistent with the policies of the Provincial Policy Statement 2020,

and is in conformity and compliance with the general intent and purpose of the City of Ottawa's Official Plan and zoning by-law. It is noted that the proposal will require zoning relief from at least two design related performance standards, and that the applicant intends to address this through the minor variance process once the plans are no longer expected to change through the site plan control process.

This report and the supporting material included with the application submission, demonstrate that the proposed development and requested approvals are appropriate for facilitating a desirable and efficient use of underutilized land intended for mixed-use intensification.

## 2. CONTEXTUAL ANALYSIS

### 2.1 SURROUNDING CONTEXT

The site is located within Ward 15 – Kitchissippi, which is within the City's Urban Policy Area. More specifically, the site is located in the Hintonburg neighbourhood of the City, and is situated approximately 70 metres north of the intersection of Holland Avenue and Wellington Street West.

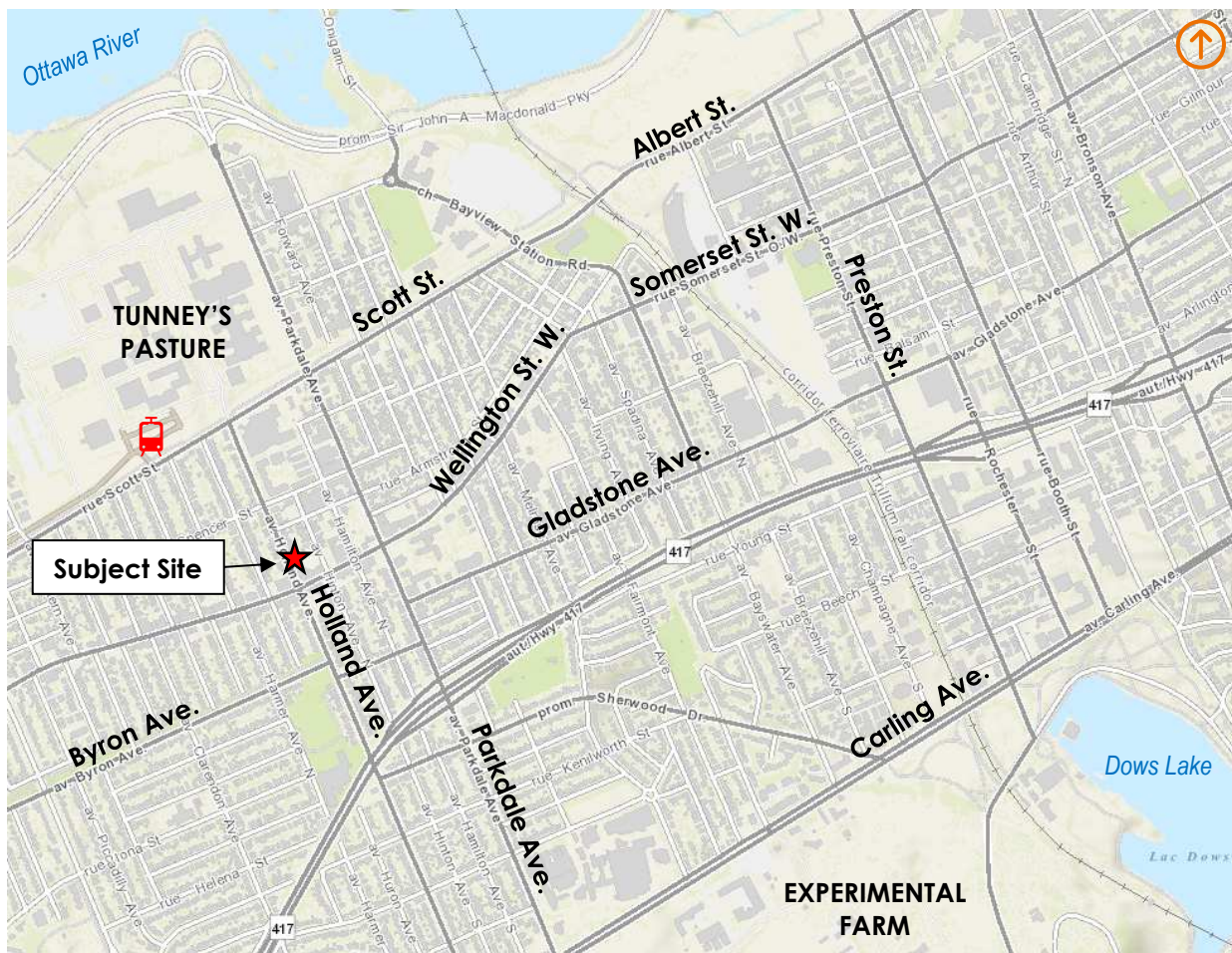


FIGURE 2. LOCATION PLAN (GEOOTTAWA)

The segment of Holland Avenue where the site is located has a distinct character that extends between Wellington Street West and Scott Street. This character consists of a mix of land uses and built-form, and is reflective of the function and overarching mainstreet and mixed-use centre Official Plan policies and zoning regulations that apply to the corridor. The below description and figures provide additional detail on the immediate and surrounding context:

- a. **North of the site and along the east side of Holland Avenue** are a number of low-rise commercial uses extending to the end of the block bound by Armstrong Street. The two subsequent blocks consist of a mix of low to mid-rise apartment buildings and non-residential uses, followed by Scott Street, Tunney's Pasture Transit Station, and the Tunney's Pasture government employment campus.
- b. **North of the site and along the west side of Holland Avenue** is low-rise housing of mixed architectural character that extends northward to Scott Street. The housing along this side of Holland Avenue is varied, with a mix of singles and multi-unit typologies.
- c. **East** of the site is 96, 92 and 88, and 84-86 Hinton Avenue North, abutting properties that are being redeveloped with a 6-storey mixed-use building to consist of 134 dwelling units, ground-floor commercial space, and 13 surface parking spaces.
- d. **South** of the site is an 8-storey bar-shaped apartment building followed by a 12-storey mixed-use building at the corner of Holland Avenue and Wellington Street West, which contains the Irving Greenberg Theatre Centre within the podium.
- e. **West** of the site is Holland Avenue, a four lane Major Collector with a protected right-of-way (ROW) of 26 metres. The existing ROW consists primarily of roadway, with limited public realm space and a pattern of small front yard setbacks on abutting private properties. To the west of Holland Avenue is low-rise housing of mixed typologies.



FIGURE 3. SURROUNDING CONTEXT (GOOGLE EARTH)

## 2.2 SITE CONTEXT

The site is made up of two rectangular-shaped properties with frontage along the east side of Holland Avenue. The site contains two restaurant buildings and a driveway along the south property line which leads to rear yard surface parking. The site consists of the following details:

**Area:** ± 724.77 m<sup>2</sup> (0.072 ha / 0.179 ac)

**Frontage:** 22.86 m on Holland Avenue (Major Collector subject to a Protected ROW of 26 m)

**Legal Des.:** Lot 1539 and Part of Lot 1537, Registered Plan 157, City of Ottawa  
PINs 04035 – 0013 & 04035 – 0012

**Encumbrance:** Part 2, Plan 4R-30448 subject to Right of Way as in INST. N616387

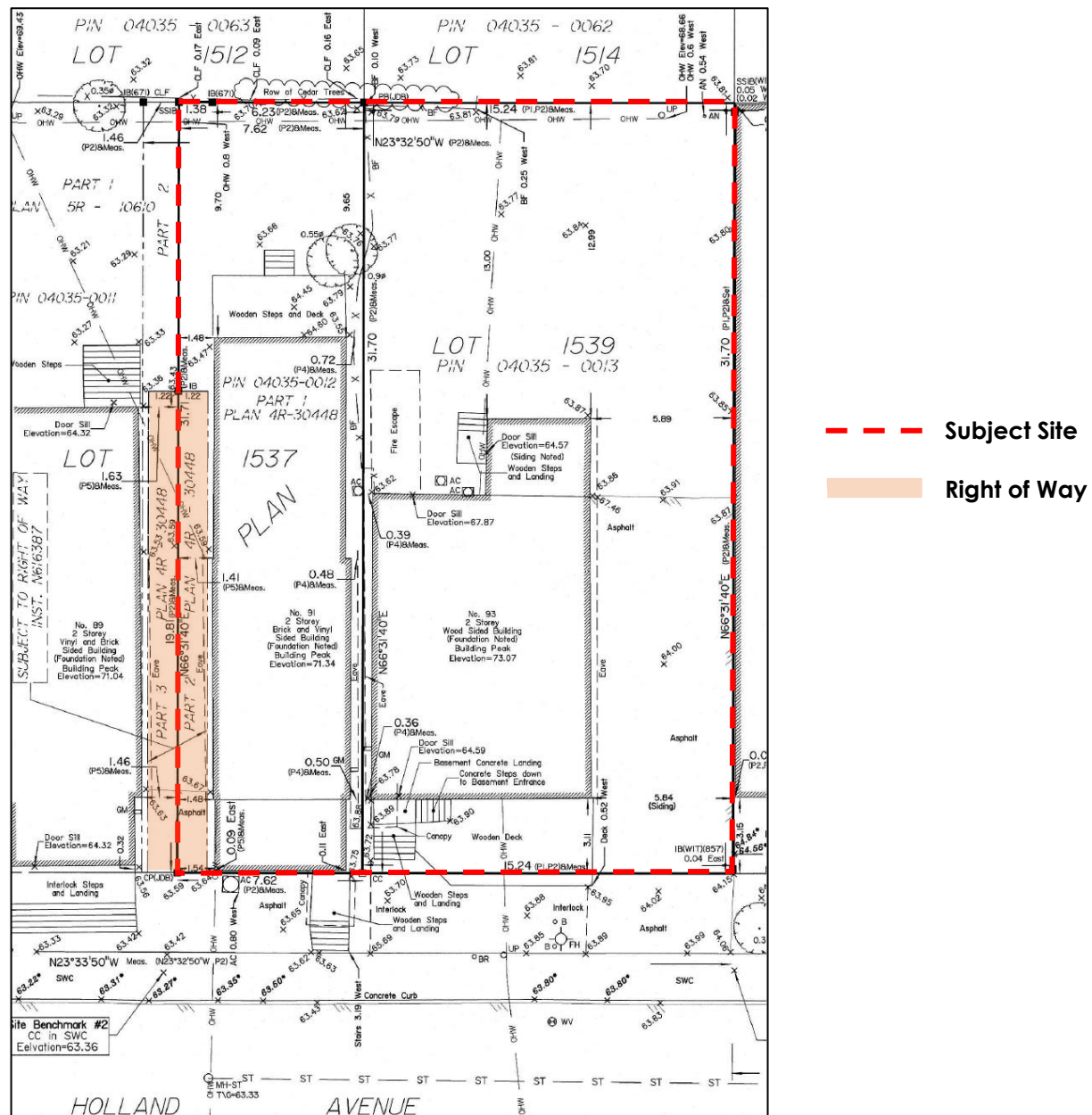


FIGURE 4. SURVEY PLAN EXTRACT (ANNIS, O'SULLIVAN, VOLLEBEKK LTD.)



FIGURE 5. SITE CONTEXT (GEOOTTAWA, 2019)

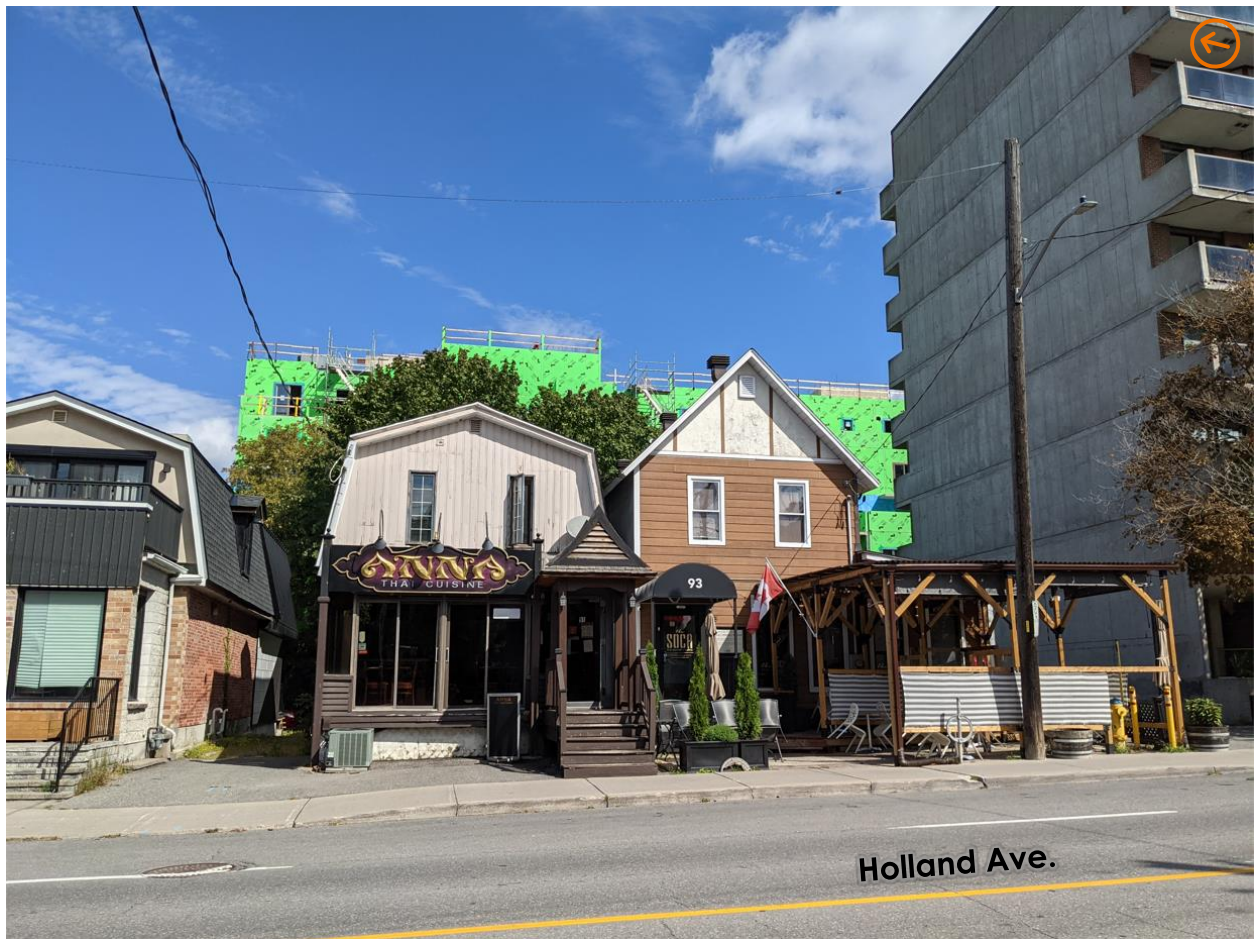


FIGURE 6. STREETVIEW OF THE SITE FACING EAST (SEPTEMBER, 2021)



- Building Height 19.3 m (6 storeys plus mechanical penthouse/rooftop access)
- Land Use(s) Mixed Use (residential and one ground-floor commercial unit)
- Residential Units 32 dwelling units (mix of 1 and 2 bedroom layouts)
- Residential GFA 1,990.88 m<sup>2</sup>
- Non-residential GFA 101.62 m<sup>2</sup>
- Vehicle Parking 4 stalls (2 reserved as visitor stalls, as required by zoning)
- Bicycle Parking 28 stalls

The below figures provide additional context to the proposal and demonstrate how it will work and fit on the site and within the context of the streetscape.



FIGURE 8. STREET-LEVEL RENDER (CHMIEL ARCHITECTS)

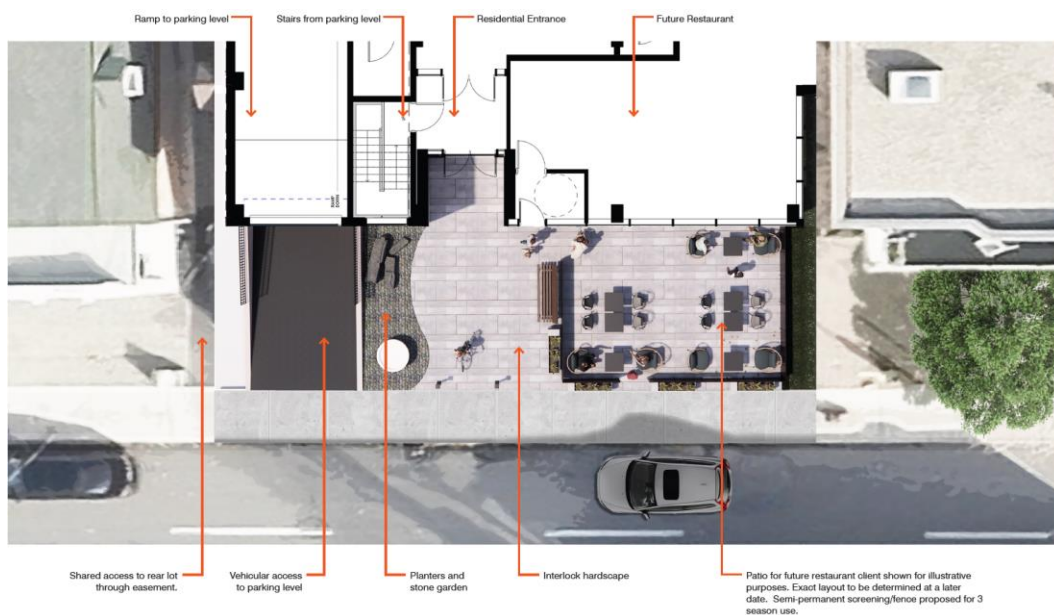


FIGURE 9. FRONT YARD FUNCTIONALITY (CHMIEL ARCHITECTS)



FIGURE 10. BIRDS EYE RENDER (CHMIEL ARCHITECTS)



FIGURE 11. STREET-LEVEL RENDER (CHMIEL ARCHITECTS)

## 4. POLICY REVIEW AND JUSTIFICATION

The following section consists of a policy review and corresponding rationale in support of the proposed development. The review covers the Provincial Policy Statement 2020, the City of Ottawa Official Plan, Zoning By-law 2008-250, and applicable urban design guidelines. The policy review also has consideration for the draft New Official Plan.

### 4.1 PROVINCIAL POLICY STATEMENT 2020

The Provincial Policy Statement 2020 (PPS) provides policy direction on planning matters in the Province of Ontario, and is issued under Section 3 of the Planning Act. This direction is for the planning of strong, sustainable, and resilient communities for all people, for clean and healthy environments, and for strong and competitive economies. Decisions affecting planning matters shall be consistent with the policies of the PPS.

The below review demonstrates that the proposed development is consistent with the applicable policies of the PPS.

#### 4.1.1 Section 1.0: Building Strong Healthy Communities

**Section 1.1.1** of the PPS sets out the criteria whereby healthy, livable, and safe communities are sustained. In short, these include: promoting efficient development and land use patterns; accommodating an appropriate affordable and market-based range and mix of residential types; avoiding development and land use patterns which may cause environmental or public health and safety concerns; promoting cost-effective development patterns such as transit-supportive intensification; improving accessibility by identifying, preventing, and removing land use barriers; ensuring there is necessary infrastructure; promoting development and land use patterns that conserve biodiversity; and, preparing for the regional and local impacts of a changing climate.

**Section 1.1.3.2** of the PPS states that land use patterns within settlement areas shall be based on densities and a mix of land uses which efficiently use land and resources, support active transportation, and are transit-supportive.

**Section 1.1.3.3** of the PPS states that Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated [...].

**Section 1.1.3.6** of the PPS states that new development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure, and public service facilities.

**Section 1.4.1** of the PPS states that planning authorities shall provide for an appropriate range and mix of housing types and densities required to meet projected requirements of current and



future residents of the regional market area, and that such housing types and densities be directed towards lands that are suitably zoned and serviced to accommodate them.

**Section 1.4.3** of the PPS states that planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by: directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs; and, promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed.

**Section 1.5.1** of the PPS states that healthy, active communities should be promoted by: planning public spaces to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity.

**Section 1.6.6** of the PPS states that planning for sewage and water services shall direct and accommodate expected growth or development in a manner that promotes the efficient use and optimization of existing municipal sewage services and municipal water services.

**Section 1.6.7.4** states that a land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.

**Section 1.6.8.3** of the PPS states that Planning authorities shall not permit development in planned corridors that could preclude or negatively affect the use of the corridor for the purpose(s) for which it was identified. New development proposed on adjacent lands to existing or planned corridors and transportation facilities should be compatible with, and supportive of, the long-term purposes of the corridor and should be designed to avoid, mitigate or minimize negative impacts on and from the corridor and transportation facilities.

**Section 1.7.1** of the PPS states that long-term economic prosperity should be supported by: promoting opportunities for economic development and community investment-readiness; and, where possible, enhancing the vitality and viability of downtowns and mainstreets.

The proposed mixed-use development is consistent with and supportive of the above policy statements of Section 1.0, as it will help to support and contribute to the following:

- encourage healthy and efficient modes of transportation, as well as a reduction in personal vehicle trips by providing the minimum amount of required vehicle parking and close to one (1) bicycle stall per unit. This objective is further supported through the site's proximity to an abundance of complementary uses and infrastructure (i.e., along a transit and cycling route;



approximately 400 metres from Tunney's Pasture Transit Station and government employment campus; less than 100 metres from Wellington Street West, a significant east-west traditional mainstreet consisting of various uses/services);

- support the viability of surrounding uses by expanding and diversifying the local consumer base with the provision of high-density housing with a range in unit sizes;
- contribute to the supply of rental apartments to address the City's housing shortage;
- Support the City's substantial investment in rapid transit by proposing high-density mixed use in proximity to an LRT station; and,
- provide an appropriate and desirable renewal of serviceable land within an area identified for mixed-use intensification.

#### **4.1.2 Section 2.0: Wise Use and Management of Resources**

Section 2.0 of the PPS states that Ontario's long-term prosperity, environmental health, and social well-being depend on conserving biodiversity, protecting the health of the Great Lakes, and protecting natural heritage, water, agricultural, mineral and cultural heritage and archaeological resources for their economic, environmental, and social benefits.

The subject site is not anticipated to contain any of the above noted resources, as it has been fully developed and redeveloped since at least 1928 as shown in City of Ottawa aerial imagery. The site has limited vegetation (i.e., two rear yard trees) and consists of building and hard surfaces. Relevant studies and plans, such as a geotechnical study and a site servicing and stormwater management report (including erosion and sediment control drawing), have been provided in support of the proposed site plan control application.

#### **4.1.3 Section 3.0: Protecting Public Health and Safety**

Section 3.0 of the PPS states that development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.

In accordance with supporting plans and studies, the proposal is not anticipated to result in any unacceptable risk to public health and safety as long as technical recommendations are followed by the applicant. The required submissions include a site servicing and stormwater management report which reviews potential storm events. The report findings will be reviewed and further discussed, if necessary, through consultation with City of Ottawa and Rideau Valley Conservation Authority. A Phase II ESA was also completed for the site and recommended that soil remediation be carried out to support the required Record of Site Condition (RSC).

The proposal will introduce a desirable high-density mixed-use development to the site, which among other benefits, will contribute to housing stability, economic vitality, and the efficient use of land and support of public infrastructure. For these reasons, and the ones noted in the above review, the proposal is consistent with the policies of the PPS 2020.

## 4.2 OFFICIAL PLAN

The City of Ottawa Official Plan is the primary long-term policy document that guides growth and development within the City. The current Official Plan was enacted in 2003 and has since gone through a number of major updates to comply with legislative requirements and to adapt to evolving context at a provincial-wide, city-wide, and community-wide scale. The City is currently in the process of preparing a new Official Plan, with Council approval targeted for late 2021.

The proposal does not require an amendment to the Official Plan, with the below policy review demonstrating how it conforms to the general intent and purpose of the in-force Official Plan.

### 4.2.1 Section 3: Designation and Land Use

The subject site is designated Mixed Use Centre on Schedule B of the Official Plan.

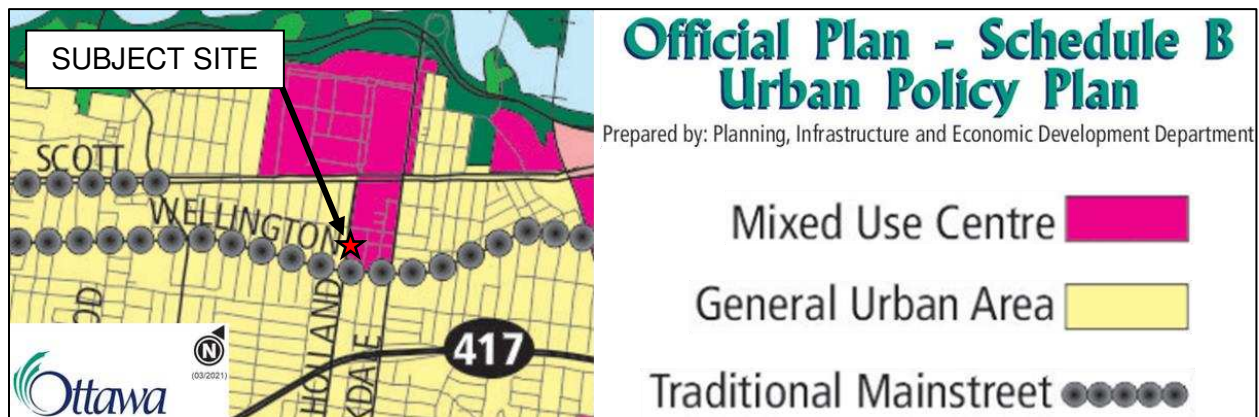


FIGURE 12. EXCERPT OF SCHEDULE B OF THE OFFICIAL PLAN

Section 3.6.2 - Mixed Use Centres and Town Centres of the Official Plan provides the following description for the designation:

*Town Centres and Mixed Use Centres occupy strategic locations on the Rapid-Transit network and act as central nodes of activity within their surrounding communities and the city as a whole. These centres are a critical element in the City's growth management strategy, being areas with potential to achieve high densities and compact and mixed-use development oriented to rapid transit. More jobs and housing at these locations will increase transit ridership and draw more commuter travel to these locations.*

The proposed mid-rise mixed use development conforms to the general intent and purpose of this land use designation, as it will provide a zoning compliant redevelopment of an underutilized site within proximity to rapid transit and employment lands. The site is located along a major collector road serviced by public transit and identified as a cycling spine route on Schedule C of the Official Plan. The site is also situated approximately 400 metres south of Tunney's Pasture Transit Station and government employment campus, and approximately 75 metres north of the Wellington Street West mixed-use corridor.

Section 3.6.2 of the Official Plan provides policy direction for Mixed Use Centres, with the below policies being relevant to the proposal.

### **Policies**

- 5.** *Mixed Use Centres will permit a broad variety of land uses at transit-supportive densities, such as offices, secondary and post-secondary schools, hotels, hospitals, large institutional buildings, community recreation and leisure centres, daycare centres, retail uses, entertainment uses, services (such as restaurants), high- and medium-density residential uses and mixed-use development containing combinations of the foregoing [...].*

The proposed land uses, being mid-rise apartment dwelling and a ground-floor commercial tenancy (i.e., restaurant), are permitted uses of the Mixed Use Centre designation.

- 9.** *All development applications and Community Design Plans for Mixed-Use Centres will be reviewed in the context of this Plan and in particular:*

- a.** *Will be evaluated in the context of the Design Objectives and Principles in Section 2.5.1 and the criteria set out in Section 4.11, particularly with regard to achieving a compact, mixed-use, transit-oriented, pedestrian-friendly environment and creating a place with visual interest;*  
**b.** *Will, where possible, contribute to a range of housing options in the area.*

The proposed development conforms to the applicable policies of Section 2.5.1 and 4.11 as demonstrated later in this section of the report. The proposal provides a range in one and two bedroom housing options.

- 11.** *Plans, public works and development proposals for Mixed-Use Centres will enhance opportunities for walking, cycling and transit and in particular will:*

- a.** *Give priority to walking and cycling in public rights-of-way;*  
**b.** *Provide direct, barrier-free connections for pedestrians and cyclists linking transit and other developments in the Mixed Use Centre along public rights-of-way, off-road pathways and open space connections*

The proposal provides close to one (1) bicycle parking space per unit and includes only four parking spaces, which will be located in the below-grade level of the development, and will ensure zoning compliance as two visitor stalls are required. The ground-level will consist, among other elements, of the primary building access and the vehicle ramp to the below-grade level, which includes a room for bicycle parking. Together, these features provide a direct connection to the public realm and street.

- 13.** *In order to demonstrate its commitment to development within Mixed-Use Centres, the City will consider them to be priority locations for:*

- b.** *The assembly of land to ensure an adequate supply that is strategically located for redevelopment or community improvement purposes;*



The proposed redevelopment, which is in keeping with the direction and objectives for lands designated Mixed Use Centre, has been made possible through a land assembly of 91 and 93 Holland Avenue.

The proposal conforms to the applicable policies of Section 3.6.2 of the Official Plan, and supports the City's objectives for appropriately scaled intensification in proximity to rapid transit.

#### **4.2.2 Section 2.2.2: Managing Intensification within the Urban Area**

Section 2.2.2 states that the Official Plan supports intensification throughout the urban area where there are opportunities to accommodate more jobs and housing and increase transit use. Intensification is directed to Target Areas for Intensification which have the potential to develop at moderate to high densities in a compact form. Lands designated Mixed Use Centre are included under Policy 3 of Section 2.2.2 of the Official Plan as Target Areas for Intensification, especially when in proximity to the transit priority network, as is the case with the subject site. The following policies of Section 2.2.2 speak to intensification and are relevant to the proposal.

##### ***Policies***

*5. Minimum density targets, expressed in jobs and people per gross hectare, are set out in Figure 2.3 and applied to those target areas with the greatest potential to support the Rapid Transit and Transit Priority Networks.*

- *Figure 2.3 - Tunney's-Quad Density Target: 500*

*6. All new development within the boundaries of the intensification target areas listed in Figure 2.3 will be required to meet the minimum density targets. [...].*

The proposal is for a mixed-use mid-rise development consisting of 32 residential dwelling units on a site of approximately 0.072 hectares in size. With a mix of single (10) and two bedroom (22) units capable of accommodating ±86 residents, the proposed development achieves the target density of 500 people per gross hectare.

#### **4.2.3 Section 2.5.1: Designing Ottawa**

The proposal conforms to the applicable design and compatibility policies of Section 2.5.1 of the Official Plan. This Section of the Official Plan provides general direction, through City-wide objectives and high-level policies, for the implementation of quality urban design within target areas. The intent of these objectives and policies is to ensure that areas targeted for growth can appropriately evolve over time while ensuring a compatible relationship with the established surrounding character. Specifically, this Section states the following with respect to growth and design:

*As the City grows and changes over time, design of these elements should work together to complement or enhance the unique aspects of a community's history, landscape and its culture. Encouraging good urban design and quality and innovative architecture can also stimulate the*



*creation of lively community places with distinctive character that will attract people and investment to the City.*

*[...] The objectives of this Plan are to direct growth and intensification to identified rapid transit and transit priority locations and to develop those areas with a mix of uses and a greater focus on active transportation. Good urban design is critical in making these places enjoyable places to live, work and socialize. Many of these areas contain existing communities. New development must enhance the existing character and the way they function*

Section 2.5.1 provides the following design objectives:

- *To enhance the sense of community by creating and maintaining places with their own distinct identity;*

The proposed building design consists of an architectural expression (i.e., segmented projections, mixed materiality, ample glazing) that will complement the existing mixed character along this stretch of Holland Avenue for an appropriate fit into the streetscape.

- *to define quality public and private spaces through development;*

The front yard of the proposed development will have a seamless connection to the public realm, and will consist of a mix in hard landscaping elements and temporary plantings to animate the ground-floor commercial use (i.e., restaurant with a seasonal outdoor patio) and to highlight the principal entrances to the residential and commercial uses of the building.

- *to create places that are safe, accessible and are easy to get to, and move through;*

The proposed building addresses the public realm with a relatively consistent streetwall composed primarily of glazing. This design provides open and accessible spaces while supporting clear sight lines. Access from the sidewalk to the principal entrances of the apartment building and commercial use will consist of barrier-free interlocking hardscape.

- *to ensure that new development respects the character of existing areas;*

Between Wellington Street West and Scott Street, the west side of Holland Avenue consists primarily of low-rise residential building types, while the east side has a consistent mix of restaurants, offices, and low to high-rise apartments. Mid-rise buildings abut the site to the east and south, and the proposed six storey building will provide an appropriate transition in scale from the corner at Wellington Street West (12 storeys) to the mid blocks towards Scott Street, which is where established and proposed building heights begin to rise again as a block bookend. The proposed uses and design respect and contribute to the character of the existing area.

- *to consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice;*



Through the proposed redevelopment of the site, the property will evolve from two ground-oriented commercial uses with rear yard parking to a well-utilized mixed-use site providing high density rental housing and ground floor commercial space. A portion of the front yard will consist of a seasonal outdoor patio with opportunity for the space to evolve over time.

- *to understand and respect natural processes and features in development design; and,*  
With the exception of two rear yard trees to be removed to accommodate the proposed building, the site does not contain any natural features. The proposed development will include landscaping elements in the front yard, and will have portions of greenspace on the rooftop terrace.
- *to maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.*

The proposal will introduce a high-density mixed use development on an underutilized lot within an area that is designated to accommodate intensification. This will result in an energy efficient redevelopment as it utilizes existing and proposed municipal infrastructure and supporting transit services. Design elements of the proposal include a large rooftop amenity area and green roof space. Inaccessible and mechanical portions of the roof are intended to utilize white ballast / membranes to reflect the heat from the conditioned space, while roof paver colour will be selected to balance light reflectivity with occupant comfort. The above design elements will help to offset the urban heat island effect.

The owner intends to explore the feasibility of additional sustainability approaches (i.e., grey water recycling and specialized glazing) as the proposal progresses through the required planning approval and permitting processes.

As demonstrated by the architectural submissions and the contents of this report, the proposal is in general conformity with the design objectives of Section 2.5.1.

#### **4.2.4 Section 4.11: Urban Design and Compatibility**

The proposal conforms to the applicable policies of Section 4.11 of the Official Plan. This Section contains a number of design policies that seek to address the matter of compatibility between new and existing development. This relationship is primarily assessed by reviewing design elements relating to built form and functionality.

In the context of the proposal, the purpose of the policies of Section 4.11 are to set the stage for requiring design excellence, as the subject site is located within a Design Priority Area. Accordingly, the design and compatibility of the proposal will be evaluated in the context of the design objectives of Section 2.5.1, which were addressed in the previous section of this report. Section 4.11 lists the following over-arching design themes to be addressed, where applicable, through proposed development:



- Views
- Building design
- Massing and scale
- High-rise buildings
- Outdoor amenity areas
- Public art
- Design priority areas
- First Nations Peoples design interests

As demonstrated by the architectural submissions and the contents of this report, the proposal is in general conformity with the policies of Section 4.11 by contributing to, providing, or maintaining the following:

- contributing a high-density mixed-use development to lands designated Mixed Use Centre and intended for such forms of intensification
- supporting the City's investment in rapid transit by locating housing, employment, and services in proximity to the Tunney's Pasture Transit Station;
- supporting the City's investment in infrastructure such as piped services and the pedestrian network by diversifying and intensifying the use of an underutilized site;
- providing storage space, secure bicycle parking, and amenity space for tenants;
- incentivizing reduced automobile use by providing minimal vehicle parking (four visitor stalls) and close to one (1) bicycle parking stall per unit;
- proposed building location and setbacks that allow it to address the street and contribute to a comfortable public realm that has a well-defined separation of public and private space;
- providing a ground floor non-residential unit with of a size and layout that allows opportunity for tenants and their space to evolve over time to adjust to market changes;
- working with existing grades and accessibility features to achieve a public realm that is human-scaled, safe, well-animated and landscaped, and accessible for all people;
- introducing a building height and massing that is aligned with the mid-rise vision of the Official Plan and Wellington Street West Secondary Plan for this area, and that is sensitive to the surrounding uses and built-form;
- orienting and articulating the building facade to have the effect of reducing visual mass and cumulative sun shadowing, and reducing potential impacts relating to microclimates at the ground level and at amenity areas; and,
- introducing a building form and materiality that will enhance visual interest and positively contribute to the streetscape along Holland Avenue.

The proposed development conforms to Section 4.11 of the Official Plan, and will function as an example of a quality mixed-use development that contributes to the evolving character of this portion of Holland Avenue towards the Official Plan's vision for lands designated Mixed Use Centre.



### 4.3 DRAFT NEW OFFICIAL PLAN

The City is in the process of finalizing a new Official Plan, with the intent for it to be approved by Council in the fall of 2021 followed by Ministerial approval in early 2022. Although the new Official Plan is in draft form and has yet to receive Council and Ministerial approval, the following provides an overview of how the proposal conforms to the general intent and purpose of the draft new Official Plan.

#### Schedule A - Transect Policy Areas

- Transect Policy Area: Outer Urban
  - *Section 5.2.1 Enhance or establish an urban pattern or built form, site design and mix of uses*  
The proposal conforms to the policies of Section 5.2.1 as it provides a mid- to high-density mixed-used development.
  - *Section 5.2.2 Prioritize walking, cycling and transit within, and to and from, the Inner Urban Transect*  
The proposal conforms to the policies of Section 5.2.2 as it prioritizes active and public modes of transportation, and will contribute to the vibrancy of the public realm.
  - *Section 5.2.3 Provide direction to the Hubs and Mainstreet Corridors located within the Inner Urban Transect*  
The proposal conforms to the policies of Section 5.2.3 as it consists of a permitted mid-rise building height.

#### Schedule B3 - Outer Urban Transect

- Designation: Hub
- Overlay: Evolving Neighbourhood  
The proposal conforms to the policies of Section 6.1 as it will provide a transit-supportive mixed-use development (high density residential and ground-floor commercial) that prioritizes pedestrians and enhances the streetscape while maintaining compatibility with adjacent uses and built form.

#### Schedule C1 - Protected Major Transit Station Areas (PMTSA)

- PMTSA: South Keys  
The proposal conforms to the policies of Section 6.1.2 as it will provide a transit-supportive mixed-use development (high density residential and ground-floor commercial with appropriate lot coverage) that prioritizes pedestrians and enhances the streetscape while maintaining compatibility with adjacent uses and built form.

The proposal conforms to the general intent and purpose of the draft new Official Plan.



#### 4.4 WELLINGTON STREET WEST COMMUNITY DESIGN PLAN

The site is subject to the Wellington Street West Community Design Plan (CDP), which was completed in 2011, and is a Council-approved design and visioning document that does not have the same statutory authority under the Ontario Planning Act as the Official Plan. These documents are intended to guide change in areas of the City that are targeted for growth and improvement as directed by the Official Plan. Typically, the guidance provided in CDPs translates into Secondary Plans that form part of the Official Plan, such as the Wellington Street West Secondary Plan which is reviewed in the following section.

The CDP designates the site as Mixed Use Centre, and contains the following description for such lands:

*Lands between Holland and Parkdale Avenues, north of Wellington Street to Scott Street, are designated “Mixed-Use Centre” (MUC). This CDP considers MUC lands between Wellington and Spencer Streets (See Figure 1-A Study Area). Mixed-use designation applies to lands that have been identified as strategic locations on rapid-transit networks and adjacent to major roads, with good accessibility by walking, cycling and automobile.*

*They are focal points of activity and diversity through careful attention to design, orientation, and mix of uses. In brief, MUC designations will:*

- encourage a broad variety of transit supportive uses*
- optimize land use through compact mixed-use development*
- enhance opportunities for walking, cycling and transit*
- provide opportunities, when possible, for a variety of activities*

Section 3.5 Parkdale Park Area contains policies relevant to the proposal, as policies under this section relate to the Mixed Use Centre lands that include the subject site along the east side of Holland Avenue. Specifically, section 3.5.4 Design Policies contains built form policies and states the following with respect to the Mixed Use Centre lands:

#### **A8. Building envelopes in Mixed Use Centre area (MUC)**

*A8a. MUC area bounded by Spencer/Wellington/Holland/Parkdale. Redevelopment on these blocks fronting onto the public streets will have a minimum ground floor setback of 2.0m and minimum building setback of 2m above the 3rd storey, when building height is over 4 storeys. An attractive, pedestrian-scaled building base, streetscape, and appropriate street-to-height ratio close to 1:1, for example, are important features in the future of this part of the mixed use area.*

The proposed design includes an approximately 3 metre ground floor setback, and consists of segmented massing along the front facade which has the effect of achieving a visually appealing architecture while maintaining an appropriately human-scaled base from the street. This architectural approach includes projecting and stepped-back portions of the front wall to achieve the intended effect of a 2 metre setback above the lower storeys. With respect to proposed



building height and a 1:1 street-to-height ration, the existing Holland Avenue right-of-way adjacent to the site is approximately 24.5 metres, whereas the proposed building is approximately 19.3 metres, with an upper roof for amenity access and mechanical elements at 22 metres.

#### **A11. Land use specifications in the Mixed Use Centre designation**

*A11a. The ground-floor space of new infill development, such as retail or restaurant space, should be designed to be adaptable, for example, providing between 170m<sup>2</sup> to 200 m<sup>2</sup> maximum of GFA per unit size. This will ensure smaller size storefronts that are more reflective of the mainstreet building format; one with proven long-term adaptability and active frontages.*

The proposed ground floor area of the single non-residential unit is appropriate and flexible for supporting small-scale commercial operations, and is intended to accommodate one of the site's existing restaurant uses. This space is approximately 101.62 m<sup>2</sup> in size and includes exterior space for an outdoor commercial patio.

*A11b. Land uses will follow those permitted by a Mixed Use Centre designation [...].*

The proposed land uses, being mid-rise apartment and a commercial tenancy, are permitted uses in the Mixed Use Centre designation.

The proposed mixed-use development conforms to the general intent and purpose of the Wellington Street West Community Design Plan.

## **4.5 WELLINGTON STREET WEST SECONDARY PLAN**

The site is subject to the Wellington Street West Secondary Plan (SP), which is a subsidiary plan that serves to provide strategic planning direction to guide future growth and redevelopment of lands that are along and in close proximity to Wellington Street West.

Schedule A - Land Use of the SP designates the site as Mixed Use Centre, which reflects the site's land use designation from Schedule B of the parent Official Plan. The below SP excerpts are relevant to the proposal, and consist of general and area-specific policies.

Section 11.3.1 General Mainstreet Policies states the following:

### **General**

*3. New buildings over four (4) storeys within the Traditional Mainstreet and Mixed-Use Centre areas shall incorporate architectural articulation and details to form a two (2) to three (3) storey base to ensure compatibility with the existing lowrise, human-scale buildings in order to be consistent with the built form vision for these areas.*

The proposed design consists of segmented massing along the front facade which has the effect of achieving a visually appealing architecture while maintaining an appropriately human-scaled base from the street. This architectural approach provides a desirable contrast from traditional podium designs consisting of flush and mundane walls, and therefore provides a compatible fit



into the evolving streetscape, which itself consists of a vibrant mix of residential architecture, especially along the west side of Holland Avenue.



FIGURE 13. PUBLIC REALM PERSPECTIVE

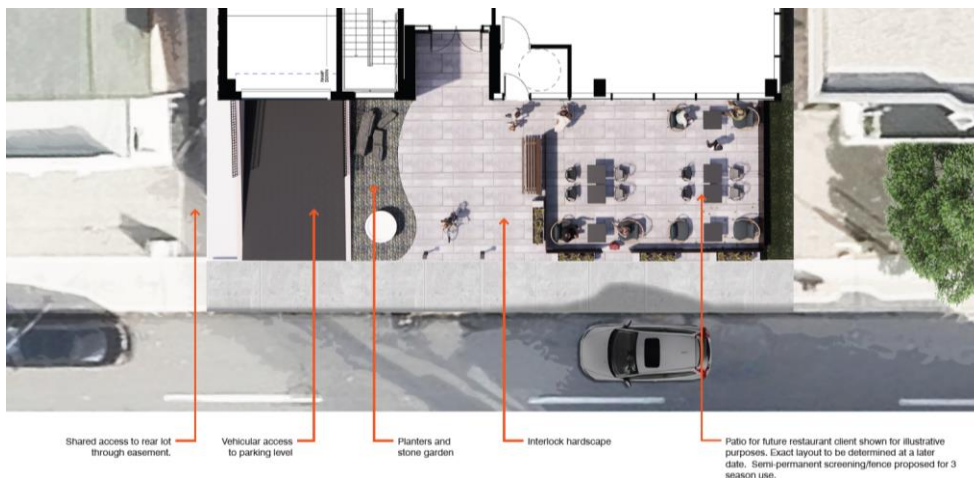


FIGURE 14. FRONT YARD DESIGN

Section 11.3.4 Parkdale Park Area Policies states the following:

### **Mixed Use Centre Area**

#### **Land Use**

7. The ground floor area of commercial uses proposed by new infill development shall be comparable to nearby mainstreet areas to support small-scale commercial operations and to discourage large format retail uses.

The proposed ground floor area of the single non-residential unit is appropriate for supporting small-scale commercial operations, and is intended to accommodate one of the site's existing restaurant uses.



The proposed mixed-use development conforms to the general intent and purpose of the Wellington Street West Secondary Plan.

## 5. URBAN DESIGN GUIDELINES

The purpose of the City's Urban Design Guidelines is to provide urban design guidance at the planning application stage in order to assess, promote, and achieve appropriate development within specified areas throughout the City. Where these guidelines apply, their objectives will not necessarily be relevant in all cases or in their entirety. Compliance with the guidelines is not a statutory requirement, as in the case of policies of the Official Plan or regulations of the zoning by-law, but instead, is encouraged to promote quality design and consistency throughout the City. The proposal is subject to the Transit-Oriented Development Guidelines, with a review of the relevant guidelines provided below.

### 5.1 TRANSIT-ORIENTED DEVELOPMENT GUIDELINES

The Transit-Oriented Development Guidelines was completed in 2007 with the purpose of providing guidance to assess, promote, and achieve appropriate Transit-oriented development within the City of Ottawa. These guidelines are to be applied throughout the City for all development within a 600 metre walking distance of a rapid transit stop or station, which includes the subject site, as it is located an approximately 400 metre walking distance from Tunney's Pasture Station.

The Transit-Oriented Development guidelines cater mostly to development within nodes surrounding transit stations, where the built environment and vision of redevelopment typically looks to achieve higher densities and more compact built form. Although the site is within a Mixed Use Centre and is in proximity to a rapid transit station, the level of intended intensification for the site, as directed by zoning and Official Plan policies, is for mid-rise redevelopment up to 6 storeys in building height. The applicable guidelines, and how the proposal has consideration for them, is detailed below.

**Guidelines 1-3:** the proposed mix of uses is transit supportive (mid-rise apartment and a ground-floor non-residential tenancy).

**Guidelines 7, 10 & 16:** the proposed building location addresses the street to better animate the public realm and to minimize walking distance to Tunney's Pasture Station and the surrounding cycling and pedestrian network.

**Guideline 8 & 28:** the proposed density and mix of land uses (ground floor non-residential) is appropriate for the location of the site and applicable Official Plan policies.

**Guideline 9:** the proposed mid-rise building provides a transition in scale in relation to the existing development to its south (mid and high-rise), east (mid-rise), and north (low-rise).



**Guideline 11, 14 & 15:** the building design provides segmented articulations along the front façade to achieve the effect of a human-scaled building base while maintaining strong visual interest complemented by its materiality and ample ground-floor glazing.

**Guideline 17 & 18:** contrasting materials are proposed for pedestrian and vehicle accesses to the site to delineate functions and enhance safety. Grades have been designed to permit barrier-free access to and from the site.

**Guidelines 29:** a ground-level commercial use is proposed and intended to accommodate one of the site's two existing restaurant operations.

**Guideline 31, 32 & 39:** minimal underground parking is proposed (4 stalls), while the number of provided bicycle parking spaces exceeds the minimum requirement.

**Guideline 48:** a ROW taking has been anticipated, and the proposed streetscape elements along the site will include a paved public sidewalk with other elements such as landscaping, lighting, etc. to be further coordinated with utilities. It is anticipated that the proposed commercial patio will require permission and an agreement to encroach into the ROW.

**Guidelines 51 & 56:** site signage and lighting to be in accordance with applicable standards and by-laws.

**Guideline 52:** where possible, ground and rooftop surfaces have been designed to be permeable and light-coloured to help reduce urban heat.

**Guidelines 54-55:** service and utility areas within the building will be shared between building uses. Utility/service equipment and functions (i.e., waste storage) have been internalized to minimize their visual impact on the street.

The proposed development provides balance between the pedestrian environment, built form (i.e., building mass and height), and land use and site functionality. The proposal will provide an appropriate degree of intensification within a Mixed-Use Centre in proximity to rapid transit and a traditional mainstreet, and has been demonstrated to have consideration for a number of applicable transit-oriented development design guidelines.

## 6. ZONING REVIEW

Under City of Ottawa Zoning By-law 2008-250 the subject site is zoned MC16 H(20) – Mixed-Use Centre Subzone 16, with a maximum height limit of 20 metres. The proposed mixed-use development complies with the general intent and purpose, the land use permissions, and the majority of the performance standards of the MC16 H(20) Zone. Zoning relief from at least two design related performance standards will be required, and the applicant intends to address this

through the minor variance process once the plans are no longer expected to change through the site plan control process.

The purpose of the MC Zone is to accommodate a combination of transit-supportive uses in compact and pedestrian-oriented mixed-use buildings. The development standards for this zone are intended to promote intensification to support the City’s applicable Official Plan policies and vision for Mixed Use Centres. The following figure and tables detail the site’s zoning and how it applies to the proposed development.

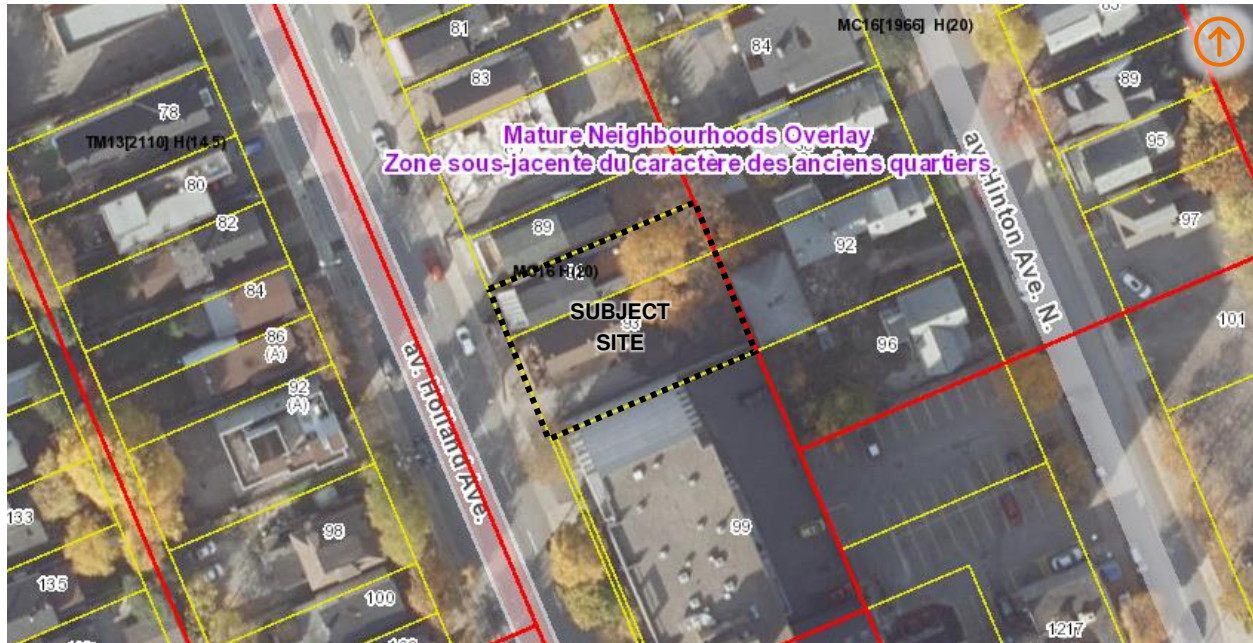


FIGURE 15. ZONING EXCERPT  
TABLE 1. MC16 H(20) ZONING MATRIX

PROVISIONS	BY-LAW SECTION	REQUIRED & PERMITTED		PROPOSED	COMPLIANCE NOTES
Minimum lot area	191(2)(a)	No minimum		±724.77 m <sup>2</sup>	Compliant
Minimum lot width	191(2)(b)	No minimum		22.86 m (Holland Avenue)	Compliant
Minimum front yard setback	191(2)(c)(iii)	No minimum		1.42 m	Compliant
Minimum ground floor setback from façade facing a public street	192(16)(d)	2 m		3.04 m	Compliant
minimum building step back above the 3rd storey when building height is over 4 storeys	192(16)(e)	2 m from a wall facing the front lot line		1.62 m (stepback consists of architectural articulations as opposed to a flush stepback)	Zoning relief required
Minimum interior side yard setback	191(2)(d)(iii)	No minimum		1.206 m (N), 0.235 m (S)	Compliant
Minimum rear yard setback	192(16)(f)	Storeys 1 to 3 of a building	3 m	5.155 m	Compliant
		Storeys 4 to 8 of a building	7.5 m	7.105 m	Zoning relief required
Maximum floor space index	191(2)(f)	No maximum		NA	Compliant
Minimum building height	191(2)(g)(ii)	No minimum		NA	Compliant
Maximum building height	191(2)(h)(iii)	20 metres		19.3 m (6 storeys plus mechanical penthouse/rooftop access)	TBD (access enclosure to be reviewed as part of the technical)

					circulation process for compliance)
Minimum width of landscaped area	191(2)(i)	No minimum, except that where a yard is provided and not used for required driveways, aisles, parking, loading spaces or outdoor commercial patio, the whole yard must be landscaped		Hard and soft landscaping	Compliant
Maximum gross floor area	192(16)(g)	per unit of retail, retail food store, personal service business, restaurant, bar, convenience store, service and repair shop, personal brewing facility	200 m <sup>2</sup>	101.62 m <sup>2</sup>	Compliant

**TABLE 2. PARKING MATRIX & DRIVE AISLE SUMMARY (SCHEDULE 1A AREA Z & SCHEDULE 1 AREA B)**

PARKING TYPE	APARTMENT, MID RISE (32 Dwelling Units) & COMMERCIAL (101.62 m <sup>2</sup> of GFA)		
	Required		Compliance
<b>Regular</b>	None (Sec.101(2))		0 Compliant
<b>Visitor</b>	0.1 per DU (first 12 DU excluded) = 2 Sec.102(2)		4 Compliant
<b>Accessible</b>	For 0-19 provided spaces = 0 By-law 2017-301: Part C, Sec. 111(2)		0 Compliant
<b>Bicycle</b>	Residential: 0.5 per DU = 16 Table 111A(b)(i)	Commercial: 1 per 250 m <sup>2</sup> of GFA = 0 Table 111A(e)	28 (indoor) Compliant
<b>Loading</b>	None Sec. 192(16)(j)		0 Compliant

**Additional Provisions & Notes**

**Drive aisles have been designed to comply with applicable provisions (By-law Sec. 107) Compliant**

- Minimum driveway width for a single traffic lane providing access to a parking garage: 3 m for a single traffic lane
- Maximum driveway width for a double traffic lane leading to a mid-rise apartment parking garage of less than 20 spaces in: 3.6 m
- Minimum aisle width for providing access to parking spaces
  - 56-90 degree parking: 6.0 m

**Bicycle parking has been designed to comply with applicable provisions (By-law Sec. 111) Compliant**

- A bicycle parking space must have access from an aisle having a minimum width of 1.5 metres.
- Where four or more bicycle parking spaces are provided in a common parking area, each bicycle parking space must contain a parking rack that is securely anchored to the ground and attached to a heavy base such as concrete.
- A minimum of 50% of the bicycle parking spaces required by this by-law must be horizontal spaces at ground level.
- Minimum bicycle parking space dimensions
  - Horizontal space: width of 0.6 m and length of 1.8 m
  - Vertical space: width of 0.5 m and length of 1.5 m

**TABLE 3. AMENITY AREA MATRIX**

PROVISION	BY-LAW SECTION	REQUIRED		PROPOSED	
		Total Amenity Area	Communal Amenity Area	Total Amenity Area	Communal Amenity Area
Amenity Area	Section 137				
	(rates are the same for both Mixed-use Buildings of 9 or more dwelling units and for mid rise apartment buildings)	6 m <sup>2</sup> per DU	A minimum of 50% of the required total amenity area	434.22 m <sup>2</sup>	183.51 m <sup>2</sup> (rooftop)
		= 6 (32 DU) = 192 m <sup>2</sup>	= 0.5 (192) = 96 m <sup>2</sup>	Compliant	Compliant
Additional provisions	i. Amenity area must be provided for a residential use that is a permitted use in the zone in which it is located, in accordance with Table 137. ii. Amenity area must be located on the same lot as the use for which it is provided. iii. Amenity area provided outdoors must not be located in a required front or corner side yard. iv. Where amenity area is located outside at grade, it may be included in the calculation of landscaped area requirements. v. Minimum required communal amenity area may only be included as part of a required landscaped buffer where it is aggregated into areas of 54 m <sup>2</sup> or more. vi. Layout of communal amenity area: aggregated into areas up to 54 m <sup>2</sup> , and where more than one aggregated area is provided, at least one must be a minimum of 54 m <sup>2</sup> .				

## 7. PUBLIC ENGAGEMENT STRATEGY

As part of the due diligence for the project, applicant-led consultation was undertaken to explore and evaluate various design options and early feedback regarding the proposal. These engagement processes included the following:

- formal pre-application consultation with City staff
- follow-up consultation with City staff
- meeting and follow-up consultation with the Councillor's office
- meeting and follow-up consultation with the Community Association

The design of the current proposal takes into account the comments and recommendations received by City planning staff, the Councillor's office, and the Community Association. Some of the primary changes to the original design include the reduction in proposed building height from 10-12 to 6 storeys, and modifications to the building design to enhance compatibility with adjacent existing development.

The following consultation processes are expected to take place as part of the Site Plan Control application process:

- Public Notice (mail/newsletter and signage) of the application
- Potential public information session chaired by the Councillor's office
- Formal review by the UDRP

## 8. SUPPORTING DOCUMENTATION

As part of a complete submission for the proposed development and requested planning approval, and through consultation with City staff, the following plans and reports have been prepared and submitted.

- |   |   |
|---|---|
| - Site Servicing and Stormwater Management Report                           | - Planning Rationale Report                       |
| - Site Servicing Plan   | - Design Brief                                    |
| - Grading and Drainage Plan (includes erosion and sediment control details) | - Site Plan                                       |
| - Geotechnical Study  | - Landscape Plan                                  |
| - Phase I ESA   | - Parking Garage Plan                             |
| - Phase II ESA  | - Tree Conservation Report                        |
| - Roadway Traffic Noise Assessment  | - Architectural Building Elevations (dimensioned) |
|   | - Survey Plan                                     |

## 9. CONCLUSION

The proposed Site Plan Control application is for the purpose of permitting the redevelopment of the properties at 91 and 93 Holland Avenue. The site consists of two older stock restaurant buildings (converted) and rear yard surface parking, with the intent for the redevelopment to accommodate one of the existing restaurants in its ground floor commercial tenancy.

The proposed redevelopment is in the form of a mixed-use mid-rise building that supports provincial and City-wide policies and objectives for well-designed intensification of underutilized land in proximity to rapid transit. As demonstrated by this report and other technical submissions

included with the required application, the proposal represents a timely, appropriate, and carefully planned development that will fit well and work well on the site and within the surrounding context.

It is our opinion that the proposal represents good land use planning that is in the public interest, is consistent with the PPS, and is in general conformity and compliance with the Official Plan and zoning by-law, save and except the minor zoning relief to be addressed by the applicant through the minor variance process. As such, we recommend the proposal for site plan control approval.

Respectfully submitted,

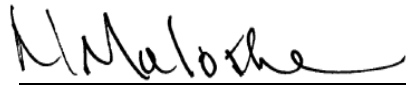
**Stantec Consulting Ltd.**

Prepared by:



**Barrett L. Wagar** RPP, MCIP, M.PL

Reviewed by:



**Nancy Meloshe** MCIP, RPP

