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Provence Orléans Subdivision Phase 5B 2128 Trim Road Ottawa, Ontario

Planning Rationale, Public Consultation Strategy,
and Urban Design Brief

Prepared for: Provence Orléans Realty Investments Inc.

**PROVENCE ORLÉANS SUBDIVISION PHASE 5B
2128 TRIM ROAD
OTTAWA, ONTARIO**

**PLANNING RATIONALE, PUBLIC CONSULTATION STRATEGY,
AND URBAN DESIGN BRIEF
IN SUPPORT OF AN APPLICATION FOR
ZONING BY-LAW AMENDMENT**

Prepared For:



Prepared By:



Engineers, Planners & Landscape Architects

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November 7, 2025

Novatech File: 117155
Ref: R-2025-76

November 7, 2025

City of Ottawa
Planning, Development, and Building Services Department
110 Laurier Avenue West, 4th Floor
Ottawa, Ontario
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Attention: Kelly Livingstone, MCIP, RPP – Planner III

**Reference: 2128 Trim Road
Provence Orléans Subdivision Phase 5B
Planning Rationale, Public Consultation Strategy, and Urban Design Brief in
Support of an Application for Zoning By-law Amendment
Approved City File No.: D07-16-22-0005, D02-02-18-0067, and D02-02-24-0034
Our File No.: 117155**

Novatech has been retained by Provence Orléans Realty Investments Inc. c/o Regional Group to prepare this Planning Rationale, Public Consultation Strategy, and Urban Design Brief in support of a *Zoning By-law Amendment* application for Phase 5B of their Provence Orléans subdivision located at 2128 Trim Road in Ward 19 – Orléans-South Navan. The herein will be referred to as the 'Subject Site'.

The *Plan of Subdivision (City File No.: D07-16-18-0021)* application received Draft Plan Approval on July 5, 2019 with a subsequent *Plan of Subdivision (City File No.: D07-16-22-0005)* application filed to extend Draft Plan Approval to July 5, 2025. The *Zoning By-law Amendment (City File No.: D02-02-18-0067)* application was approved on September 11, 2019 by Ottawa City Council and enacted as *By-law 2019-301*. A *Zoning By-law Amendment (City File No.: D02-02-24-0034)* application was approved by Ottawa City Council on October 30, 2024 to permit reduced lot widths in Provence Orléans Phase 5A.

Provence Orléans Realty Investments Inc. is proposing to rezone the Subject Site to amend provisions of *Zoning By-law 2008-250* to facilitate the construction of a new detached dwelling and standard townhome designs. The rezoning will only apply to **ten (10) detached dwelling lots** and **one (1) townhome block** within Phase 5B of the Provence Orléans subdivision and not any of the previously approved phases. No changes are being proposed to the layout or roadway configuration of the approved Draft Plan of Subdivision as part of this application.

City of Ottawa staff confirmed that to support the suggested zoning, a Studies, Plans, and Identification List (SPIL) similar to the one provided for Phase 5A which included a Planning Rationale, Public Consultation Strategy, and Urban Design Brief would be required. This report will demonstrate how the proposal is consistent with the *Provincial Planning Statement (2024)*, conforms with the *City of Ottawa Official Plan (2022)*, and complies with the provisions of *Zoning*



By-law 2008-250. This report shall be read in conjunction with the approved Planning Rationale, Urban Design Brief, and Integrated Environmental Review Statement (IERS) prepared by Novatech dated November 9, 2018 and all previously approved reports for the Provence Orléans subdivision.

Yours truly,

NOVATECH

Robert Tran, M.Pl.
Project Planner, Planning & Development

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EXECUTIVE SUMMARY

Novatech has been retained by Provence Orléans Realty Investments Inc. c/o Regional Group to prepare this Planning Rationale, Public Consultation Strategy, and Urban Design Brief in support of a *Zoning By-law Amendment* application for Phase 5B of their Provence Orléans subdivision municipally known as 2128 Trim Road in Ward 19 – Orléans-South Navan. The herein will be referred to as the ‘Subject Site’.

The Subject Site is an irregularly shaped parcel of land situated within the Orléans community within the urban boundary. Phase 5B is one of the remaining phases in an approved subdivision and includes forty-one (41) detached dwelling units and fifty-four (54) townhome dwelling units for a total of ninety-five (95) residential dwelling units. To facilitate the construction of a new detached dwelling and standard townhome design, Provence Orléans Realty Investments Inc. is proposing to rezone the Subject Site to amend provisions of *Zoning By-law 2008-250*. The rezoning will only apply to **ten (10) detached dwelling lots** and **one (1) townhome block** within Phase 5B of the Provence Orléans subdivision.

The Subject Site is currently zoned as *Residential Third Density, Subzone YY, Urban Exception 2582 – R3YY[2582]* under the *City of Ottawa’s Zoning By-law 2008-250*. The zoning of **ten (10) detached dwelling lots** will be amended from *Residential Third Density, Subzone YY, Urban Exception 2582 – R3YY[2582]* to *Residential Third Density, Subzone YY, Urban Exception 2989 – R3YY[2989]* to permit a reduction to the minimum lot width from 9.0 metres to 8.3 metres and permitting a maximum driveway area of 55% for lots that are less than 9.0 metres wide

The **one (1) townhome block** will remain zoned as *Residential Third Density, Subzone YY, Urban Exception 2582 – R3YY[2582]* with the added provision to reduce the corner side yard setback from 4.5 metres to 3.0 metres. The proposed rezoning and amended provisions will maximize the Subject Site’s land resources and contribute to additional density and housing needs within the City. No new land uses are being proposed as part of this application.

The Planning Rationale, Public Consultation Strategy, and Urban Design Brief will demonstrate how the proposed development is consistent with the *Provincial Planning Statement (2024)*, conforms to the *City of Ottawa Official Plan (2022)*, and complies with the provisions of *Zoning By-law 2008-250*.

1.0 INTRODUCTION AND PROPOSED DETAILS

Novatech has been retained by Provence Orléans Realty Investments Inc. c/o Regional Group to prepare this Planning Rationale, Public Consultation Strategy, and Urban Design Brief in support of a *Zoning By-law Amendment* application for Phase 5B of their Provence Orléans subdivision located at 2128 Trim Road in Ward 19 – Orléans-South Navan. The herein will be referred to as the ‘Subject Site’.

The *Plan of Subdivision (City File No.: D07-16-18-0021)* application received Draft Plan Approval on July 5, 2019 with a subsequent *Plan of Subdivision (City File No.: D07-16-22-0005)* application filed to extend Draft Plan Approval to July 5, 2025. The *Zoning By-law Amendment (City File No.: D02-02-18-0067)* application was approved on September 11, 2019 by Ottawa City Council and enacted as *By-law 2019-301*. A *Zoning By-law Amendment (City File No.: D02-02-24-0034)* application was approved by Ottawa City Council on October 30, 2024 to permit reduced lot widths in Provence Orléans Phase 5A.

Provence Orléans Realty Investments Inc. is proposing to rezone the Subject Site to amend provisions of *Zoning By-law 2008-250* to facilitate the construction of a new detached dwelling and standard townhome design. The rezoning will only apply to **ten (10) detached dwelling lots** and **one (1) townhome block** within Phase 5B of the Provence Orléans subdivision and not any of the previously approved phases. No changes are proposed to the layout or roadway configuration of the approved Draft Plan of Subdivision as part of this application as shown in **Appendix A**. A copy of the preliminary 4M-Plan for the Subject Site is provided in **Appendix B**.

City of Ottawa staff confirmed that to support the suggested zoning, a Studies, Plans, and Identification List (SPIL) similar to the one provided for Phase 5A which included a Planning Rationale, Public Consultation Strategy, and Urban Design Brief would be required. This report will demonstrate how the proposal is consistent with the *Provincial Planning Statement (2024)*, conforms with the *City of Ottawa Official Plan (2022)*, and complies with the provisions of *Zoning By-law 2008-250*. This report shall be read in conjunction with the approved Planning Rationale, Urban Design Brief, and Integrated Environmental Review Statement (IERS) prepared by Novatech dated November 9, 2018 and all previously approved reports for the Provence Orléans subdivision.

1.1 Site Description and Surrounding Uses

The Subject Site is situated east of Provence Boulevard and south of Béatrice-Desloges Catholic Secondary School and is the remaining phase of the Provence Orléans subdivision as shown on **Figure 1**. The previous phases of the development are at various stages including fully built out and occupied and currently under construction.

The Subject Site is legally described as follows:

PART LOT 2, CONCESSION 9 CUMBERLAND, SAVE & EXCEPT 4M1734 SUBJECT TO AN EASEMENT OVER PART 57 PLAN 4R33773 AS IN OC2364283 SUBJECT TO AN EASEMENT OVER PART 56 PLAN 4R33773 AS IN OC2364285 CITY OF OTTAWA



Figure 2: Looking east towards the Subject Site (Google Streetview, September 2025).



Figure 3: Looking south towards the Subject Site (Google Streetview, September 2025).



Figure 4: Looking west towards the Subject Site (Google Streetview, September 2025).

1.3 Planning and Regulatory Context

1.3.1 City of Ottawa Official Plan (2022)

The Subject Site is designated as *Neighbourhood* with an *Evolving Neighbourhood Overlay* as per the *City of Ottawa Official Plan Schedule B8 – Suburban (East) Transect* as shown in **Figure 5**.

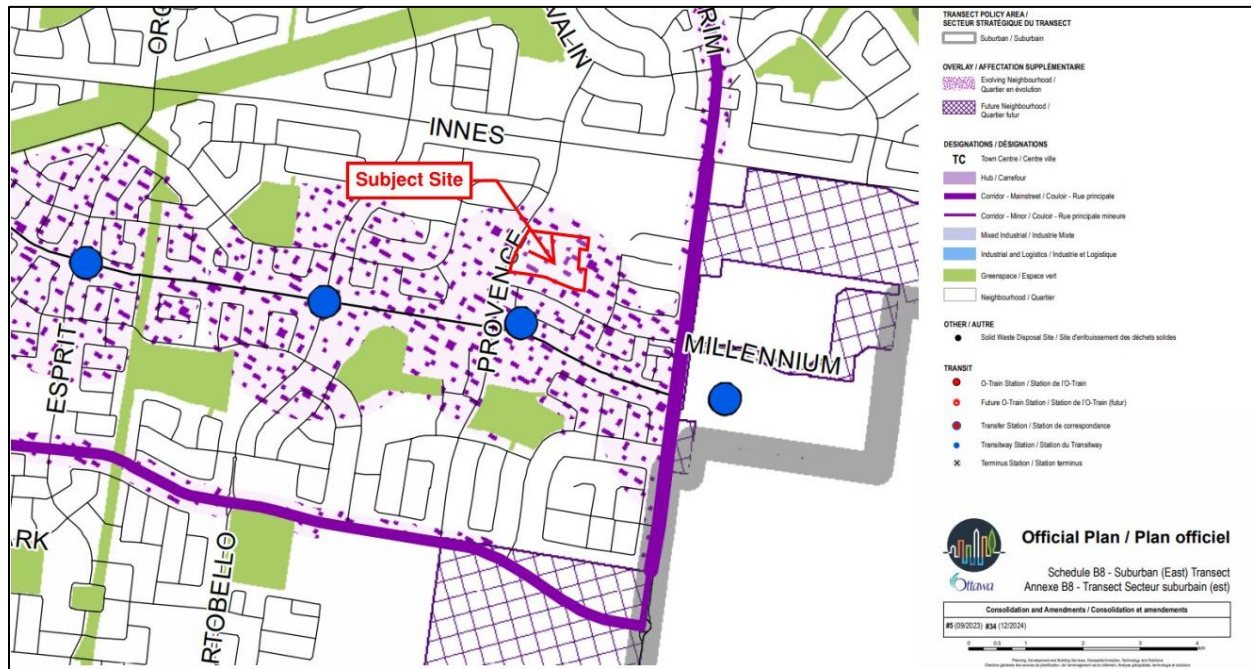


Figure 5: Excerpt from City of Ottawa Official Plan Schedule B8 – Suburban (East) Transect with the Subject Site added by Novatech.

A Major Pathway is shown east of the Subject Site along Trim Road as well as the future BRT to the south as per the City of Ottawa Official Plan Schedule C3 – Active Transportation Network Urban – Major Pathways as shown in Figure 6.

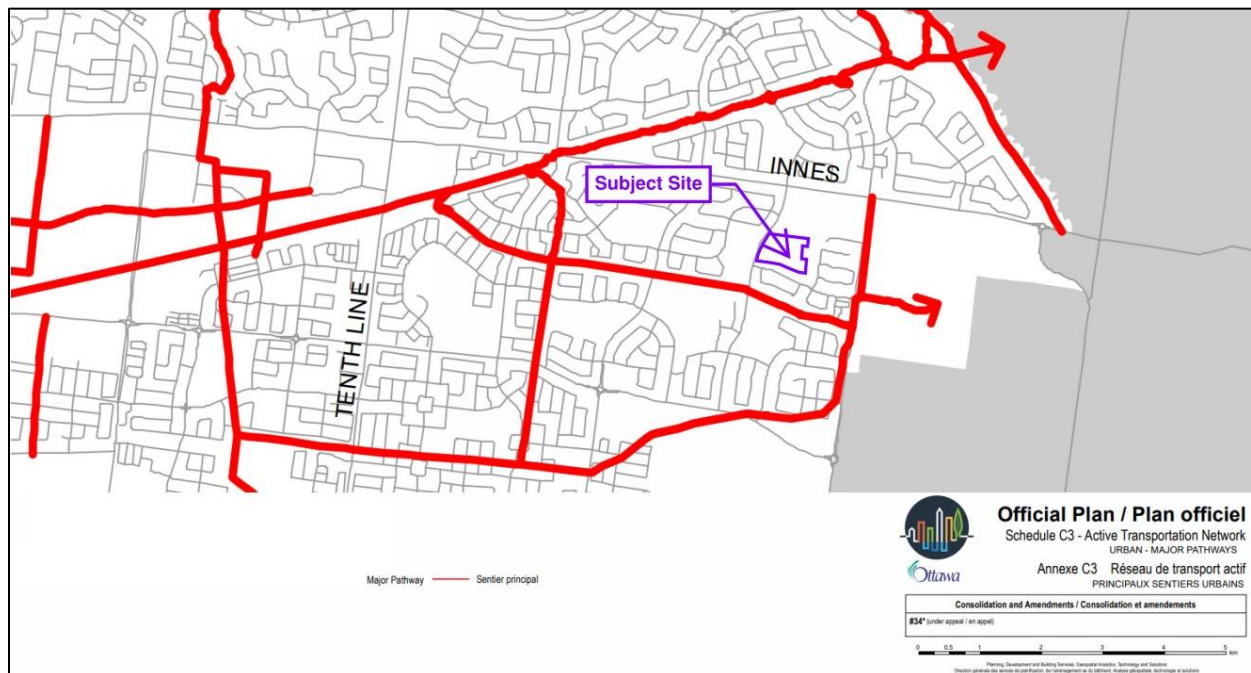


Figure 6: Excerpt from the City of Ottawa Official Plan Schedule C3 – Active Transportation Network Urban – Major Pathways with the Subject Site added by Novatech.

Trim Road is designated as an *Arterial – Existing* roadway whereas Provence Avenue is designated as a *Collector – Existing* roadway as per the *City of Ottawa Official Plan Schedule C4 – Urban Road Network* as shown in **Figure 7**.

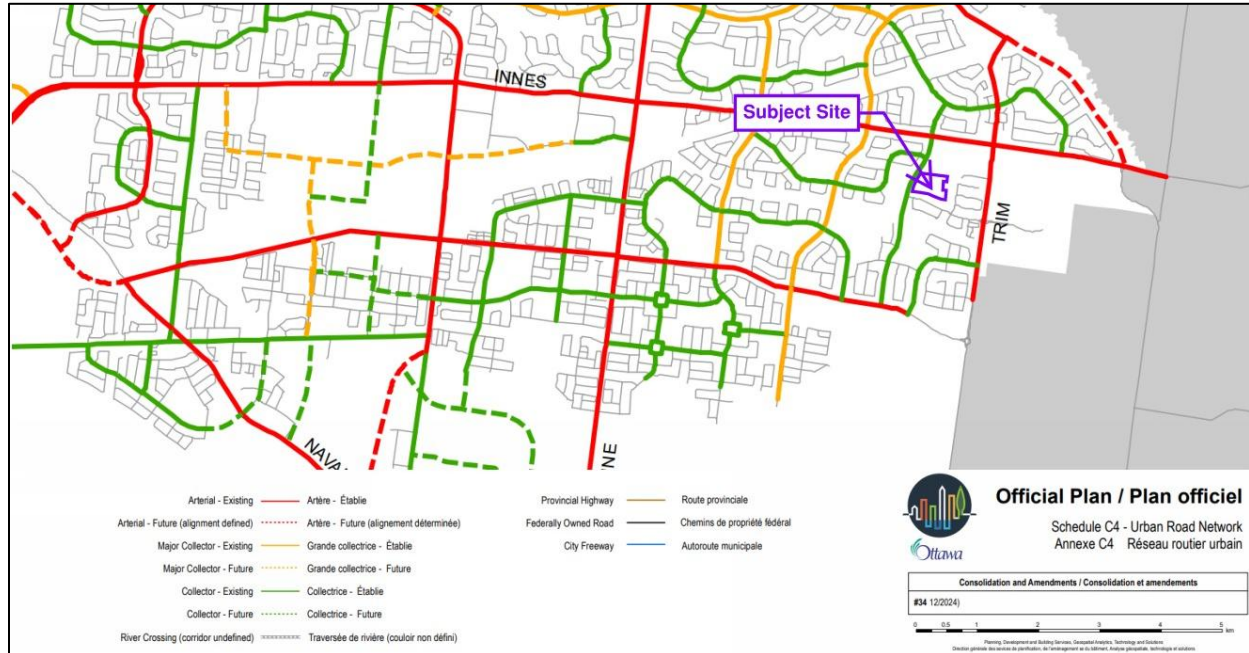


Figure 7: Excerpt from City of Ottawa Official Plan Schedule C4 – Urban Road Network with the Subject Site added by Novatech.

The Subject Site has no natural heritage features as per the *City of Ottawa Official Plan Schedule C11-C – Natural Heritage System (East)* as shown in **Figure 8**. The Nantes Woods is designated as an *Urban Natural Feature* and is identified southwest of the Subject Site.

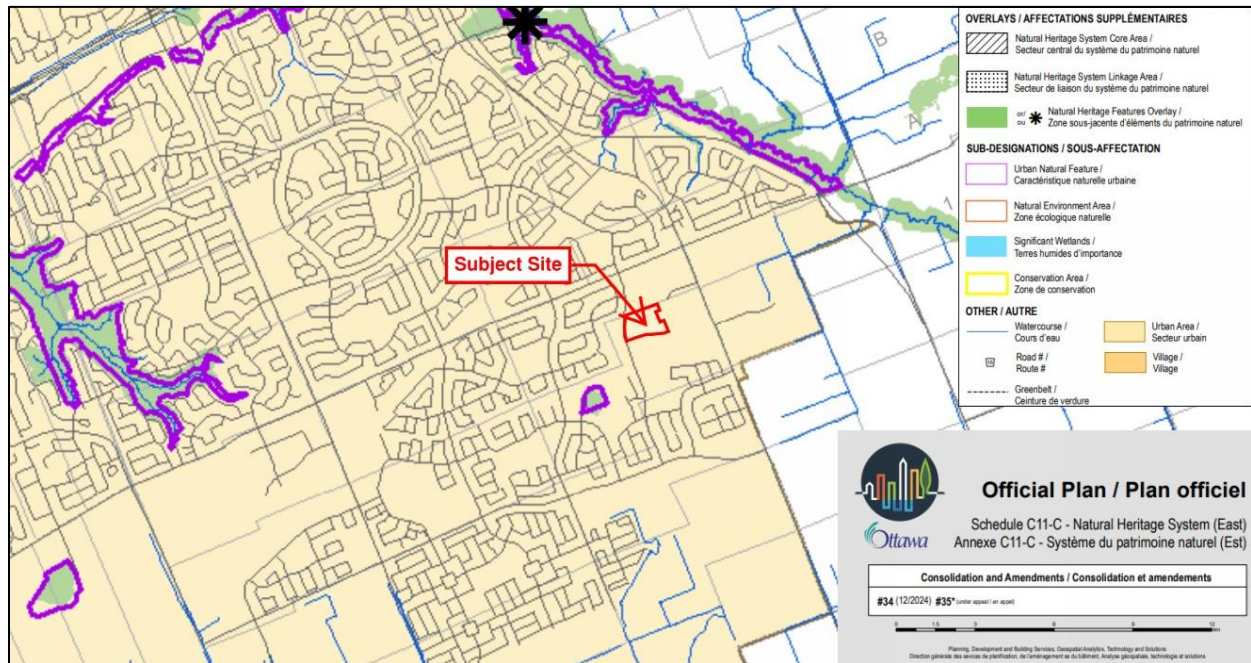


Figure 8: Excerpt from the City of Ottawa Official Plan Schedule C11-C – Natural Heritage System (West) with the Subject Site added by Novatech.

The Subject Site is not within an *Open Space* sub-designation as per the *City of Ottawa Official Plan Schedule C12 – Urban Greenspace* as shown in **Figure 9**.



Figure 9: Excerpt from the City of Ottawa Official Plan Schedule C12 – Urban Greenspace with the Subject Site added by Novatech.

The Subject Site is not identified as being within a *Flood Plain* or containing *Unstable Slopes* as per the *City of Ottawa Official Plan Schedule C15 – Environmental Constraints* as shown in **Figure 10**.

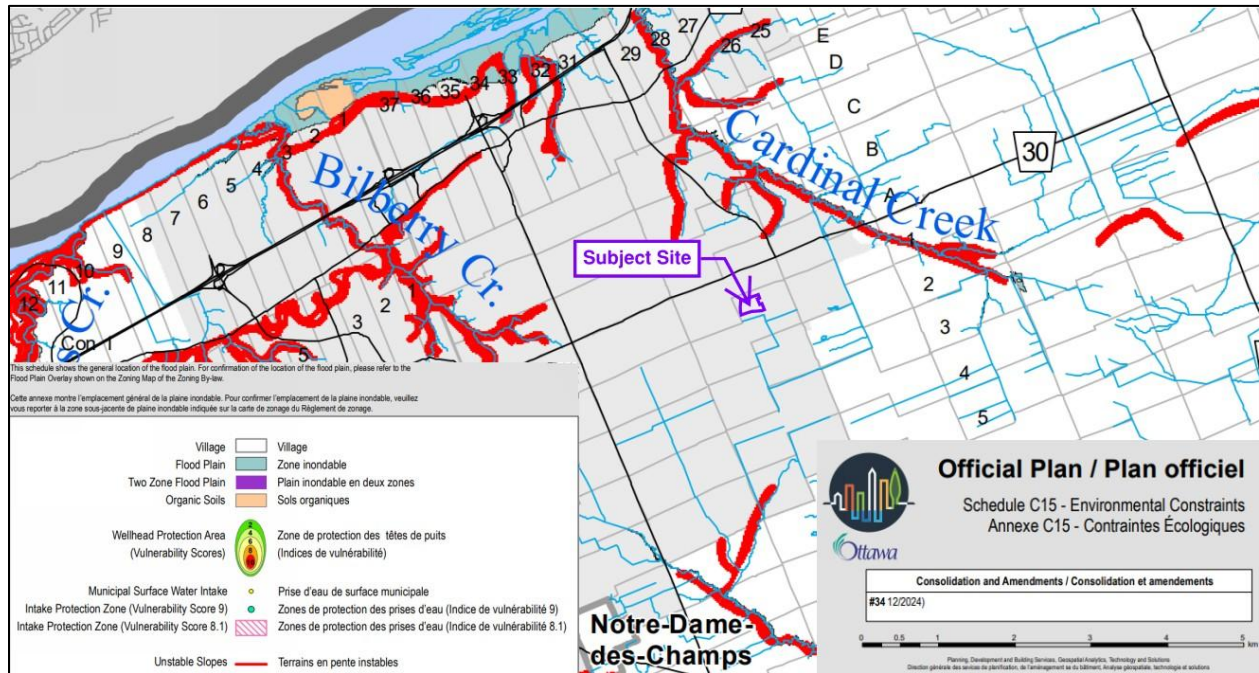


Figure 10: Excerpt from the City of Ottawa Official Plan Schedule C15 – Environmental Constraints with the Subject Site added by Novatech.

1.3.2 City of Ottawa Zoning By-law 2008-250

The Subject Site is currently zoned as *Residential Third Density, Subzone YY, Urban Exception 2582 – R3YY[2582]* under the *City of Ottawa’s Zoning By-law 2008-250* as shown in **Figure 11**. The purpose of the *Residential Third Density – R3* zone is to:

1. allow a mix of residential building forms ranging from detached to townhouse dwellings in areas designated as General Urban Area in the Official Plan; (By-law 2012-334)
 2. allow a number of other residential uses to provide additional housing choices within the third density residential areas;
 3. allow ancillary uses to the principal residential use to allow residents to work at home;
 4. regulate development in a manner that is compatible with existing land use patterns so that the mixed dwelling, residential character of a neighbourhood is maintained or enhanced; and
 5. permit different development standards, identified in the Z subzone, primarily for areas designated as Developing Communities, which promote efficient land use and compact form while showcasing newer design approaches
- To facilitate the new detached dwelling and standard townhome design for Phase 5B of the Provence Orléans subdivision, a **Zoning By-law Amendment** application will be required to amend the existing zoning for ten (10) detached lots and one (1) townhome block.

The standard townhomes will remain zoned under *Residential Third Density, Subzone YY, Urban Exception 2982 – R3YY[2982]* with the added provision to include a reduction to the minimum corner side yard setback to one of the exterior townhome blocks from 4.5 metres to 3.0 metres. This rezoning would apply only to Block 48, identified on the preliminary 4M-Plan as shown in **Figure 12**. No new land uses are being proposed as part of this application.

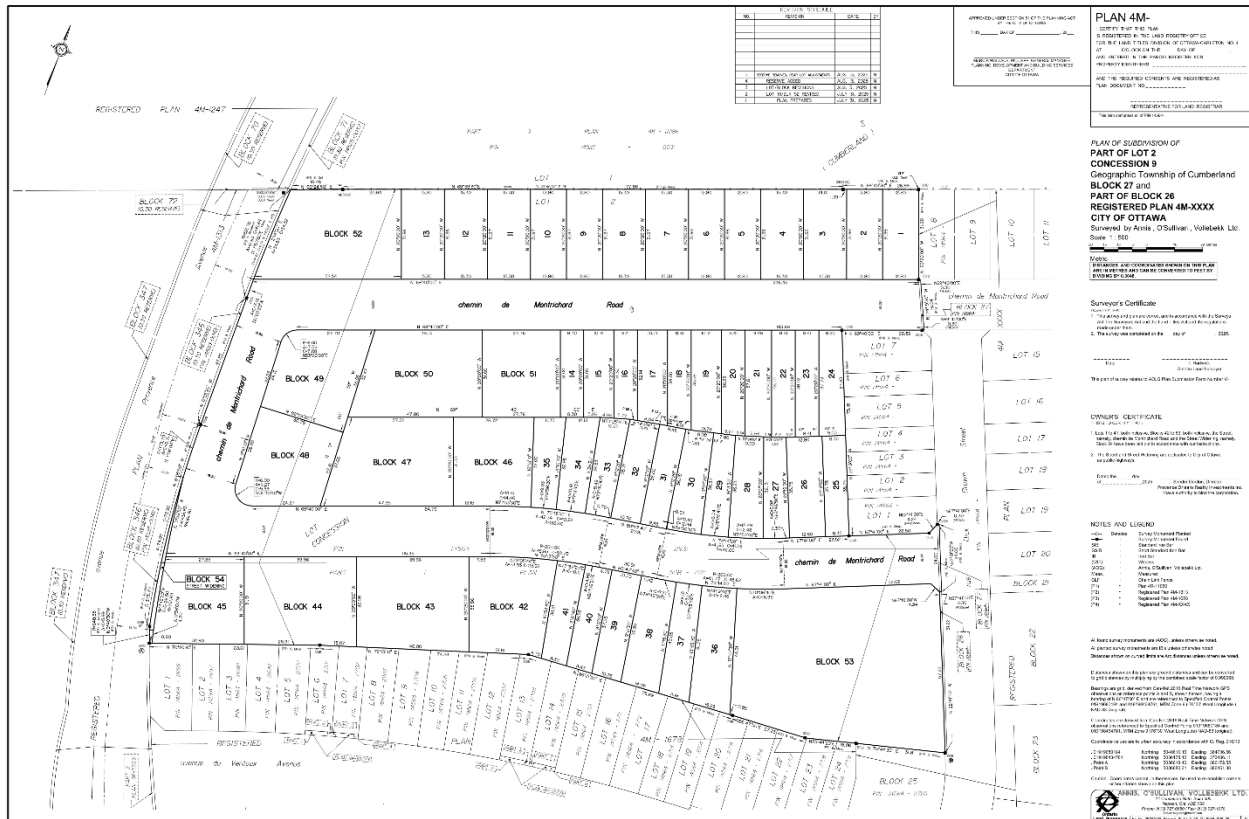


Figure 12: Preliminary 4M-Plan prepared by Annis O'Sullivan Vollebek LTD.

The reduction to the lot width and corner side yard setback achieves a more efficient use of land resources. As the full 9.0 metre lot width is not required based on the new detached dwelling design and the full 4.5 metre corner side yard setback is not required based on the new standard townhome design. As such, the suggested zoning amendment to the minimum lot width as well as corner side yard setback have been tailored to reflect this. The reduced minimum lot width will enable a more attainable housing option and diversify the residential dwelling types in terms of the subdivision design. The reduction to the corner side yard setback will maintain the suburban streetscape whereas the driveway area is a standardized size designed for detached dwellings and results from the reduction to the minimum lot width.

The reduction to the minimum lot widths, standard driveway areas, and corner side yard setback result in negligible changes to the imperviousness of the development area. Stormwater management for the development will continue to function as intended in the draft plan approval and the previously approved phases of the development. A Site Servicing and Stormwater Management Brief prepared by Novatech dated October 2025 has been submitted as part of the

detailed design review by the City of Ottawa which confirms that the stormwater management criteria for the development can be achieved with the suggested zoning.

Accompanying Conceptual Site Plan and Landscape Plan have been prepared by Novatech in support of the *Zoning By-law Amendment* application to illustrate the suggested zoning amendments as shown in **Appendix C**. The Conceptual Landscape Plan demonstrates that the suggested zoning amendments will not impact soil volume requirements or streetscape tree planting required as part of the proposed development while acknowledging limitations due to servicing and utility conflicts. The City of Ottawa's standard Draft Plan Condition requires that one tree be planted for interior lots and two trees be planted for corner lots. A Phase 5B Landscape Plan Drawing 117155-L5B REV 1 prepared by Novatech dated October 14, 2025 has been submitted as part of the detailed design review by the City of Ottawa and included in **Appendix C** as well.

A total of ninety-five (95) residential dwelling units are proposed for the Subject Site and include forty-one (41) detached dwelling units and fifty-four (54) townhome dwelling units. The rezoning will only apply to **ten (10) detached dwelling lots** and **one (1) townhome block** within Phase 5B.

2.2 Details of Proposed Amendment(s)

As part of the proposed development, it is suggested to rezone the ten (10) detached lots from *Residential Third Density, Subzone YY, Urban Exception 2582 – R3YY[2582]* to *Residential Third Density, Subzone YY, Urban Exception 2989 – R3YY[2989]* to permit the new detached dwelling design. The one (1) townhome block will remain zoned as *Residential Third Density, Subzone YY, Urban Exception 2582 – R3YY[2582]* with the added provision to reduce the corner side yard setback from 4.5 metres to 3.0 metres as shown in **Figure 13**. The suggested zoning provisions for the proposed development will be subject to further discussions with City of Ottawa staff.

The purpose of the *Residential Third Density – R3* zone is to:

1. *allow a mix of residential building forms ranging from detached to townhome dwellings in areas designated as General Urban Area in the Official Plan;*
 2. *allow a number of other residential uses to provide additional housing choices within the third density residential areas;*
 3. *allow ancillary uses to the principal residential use to allow residents to work at home;*
 4. *regulate development in a manner that is compatible with existing land use patterns so that the mixed dwelling, residential character of a neighbourhood is maintained or enhanced; and*
 5. *permit different development standards, identified in the Z subzone, primarily for areas designated as Developing Communities, which promote efficient land use and compact form while showcasing newer design approaches.*
- **The suggested zoning for the detached dwelling will maintain the purpose of the underlying *Residential Third Density – R3* zone currently in force and effect for the Subject Site.**

- The current zoning of the townhome block will remain with the added provision to reduce the corner side yard setback from 4.5 metres to 3.0 metres for one (1) townhome block.
- The zoning of ten (10) detached dwelling lots will be amended from *Residential Third Density, Subzone YY, Urban Exception 2582 – R3YY[2582]* to *Residential Third Density, Subzone YY, Urban Exception 2989 – R3YY[2989]* to permit a maximum driveway area of 55% for lots that are less than 9.0 metres wide and a minimum lot width of 8.3 metres.
- The *R3YY[2989]* zone was approved by City Council as part of the previous *Zoning By-law Amendment* application for Phase 5A on October 30, 2024 for and enacted as enacted as *By-law 2024-461*.
- The suggested rezoning and amended provisions will achieve an efficient development and land use pattern by maximizing the Subject Site’s land resources for residential development while contributing to additional density within the *Suburban Transect*.

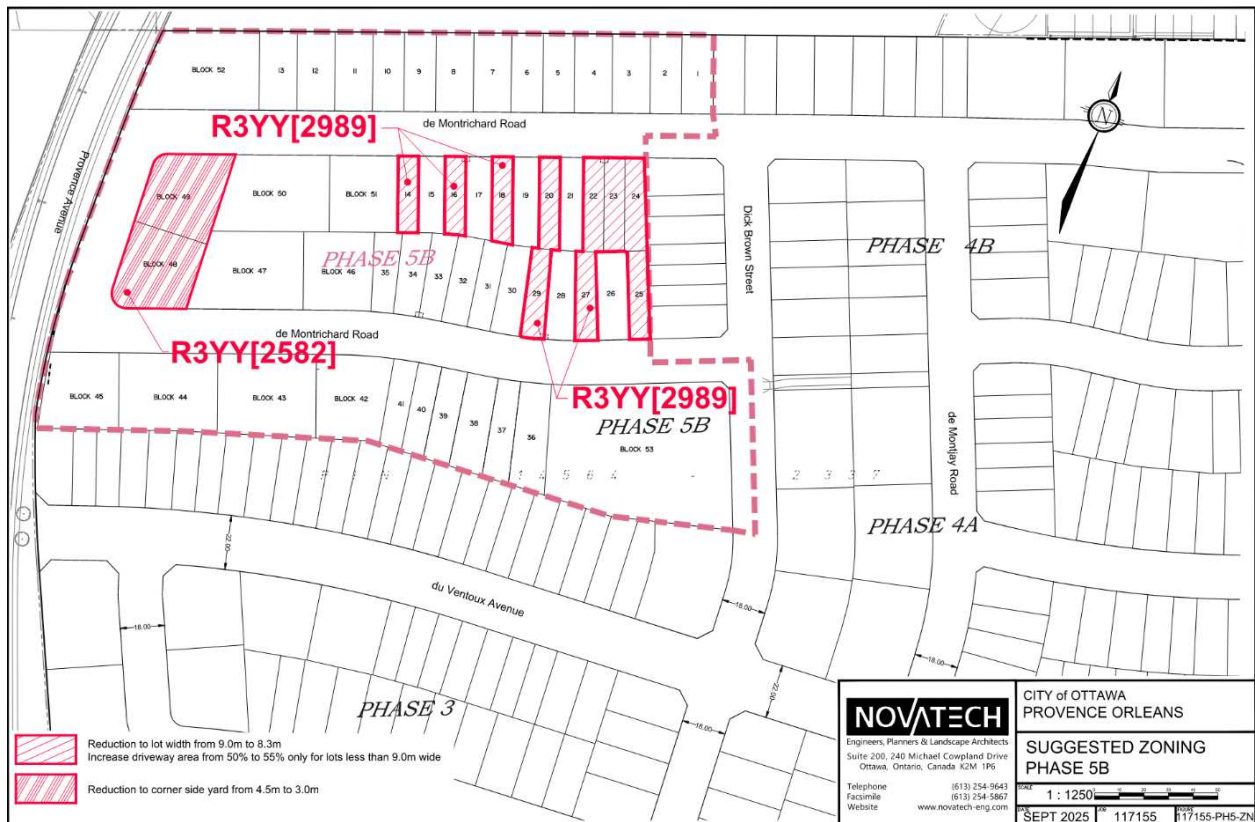


Figure 13: Excerpt from Suggested Zoning Key Plan prepared by Novatech dated September 2025.

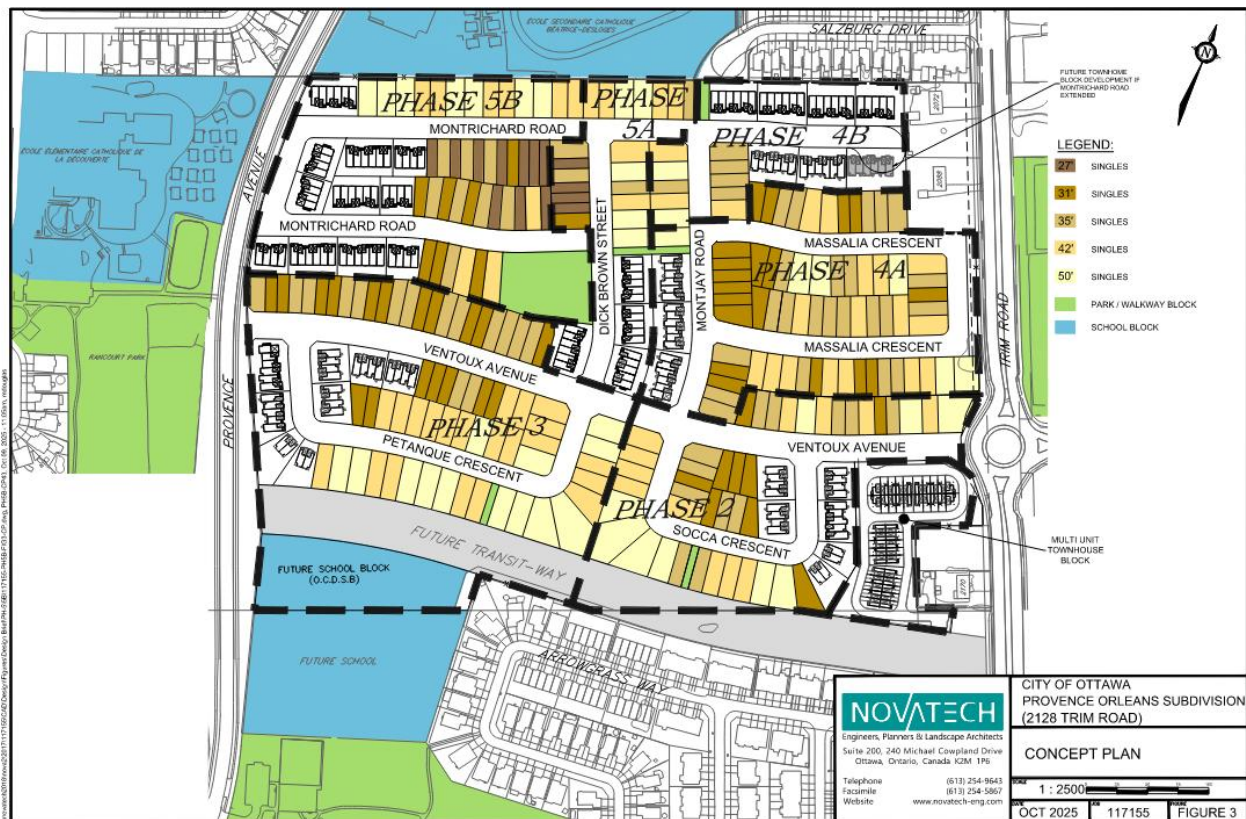


Figure 14: Concept Plan prepared by Novatech dated October 2025.

2.3 Previous Consultations, Applications and Approvals

As previously noted, the Subject Site is part of Phase 5B in the approved Provence Orléans subdivision located at 2128 Trim Road.

- The *Plan of Subdivision* (City File No.: D07-16-18-0021) application received Draft Plan Approval on July 5, 2019 with a subsequent *Plan of Subdivision* (City File No.: D07-16-22-0005) application filed to extend Draft Plan Approval to July 5, 2025. The *Zoning By-law Amendment* (City File No.: D02-02-18-0067) application was approved on September 11, 2019 by Ottawa City Council and enacted as *By-law 2019-301*. A *Zoning By-law Amendment* (City File No.: D02-02-24-0034) application was approved by Ottawa City Council on October 30, 2024 to permit reduced lot widths in Provence Orléans Phase 5A.
- City of Ottawa staff confirmed that to support the suggested zoning, a Studies, Plans, and Identification List (SPIL) similar to the one provided for Phase 5A which included a Planning Rationale, Public Consultation Strategy, and Urban Design Brief would be required. This report shall be read in conjunction with the approved Planning Rationale, Urban Design Brief, and Integrated Environmental Review Statement (IERS) prepared by Novatech dated November 9, 2018 and all previously approved reports for the Provence Orléans subdivision.

3.0 PLANNING POLICY AND REGULATORY FRAMEWORK

3.1 Provincial Planning Statement (PPS) 2024

The *Provincial Planning Statement [PPS] (2024)* came into effect on October 20, 2024 and replaces the previous *Provincial Policy Statement (2020)* that came into effect on May 1, 2020. The PPS provides policy direction on matters of provincial interest related to land use planning and development. It sets the policy foundation for regulating development and land use province-wide, helping achieve the provincial goal of meeting the needs of a fast-growing province while enhancing the quality of life for all Ontarians. The decisions that affect all planning matters “*shall be consistent with*” relevant policy statements under the authority of Section 3 of the *Planning Act*. The following is an overall review of the applicable PPS 2024 policies.

Chapter 2: Building Homes, Sustaining Strong and Competitive Communities

2.1 Planning for People and Homes

- 1) *As informed by provincial guidance, planning authorities shall base population and employment growth forecasts on Ontario Population Projections published by the Ministry of Finance and may modify, as appropriate.*
- 2) *Notwithstanding policy 2.1.1, municipalities may continue to forecast growth using population and employment forecasts previously issued by the Province for the purposes of land use planning.*
- 3) *At the time of creating a new official plan and each official plan update, sufficient land shall be made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of at least 20 years, but not more than 30 years, informed by provincial guidance. Planning for infrastructure, public service facilities, strategic growth areas and employment areas may extend beyond this time horizon. Where the Minister of Municipal Affairs and Housing has made a zoning order, the resulting development potential shall be in addition to projected needs over the planning horizon established in the official plan. At the time of the municipality’s next official plan update, this additional growth shall be incorporated into the official plan and related infrastructure plans.*
- 4) *To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:*
 - a) *maintain at all times the ability to accommodate residential growth for a minimum of 15 years through lands which are designated and available for residential development; and*
 - b) *maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned, including units in draft approved or registered plans.*
- 5) *Where planning is conducted by an upper-tier municipality, the land and unit supply maintained by the lower-tier municipality identified in policy 2.1.4 shall be based on and reflect the allocation of population and units by the upper-tier municipality.*
- 6) *Planning authorities should support the achievement of complete communities by:*
 - a) *accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities*

- and other institutional uses (including schools and associated child care facilities, long term care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs;*
- b) improving accessibility for people of all ages and abilities by addressing land use barriers which restrict their full participation in society; and*
 - c) improving social equity and overall quality of life for people of all ages, abilities, and incomes, including equity-deserving groups.*
- **The Subject Site is situated within the City of Ottawa’s settlement area and is designated within the *Official Plan (2022)* for future low-rise residential development.**
 - **The range and mix of housing options and densities will contribute to the overall housing typologies and forms for the Orléans community.**

2.2 Housing

- 1) *Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by:*
 - a) establishing and implementing minimum targets for the provision of housing that is affordable to low and moderate income households, and coordinating land use planning and planning for housing with Service Managers to address the full range of housing options including affordable housing needs;*
 - b) permitting and facilitating:*
 - i. all housing options required to meet the social, health, economic and wellbeing requirements of current and future residents, including additional needs housing and needs arising from demographic changes and employment opportunities; and*
 - ii. all types of residential intensification, including the development and redevelopment of underutilized commercial and institutional sites (e.g., shopping malls and plazas) for residential use, development and introduction of new housing options within previously developed areas, and redevelopment, which results in a net increase in residential units in accordance with policy 2.3.1.3;*
 - c) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation; and*
 - d) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations.*
- **The range and mix of housing options and densities will contribute to the overall housing typologies and forms for the Orléans community.**
 - **The Subject Site is situated within the City of Ottawa’s settlement area and efficiently utilizes land, resources, infrastructure, and public service facilities.**
 - **The Subject Site is situated near a future transit station as part of the future BRT. Innes Road and a portion of Trim Road are identified as a *Transit Priority Corridor* as per the *City of Ottawa Official Plan Schedule C2 – Transit Network Ultimate*.**

2.3 Settlement Areas for Settlement Area Boundary Expansions

2.3.1 General Policies for Settlement Areas

- 1) *Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.*
 - 2) *Land use patterns within settlement areas should be based on densities and a mix of land uses which:*
 - a) *efficiently use land and resources;*
 - b) *optimize existing and planned infrastructure and public service facilities;*
 - c) *support active transportation;*
 - d) *are transit-supportive, as appropriate; and*
 - e) *are freight-supportive.*
 - 3) *Planning authorities shall support general intensification and redevelopment to support the achievement of complete communities, including by planning for a range and mix of housing options and prioritizing planning and investment in the necessary infrastructure and public service facilities.*
 - 4) *Planning authorities shall establish and implement minimum targets for intensification and redevelopment within built-up areas, based on local conditions.*
 - 5) *Planning authorities are encouraged to establish density targets for designated growth areas, based on local conditions. Large and fast-growing municipalities are encouraged to plan for a target of 50 residents and jobs per gross hectare in designated growth areas.*
 - 6) *Planning authorities should establish and implement phasing policies, where appropriate, to ensure that development within designated growth areas is orderly and aligns with the timely provision of the infrastructure and public service facilities.*
- **The Subject Site is situated within the City of Ottawa’s settlement area and efficiently utilizes land, resources, infrastructure, and public service facilities.**
 - **The range and mix of housing options and densities will contribute to the overall housing typologies and forms for the Orléans community.**

2.4.2 Major Transit Station Areas

- 1) *Planning authorities shall delineate the boundaries of major transit station areas on higher order transit corridors through a new official plan or official plan amendment adopted under section 26 of the Planning Act. The delineation shall define an area within an approximately 500 to 800-metre radius of a transit station and that maximizes the number of potential transit users that are within walking distance of the station.*
- 2) *Within major transit station areas on higher order transit corridors, planning authorities shall plan for a minimum density target of:*
 - a) *200 residents and jobs combined per hectare for those that are served by subways;*
 - b) *160 residents and jobs combined per hectare for those that are served by light rail or bus rapid transit; or*
 - c) *150 residents and jobs combined per hectare for those that are served by commuter or regional rail.*
- 3) *Planning authorities are encouraged to promote development and intensification within major transit station areas, where appropriate, by:*

- a) *planning for land uses and built form that supports the achievement of minimum density targets; and*
- b) *supporting the redevelopment of surface parking lots within major transit station areas, including commuter parking lots, to be transit-supportive and promote complete communities.*
- 4) *For any particular major transit station area, planning authorities may request the Minister to approve an official plan or official plan amendment with a target that is lower than the applicable target established in policy 2.4.2.2, where it has been demonstrated that this target cannot be achieved because:*
 - a) *development is prohibited by provincial policy or severely restricted on a significant portion of the lands within the delineated area; or*
 - b) *there are a limited number of residents and jobs associated with the built form, but a major trip generator or feeder service will sustain high ridership at the station or stop.*
- 5) *Planning authorities may plan for major transit station areas that are not on higher order transit corridors by delineating boundaries and establishing minimum density targets.*
- 6) *All major transit station areas should be planned and designed to be transit-supportive and to achieve multimodal access to stations and connections to nearby major trip generators by providing, where feasible:*
 - a) *connections to local and regional transit services to support transit service integration;*
 - b) *infrastructure that accommodates a range of mobility needs and supports active transportation, including sidewalks, bicycle lanes, and secure bicycle parking; and*
 - c) *commuter pick-up/drop-off areas.*

2.4.3 Frequent Transit Corridors

- 1) *Planning authorities shall plan for intensification on lands that are adjacent to existing and planned frequent transit corridors, where appropriate.*
- **The Subject Site is situated near a future transit station as part of the future BRT. Innes Road and a portion of Trim Road are identified as a *Transit Priority Corridor* as per the *City of Ottawa Official Plan Schedule C2 – Transit Network Ultimate*.**

2.9 Energy Conservation, Air Quality and Climate Change

- 1) *Planning authorities shall plan to reduce greenhouse gas emissions and prepare for the impacts of a changing climate through approaches that:*
 - a) *support the achievement of compact, transit-supportive, and complete communities;*
 - b) *incorporate climate change considerations in planning for and the development of infrastructure, including stormwater management systems, and public service facilities;*
 - c) *support energy conservation and efficiency;*
 - d) *promote green infrastructure, low impact development, and active transportation, protect the environment and improve air quality; and*
 - e) *take into consideration any additional approaches that help reduce greenhouse gas emissions and build community resilience to the impacts of a changing climate.*

- The Provence Orléans subdivision will achieve a compact and transit-supportive built form with a range and mix of housing options and densities that will contribute an overall complete community.
- The Subject Site is situated near a future transit station as part of the future BRT. Innes Road and a portion of Trim Road are identified as a *Transit Priority Corridor* as per the *City of Ottawa Official Plan Schedule C2 – Transit Network Ultimate*.

Chapter 3: Infrastructure and Facilities

3.1 General Policies for Infrastructure and Public Service Facilities

- 1) *Infrastructure and public service facilities shall be provided in an efficient manner while accommodating projected needs. Planning for infrastructure and public service facilities shall be coordinated and integrated with land use planning and growth management so that they:*
 - a) *are financially viable over their life cycle, which may be demonstrated through asset management planning;*
 - b) *leverage the capacity of development proponents, where appropriate; and*
 - c) *are available to meet current and projected needs.*
- **A Site Servicing and Stormwater Management Brief prepared by Novatech dated October 2025 has been submitted as part of the detailed design review by the City of Ottawa which confirms that the stormwater management criteria for the development can be achieved with the suggested zoning.**

3.2 Transportation Systems

- 1) *Transportation systems should be provided which are safe, energy efficient, facilitate the movement of people and goods, are appropriate to address projected needs, and support the use of zero- and low- emission vehicles.*
 - 2) *Efficient use should be made of existing and planned infrastructure, including through the use of transportation demand management strategies, where feasible.*
- **The Provence Orléans subdivision will include a network of new roadways and sidewalks that were approved through Draft Plan Approval. This *Zoning By-law Amendment* application will not result in any changes to the approved roadway configuration of the Provence Orléans subdivision.**

3.6 Sewage, Water, and Stormwater

- 1) *Planning for sewage and water services shall:*
 - a) *accommodate forecasted growth in a timely manner that promotes the efficient use and optimization of existing municipal sewage services and municipal water services and existing private communal sewage services and private communal water services;*
 - b) *ensure that these services are provided in a manner that:*
 1. *can be sustained by the water resources upon which such services rely;*

2. *is feasible and financially viable over their life cycle;*
 3. *protects human health and safety, and the natural environment, including the quality and quantity of water; and*
 4. *aligns with comprehensive municipal planning for these services, where applicable.*
 - c) *promote water and energy conservation and efficiency;*
 - d) *integrate servicing and land use considerations at all stages of the planning process;*
 - e) *consider opportunities to allocate, and re-allocate if necessary, the unused system capacity of municipal water services and municipal sewage services to support efficient use of these services to meet current and projected needs for increased housing supply; and*
 - f) *be in accordance with the servicing options outlined through policies 3.6.2, 3.6.3, 3.6.4 and 3.6.5.*
 - 2) *Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. For clarity, municipal sewage services and municipal water services include both centralized servicing systems and decentralized servicing systems.*
 - 8) *Planning for stormwater management shall:*
 - a) *be integrated with planning for sewage and water services and ensure that systems are optimized, retrofitted as appropriate, feasible and financially viable over their full life cycle;*
 - b) *minimize, or, where possible, prevent or reduce increases in stormwater volumes and contaminant loads;*
 - c) *minimize erosion and changes in water balance including through the use of green infrastructure;*
 - d) *mitigate risks to human health, safety, property and the environment;*
 - e) *maximize the extent and function of vegetative and pervious surfaces;*
 - f) *promote best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development; and*
 - g) *align with any comprehensive municipal plans for stormwater management that consider cumulative impacts of stormwater from development on a watershed scale.*
- **A Site Servicing and Stormwater Management Brief prepared by Novatech dated October 2025. The proposed development will be serviced by an extension of existing municipal infrastructure including water, stormwater, and sanitary services as noted in the report.**
- **Detailed information and findings can be found in the report which will accompany this submission.**

3.9 Public Spaces, Recreation, Parks, Trails and Open Space

- 1) *Healthy, active, and inclusive communities should be promoted by:*
 - a) *planning public streets, spaces and facilities to be safe, meet the needs of persons of all ages and abilities, including pedestrians, foster social interaction and facilitate active transportation and community connectivity;*

- b) *planning and providing for the needs of persons of all ages and abilities in the distribution of a full range of publicly-accessible built and natural settings for recreation, including facilities, parklands, public spaces, open space areas, trails and linkages, and, where practical, water-based resources;*
 - c) *providing opportunities for public access to shorelines; and*
 - d) *recognizing provincial parks, conservation reserves, and other protected areas, and minimizing negative impacts on these areas.*
- **The Provence Orléans subdivision Phase 5B includes a future park block which will be transferred to the City of Ottawa at the time of registration. This *Zoning By-law Amendment* application will not result in any changes to the future park block which is already zoned as *Parks and Open Space – O1*.**

Chapter 4: Wise Use and Management of Resources

4.1 Natural Heritage

- 1) *Natural features and areas shall be protected for the long term.*
- 2) *The diversity and connectivity of natural features in an area, and the long-term ecological function and biodiversity of natural heritage systems, should be maintained, restored or, where possible, improved, recognizing linkages between and among natural heritage features and areas, surface water features and ground water features.*
- 3) *Natural heritage systems shall be identified in Ecoregions 6E & 7E 1, recognizing that natural heritage systems will vary in size and form in settlement areas, rural areas, and prime agricultural areas.*
- 4) *Development and site alteration shall not be permitted in:*
 - a) *significant wetlands in Ecoregions 5E, 6E and 7E1; and*
 - b) *significant coastal wetlands*
- 5) *Development and site alteration shall not be permitted in:*
 - a) *significant wetlands in the Canadian Shield north of Ecoregions 5E, 6E and 7E1;*
 - b) *significant woodlands in Ecoregions 6E and 7E (excluding islands in Lake Huron and the St. Marys River)1;*
 - c) *significant valleylands in Ecoregions 6E and 7E (excluding islands in Lake Huron and the St. Marys River)1;*
 - d) *significant wildlife habitat;*
 - e) *significant areas of natural and scientific interest; and*
 - f) *coastal wetlands in Ecoregions 5E, 6E and 7E1 that are not subject to policy 4.1.4.b), unless it has been demonstrated that there will be no negative impacts on the natural features or their ecological functions.*
- 6) *Development and site alteration shall not be permitted in fish habitat except in accordance with provincial and federal requirements.*
- 7) *Development and site alteration shall not be permitted in habitat of endangered species and threatened species, except in accordance with provincial and federal requirements.*
- 8) *Development and site alteration shall not be permitted on adjacent lands to the natural heritage features and areas identified in policies 4.1.4, 4.1.5, and 4.1.6 unless the ecological function of the adjacent lands has been evaluated and it has been*

demonstrated that there will be no negative impacts on the natural features or on their ecological functions.

- **The Subject Site has no natural heritage features as per the *City of Ottawa Official Plan Schedule C11-C – Natural Heritage System (East)*. The Nantes Woods is designated as an *Urban Natural Feature* and is identified southwest of the Subject Site.**
- **This *Zoning By-law Amendment* application will not result in any impacts to any natural heritage features.**

4.2 Water

- 1) *Planning authorities shall protect, improve or restore the quality and quantity of water by:*
 - a) *using the watershed as the ecologically meaningful scale for integrated and long-term planning, which can be a foundation for considering cumulative impacts of development;*
 - b) *minimizing potential negative impacts, including cross-jurisdictional and cross-watershed impacts;*
 - c) *identifying water resource systems;*
 - d) *maintaining linkages and functions of water resource systems;*
 - e) *implementing necessary restrictions on development and site alteration to:*
 1. *protect all municipal drinking water supplies and designated vulnerable areas;*
and
 2. *protect, improve or restore vulnerable surface and ground water, and their hydrologic functions;*
 - f) *planning for efficient and sustainable use of water resources, through practices for water conservation and sustaining water quality; and*
 - g) *ensuring consideration of environmental lake capacity, where applicable.*
- 2) *Development and site alteration shall be restricted in or near sensitive surface water features and sensitive ground water features such that these features and their related hydrologic functions will be protected, improved or restored, which may require mitigative measures and/or alternative development approaches.*

- **The Subject Site is not identified to be within a *Flood Plain* as per the *City of Ottawa Official Plan Schedule C15 – Environmental Constraints*.**

4.4 Minerals and Petroleum

4.4.1 General Policies for Minerals and Petroleum

- 1) *Minerals and petroleum resources shall be protected for long-term use.*

4.5 Mineral Aggregate Resources

4.5.1 General Policies for Mineral Aggregate Resources

- 1) *Mineral aggregate resources shall be protected for long-term use and, where provincial information is available, deposits of mineral aggregate resources shall be identified.*
- **This Zoning By-law Amendment application will not impact any current mineral mining operations or petroleum resource operations.**

4.6 Cultural Heritage and Archaeology

- 1) *Protected heritage property, which may contain built heritage resources or cultural heritage landscapes, shall be conserved.*
 - 2) *Planning authorities shall not permit development and site alteration on lands containing archaeological resources or areas of archaeological potential unless the significant archaeological resources have been conserved.*
 - 3) *Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property unless the heritage attributes of the protected heritage property will be conserved.*
 - 4) *Planning authorities are encouraged to develop and implement:*
 - a) *archaeological management plans for conserving archaeological resources; and*
 - b) *proactive strategies for conserving significant built heritage resources and cultural heritage landscapes.*
- **The Subject Site is not identified as containing “Archaeological Potential” as per the City of Ottawa’s GeoOttawa.**
 - **A Stage 1 Archaeological Assessment was prepared by Paterson Group dated March 15, 2018 as part of the overall Provence Orléans subdivision. A subsequent Stage 2 Archaeological Assessment was completed by Paterson dated June 20, 2018. The report concluded that no archaeological remains, artifacts, or culturally significant soil profiles were encountered and no further archaeological study is required.**
 - **A Review and Entry into the Ontario Public Register of Archaeological Reports letter was prepared by the Ministry of Tourism, Culture, and Sport (MTCS) dated September 5, 2018. The letter confirms that the MTCS is satisfied that the fieldwork and report are consistent with the MCM’s *Standards and Guidelines for Consultant Archaeologists (2011)* and terms and conditions for archaeological licenses.**

Chapter 5: Protecting Public Health and Safety

5.1 General Policies for Natural and Human-Made Hazards

- 1) *Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.*

5.2 Natural Hazards

- 1) *Planning authorities shall, in collaboration with conservation authorities where they exist, identify hazardous lands and hazardous sites and manage development in these areas, in accordance with provincial guidance.*

- 2) *Development shall generally be directed to areas outside of:*
 - a) *hazardous lands adjacent to the shorelines of the Great Lakes -St. Lawrence River System and large inland lakes which are impacted by flooding hazards, erosion hazards and/or dynamic beach hazards;*
 - b) *hazardous lands adjacent to river, stream and small inland lake systems which are impacted by flooding hazards and/or erosion hazards; and*
 - c) *hazardous sites.*
- 3) *Development and site alteration shall not be permitted within:*
 - a) *the dynamic beach hazard;*
 - b) *defined portions of the flooding hazard along connecting channels (the St. Marys, St. Clair, Detroit, Niagara and St. Lawrence Rivers);*
 - c) *areas that would be rendered inaccessible to people and vehicles during times of flooding hazards, erosion hazards and/or dynamic beach hazards, unless it has been demonstrated that the site has safe access appropriate for the nature of the development and the natural hazard; and*
 - d) *a floodway regardless of whether the area of inundation contains high points of land not subject to flooding.*
- 4) *Planning authorities shall prepare for the impacts of a changing climate that may increase the risk associated with natural hazards.*
- 5) *Despite policy 5.2.3, development and site alteration may be permitted in certain areas associated with the flooding hazard along river, stream and small inland lake systems:*
 - a) *in those exceptional situations where a Special Policy Area has been approved. The designation of a Special Policy Area, and any change or modification to the official plan policies, land use designations or boundaries applying to Special Policy Area lands, must be approved by the Ministers of Municipal Affairs and Housing and Natural Resources and Forestry prior to the approval authority approving such changes or modifications; or*
 - b) *where the development is limited to uses which by their nature must locate within the floodway, including flood and/or erosion control works or minor additions or passive non-structural uses which do not affect flood flows.*
- 8) *Further to policy 5.2.7, and except as prohibited in policies 5.2.3 and 5.2.6, development and site alteration may be permitted in those portions of hazardous lands and hazardous sites where the effects and risk to public safety are minor, could be mitigated in accordance with provincial standards, and where all of the following are demonstrated and achieved:*
 - a) *development and site alteration is carried out in accordance with floodproofing standards, protection works standards, and access standards;*
 - b) *vehicles and people have a way of safely entering and exiting the area during times of flooding, erosion and other emergencies;*
 - c) *new hazards are not created and existing hazards are not aggravated; and*
 - d) *no adverse environmental impacts will result.*

5.3 Human-Made Hazards

- 1) *Development on, abutting or adjacent to lands affected by mine hazards; oil, gas and salt hazards; or former mineral mining operations, mineral aggregate operations or petroleum resource operations may be permitted only if rehabilitation or other measures to address and mitigate known or suspected hazards are under way or have been completed.*

2) *Sites with contaminants in land or water shall be assessed and remediated as necessary prior to any activity on the site associated with the proposed use such that there will be no adverse effects.*

- **The Subject Site is not identified to be within a *Flood Plain* or containing any *Unstable Slopes* as per the *City of Ottawa Official Plan Schedule C15 – Environmental Constraints*.**
- **This *Zoning By-law Amendment* application will not result in any natural or human-made hazards.**

The *Zoning By-law Amendment* application is consistent with the policies of the *Provincial Planning Statement (2024)*.

3.2 City of Ottawa Official Plan

The Subject Site is designated as *Neighbourhood* with an *Evolving Neighbourhood Overlay* as per the *City of Ottawa Official Plan Schedule B8 – Suburban (East) Transect*. The applicable policies under the relevant sections of the *Official Plan* are listed below. A description of how the proposal responds to the policies then follows.

3.2.1 Strategic Directions & Cross Cutting Issues

Section 2 of the *Official Plan* provides the *Strategic Directions* for the City of Ottawa to become the most liveable mid-sized city in North America over the next century. These include five *Big Policy Moves* and six *Cross-Cutting Issues*.

The five *Big Policy Moves* provide broad policy directions and are the foundation of the *Official Plan*. The *Big Policy Moves* are:

- 1) *Achieve, by the end of the planning period, more growth but intensification than by greenfield development.*
- 2) *By 2046, the majority of trips in the city will be made by sustainable transportation.*
- 3) *Improve our sophistication in urban and community design and put this knowledge to the service of good urbanism at all scales, from the largest to the very small.*
- 4) *Embed environmental, climate and health resiliency and energy into the framework of our planning policies.*
- 5) *Embed economic development into the framework of our planning policies.*

- **The Subject Site is situated within the City of Ottawa’s urban area that is accessible by active and sustainable modes of transportation with public transit stops located abutting the Subject Site.**
- **The Subject Site is situated near a future transit station as part of the future BRT. Innes Road and a portion of Trim Road are identified as a *Transit Priority Corridor* as per the *City of Ottawa Official Plan Schedule C2 – Transit Network Ultimate*.**

These *Big Policy Moves* inform the six themes, or *Cross-Cutting Issues*, that are embedded throughout the policies and sections of the *Official Plan* and are essential to the achievement of

a liveable city, but are implemented through the policies in multiple sections of the *Official Plan*. The *Cross-Cutting Issues* are:

- *Intensification and Diversifying Housing Options*
- *Economic Development*
- *Energy and Climate Change*
- *Healthy and Inclusive Communities*
- *Gender and Racial Equity*
- *Culture*

Policy 2.2.1 – Intensification and Diversifying Housing Options

- **The Provence Orléans subdivision will achieve a compact and transit-supportive built form with a range and mix of housing options and densities.**
- **The Subject Site is situated near a future transit station as part of the future BRT. The proposed development will contribute to principles of 15-minute neighbourhood which will allow future residents from the community to walk to meet their daily or weekly needs.**

Policy 2.2.2 – Economic Development

- **The proposed development will contribute to a range and mix of housing options that will support economic development as it relates to the retention of skilled workforces for businesses and employers in the City of Ottawa.**

Policy 2.2.3 – Energy and Climate Change

- **The Subject Site is situated within the City of Ottawa’s urban area and is already accessible by active and sustainable modes of transportation with public transit stops abutting the Subject Site.**
- **The Subject Site is situated near a future transit station as part of the future BRT. The proposed development will contribute to principles of 15-minute neighbourhood which will allow future residents from the community to walk to meet their daily or weekly needs.**
- **The proposed development will include a future open space area and pathways that will connect to the existing community and the multi-use pathway along Trim Road.**

Policy 2.2.4 – Healthy and Inclusive Communities

- **The Subject Site is within 900 metres radius of a future transit station. The proposed development will contribute to principles of 15-minute neighbourhood which will allow future residents from the community to walk to meet their daily or weekly needs.**
- **An existing commercial shopping plaza consisting of retail stores, restaurants, and personal service businesses is located northeast of the Subject Site at the intersection of Innes Road and Trim Road.**

Policy 2.2.5 – Gender and Racial Equality

- The Subject Site is accessible by active and sustainable modes of transportation with public transit stops located abutting the Subject Site. This ensures that equitable access is provided for all regardless of socio-economic status.
- The Subject Site is situated near a future transit station as part of the future BRT. Innes Road and a portion of Trim Road are identified as a *Transit Priority Corridor* as per *City of Ottawa Official Plan Schedule C2 – Transit Network Ultimate*.

Policy 2.2.6 – Culture

- No cultural spaces are identified on the Subject Site as per the City of Ottawa’s GeoOttawa “Cultural Spaces” mapping overlay. The Cumberland Millennium Sports Park and Rancourt Park located to the east and south of the Subject Site are identified as cultural spaces.

3.2.1 Growth Management Framework

Section 3 of the *Official Plan* provides the *Growth Management Framework* for the City of Ottawa premised on the ability to provide sufficient development opportunities and an appropriate range of choices, locating and designing growth so as to increase sustainable transportation mode shares and existing infrastructure efficiently, while reducing greenhouse gas emissions.

Policy 3.1 – Designate Sufficient Land for Growth

- 1) Sufficient land shall be designated for growth to meet the projected requirement for population, housing, employment and other purposes for a period of 25 years in accordance with the *Provincial Policy Statement*.
- 2) Projected growth in Table 1 incorporates a method to project market demand, which includes the needs of existing and future residents.
- 3) The urban area and villages shall be the focus of growth and development.
- 4) The City will allocate household growth targets as follows:
 - a. 93 per cent within the urban area where:
 - i. 47 per cent is within the urban area that is built-up or developed as of July 1, 2018; and
 - ii. 46 per cent is within the greenfield portion of the urban area;
 - b. 7 per cent within the rural area where:
 - i. 5 per cent is within the villages; and
 - ii. 2 per cent is outside of villages.

The boundaries establishing the urban areas and the villages are designated on Schedule A and Schedules B1 through B9.

- The Subject Site is situated within the City of Ottawa’s urban area and is designated within the *Official Plan (2022)* for future residential development. The approved Provence Orléans subdivision will feature low-rise residential development.

Policy 3.2 – Support Intensification

- 1) The target amount of dwelling growth in the urban area that is to occur through intensification is 51 per cent and represents the proportion of new residential dwelling

units, excluding institutional and collective units such as senior's and student residences, based upon building permit issuance within the built-up portion of the urban area...

- 2) *Intensification may occur in a variety of built forms and height categories, from Low-rise to High-Rise 41+ buildings provided density requirements are met. Unless more specific policies provide alternate direction, minimum densities are intended to establish a minimum starting point for the intensity of development, and maximum building heights are intended to establish a limit to building height. The definitions section of this Plan establishes the building height thresholds as expressed in storeys to describe height categories throughout this Plan.*
- 3) *The vast majority of Residential intensification shall focus within 15-minute neighbourhoods, which are comprised of Hubs, Corridors and lands within the Neighbourhood designations that are adjacent to them as shown on Schedules B1 through B8. Hub and Corridor designations are intended to be diverse concentrations of employment, commercial, community and transportation services (in addition to accommodating significant residential opportunities) that are accessible to adjacent Neighbourhood designations on a daily and weekly basis.*
- 4) *Intensification is permitted in all designations where development is permitted taking into account whether the site has municipal water and sewer services. This Plan supports intensification and the approval of applications for intensification shall be in conformity with transect and overlay policies as applicable. When reviewing planning applications for intensification, the City shall ensure that surface water and groundwater resources are protected, particularly where the groundwater resource is used for drinking water.*
- 8) *Intensification should occur in a variety of dwelling unit floorspace sizes to provide housing choices. Dwelling sizes are categorized into two broad categories, with a range of floorspaces occurring within each category:*
 - a) *Small-household dwellings are units with up to two bedrooms and are typically within apartment-built forms; and*
 - b) *Large-household dwellings are units with three or more bedrooms or an equivalent floor area and are typically within ground-oriented built forms.*

- **The proposed development represents residential intensification that develops the Subject Site from greenfield to housing.**
- **The range and mix of housing options and densities will contribute to the overall housing typologies and forms for the Orléans community.**
- **The Subject Site is accessible by active and sustainable modes of transportation with public transit stops located abutting the Subject Site.**

Policy 3.3 – Design new neighbourhoods to be 15-minute neighbourhoods

- 1) *Residential growth within the greenfield portions of the urban area will be planned as complete 15- minute neighbourhoods through the creation of a framework for a compact design, mix of uses and densities, a fully-connected street grid and viable options for sustainable transportation modes. Growth will also proceed in a logical, orderly, and coordinated progression through phasing and in accordance with secondary plans.*
- 2) *Urban Greenfield growth areas include previous urban expansion areas that were undeveloped as of July 1, 2018 and areas subject to the Future Neighbourhood Overlay on the B-series of schedules.*

- 4) *New greenfield neighbourhoods shall be designed to include and if necessary, reserve land for a mix of uses that ensures their development into 15-minute neighbourhoods. A mix of residential dwellings types and sizes shall also be provided and if necessary, reserved to provide a range of housing over time. New developments adjacent to existing neighbourhoods or vacant lands that are part of an approved secondary plan shall consider the existing and planned uses within a 15-minute walk as part of an appropriate mix of uses for a complete neighbourhood.*
 - 5) *New neighbourhoods shall be designed around the notion of easy pedestrian access to a rapid transit station, or frequent street bus route leading to a station on the high-frequency transit network, so that its first residents can have easy transit access to areas of the city that already are 15-minute neighbourhoods while their new neighbourhood develops the critical mass needed to become one itself, and so that residents have easy transit access to services, amenities and major cultural venues.*
- **The proposed development will contribute to principles of 15-minute neighbourhood which will allow future residents from the community to walk to meet their daily or weekly needs.**
 - **The Subject Site is accessible by active and sustainable modes of transportation with public transit stops located abutting the Subject Site. The proposed development will include pathways that will connect to the existing community and the multi-use pathway along Trim Road.**

Policy 3.5 – Meet employment needs

- 1) *Employment uses that can mix with residential uses are permitted within Hubs and Corridors. Generally, employers with the highest densities are preferred to locate in proximity to rapid transit stations. These uses tend to be office-based or regional scale retail-focused facilities.*
 - 2) *Employment uses within Neighbourhoods are service oriented and are limited in size or area. These uses tend to be office-based, institutional or in the retail sector.*
 - 3) *Employment that cannot typically compete with rents of other uses in Hubs, Corridors or Neighbourhoods designations, have potential adverse impacts on adjacent sensitive uses, and require clustering of similar uses are located within Industrial and Logistics areas. These uses tend to be manufacturing, construction, storage and logistics related. Business parks that have a mix of industrial and non-industrial uses such as small-scale office and other service-oriented uses are located within Mixed Industrial areas.*
- **The proposed development will not include employment land uses.**

3.2.2 City-wide Policies

Section 4 of the *Official Plan* sets out the city-wide policies to be considered where all new development is proposed.

Subsection 4.1 – Mobility

Policy 4.1.1 – Provide mobility options to safely and equitably navigate the city

- 1) *In the Urban area and Villages, people who walk, cycle and use transit shall, by default, be given priority for safety and movement. The resolution of overlapping priorities in the Urban area and Villages, as well as the establishment of priorities in the Rural area, will be informed by Multi-Modal Level of Service targets outlined in the Transportation Master Plan (TMP) and Multi-Modal Level of Service Guidelines.*
 - 2) *Equity considerations, in accordance with the City's Equity and Inclusion Lens, and Subsection 2.2.4, Policy 2) of this Plan, shall be included in the planning and evaluation of all transportation investments.*
 - 3) *The City's street hierarchy includes provincial and City highways, federally owned roads, arterials, major collectors, collectors, local roads or streets and lanes. The City will identify streets or segments of streets in the hierarchy that are intended to function as:*
 - a) *An access street, which is a public or private street with a close relationship to its surrounding land uses, exhibits high vehicular friction and slow speeds and prioritizes sustainable modes of transportation; or*
 - b) *A capacity and flow street, which is a public street that plays a structural role in the overall street grid by virtue of its length and its ability to link several areas of the City, and where the movement of people is an important part of its function;*
 - c) *The access or capacity and flow function shall be based on the transect, designations, overlays and consider elements such as surrounding land uses, density, connectivity, street frontage, mix of users, urban design considerations, as well as the allocation of space in the right of way and the road network functions; and*
 - d) *When capacity and flow functions are identified for a street, the priority is to ensure a high-quality public realm that maintains the safety of vulnerable users and the capacity and flow of public transit and sustainable modes in priority over general traffic.*
 - 5) *New subdivision development shall connect to existing pedestrian, cycling, transit and street networks and provide for the potential future extension of these networks up to abutting property boundaries, including those lands beyond an existing Urban boundary or Village boundary.*
- **The Provence Orléans subdivision will include a network of new roadways and sidewalks that were approved through Draft Plan Approval. This *Zoning By-law Amendment* application will not result in any changes to the approved roadway configuration of the Provence Orléans subdivision.**
 - **The Provence Orléans subdivision will connect to existing pedestrian, cycling, transit, and street networks. An 18.0 m right-of-way cross-section proposed for the future Montrichard Road will include a pedestrian sidewalk on the south leg of the road. This sidewalk enables safe, direct, and convenient movement throughout the Provence Orléans subdivision and greater community. The pedestrian sidewalk was always envisioned as part of the proposed development and will be constructed in accordance with the City of Ottawa's standard cross-section to support a healthy 15-minute neighbourhood.**
 - **As the suggested zoning will apply to only ten (10) detached lots and one (1) townhome block within Phase 5B, on-street parking is not anticipated to be impacted along the future Montrichard Road.**

4.1.2 – Promote healthy 15-minute neighbourhoods

- 1) *In general, this Plan equates a walking time of:*

- a) 5 minutes to be equivalent to a radius of 300 metres, or 400 metres on the pedestrian network;
 - b) 10 minutes to be equivalent to a radius of 600 metres, or 800 metres on the pedestrian network; and
 - c) 15 minutes to be equivalent to a radius of 900 metres or 1,200 metres on the pedestrian network.
- 2) Provide safe and convenient pedestrian routes and facilities in Hubs and Corridors and, within the following distances from transit:
- a) 600 metre radius or 800 metres walking distance, whichever is greatest, to existing or planned rapid transit stations; and
 - b) 300 metre radius or 400 metres walking distance, whichever is greatest, to existing or planned frequent street transit stops and street transit stops along a Transit Priority network.
- 3) The improvement of pedestrian and cycling networks shall be based on the TMP and associated plans, Multi-Modal Level of Service Guidelines (MMLOS), the Safe Systems Approach and all the following:
- a) All new and reconstructed streets in the Urban area and Villages shall include pedestrian and cycling facilities appropriate for their context, as specified in the TMP and associated plans; and
 - b) Safe, direct and convenient pedestrian and cycling networks and crossings; including along desire lines where needed and appropriate; and
 - c) Pedestrian and cycling networks and shortcut public access through private properties may be required at the time of development approval, where appropriate or identified in this Plan and the TMP and associated plans; and
 - d) New and reconstructed local residential streets shall be designed to low operating speed; and
 - e) Winter maintenance standards shall support the priority of active transportation networks and the achievement of active transportation mode share targets set out in the TMP and associated plans and will prioritize areas identified with an Evolving overlay.
- 4) Development of land abutting an existing or planned cycling facility identified in the TMP and associated plans will be designed to minimize vehicle access across the cycling facility in order to reduce potential conflict points, such as by providing vehicular access to parking and service areas from side streets or rear lanes.
- 5) Where public pedestrian and cycling routes or facilities intersect with roads, appropriate traffic control devices shall be provided to accommodate pedestrian and cycling movements.
- 6) New developments will provide direct connections to the existing or planned network of public sidewalks, pathways and cycling facilities.
- 11) During the review of development and as part of new road construction and road reconstruction projects, and where feasible through infrastructure renewal works, the City shall require the provision of pedestrian and cycling facilities, consistent with the Safe Systems Approach, as follows:
- a) Arterials, Major Collectors and Collectors in the Urban area and Villages shall include:
 - i. Sidewalks on both sides;
 - ii. Unidirectional cycling facilities on each side or in limited circumstances bidirectional cycle tracks on one side; and
 - iii. Notwithstanding Policies a) i) and ii) above, multi-use pathways will be permitted within the Greenbelt Transect and the City may consider a multi-use pathways

- elsewhere in lieu of separate facilities where it would provide for improved system continuity and a safer condition for users. iv) In the case of Mainstreet and Minor Corridors with narrow rights of way, additional consideration shall be given to the provision of on-street parking to support small business, while balancing the need for pedestrian and cycling supportive infrastructure.
- b) New local streets:
- i. In the Downtown Core and Inner Urban Transects, shall include sidewalks on both sides in all cases;
 - ii. In the Outer Urban and Suburban Transects and in villages, shall include sidewalks on at least one side, and sidewalks on both sides of the street wherever needed to create continuous and direct connections to destinations on both sides of the street such as public transit stops or stations, schools, public parks, pathways, recreation centres, public buildings and institutions and commercial areas.
- c) Existing Locals in the Urban Area and Villages: sidewalks will be pursued where possible, subject to practical considerations such as the existing context, available space in the right-of-way, impacts to the stormwater system and trees, network connectivity and financial affordability. Where the addition of sidewalks results in trade-offs between modes, safety for vulnerable road users will be prioritized over vehicular capacity;
- 12) The City has identified a network of active transportation facilities identified in the policies outlined above and in Schedules C3 and C8 and in the TMP and associated plans that will be implemented through the review of development applications, development of spaces within the public realm and as part of capital programs to build new transportation facilities or to maintain or upgrade existing facilities. Although not illustrated in Schedule C3, all urban area collectors, major collectors and arterials are cycling routes that, over time, are to include cycling facilities as set out in Policy 11) above.
- **The proposed development has been designed to be compact and well-connected with a surrounding diverse mix of land uses where the daily and weekly needs of future residents can be accessed within a 15-minute walk.**
 - **The Subject Site is accessible by active and sustainable modes of transportation with public transit stops located abutting the Subject Site. The proposed development will include pathways that will connect to the existing community and to the multi-use pathway along Trim Road as per the *City of Ottawa Official Plan Schedule C3 – Active Transportation Network Urban – Major Pathways*.**

Policy 4.1.3 – Support growth management and a greener and more resilient city

- 1) *The street and road network shall support multi-modal travel, the movement of goods and services, access to properties, public space functions, street trees and/or shade corridors and contribute to the overall quality of the urban environment.*
 - 4) *The street and road network shall be developed as identified on Schedules C4, C5, C9 and C10 and in approved local plans and Environmental Assessments.*
 - 6) *New streets shall, and reconstructed streets shall wherever possible, include street trees that contribute to the urban forest and streetscaping elements appropriate for its context.*
- **The Provence Orléans subdivision will connect to existing pedestrian, cycling, transit, and street networks. An 18.0 m right-of-way cross-section proposed for the future**

Montrichard Road will include a pedestrian sidewalk on the south leg of the road. This sidewalk enables safe, direct, and convenient movement throughout the Provence Orléans subdivision and greater community. The pedestrian sidewalk was always envisioned as part of the proposed development and will be constructed in accordance with the City of Ottawa's standard cross-section to support a healthy 15-minute neighbourhood.

Subsection 4.2 – Housing

Policy 4.2.1 – Enable greater flexibility and an adequate supply and diversity of housing options throughout the city

- 1) *A diverse range of flexible and context-sensitive housing options in all areas of the city shall be provided through the Zoning By-law, by:*
 - a) *Primarily regulating the density, built form, height, massing and design of residential development, rather than regulating through restrictions on building typology;*
 - b) *Promoting diversity in unit sizes, densities and tenure options within neighbourhoods including diversity in bedroom count availability;*
 - c) *Permitting a range of housing options across all neighbourhoods to provide the widest possible range of price, occupancy arrangements and tenure;*
 - d) *Establishing development standards for residential uses, appropriately balancing the value to the public interest of new policies or development application requirements against the impacts to housing affordability; and*
 - e) *The City shall maintain, at all times, land with servicing capacity sufficient to provide at least a three year supply of residential units available through lands suitably zoned to facilitate intensification and land in draft approved and registered plans.*
- **The range and mix of housing options and densities will contribute to the overall housing typologies and forms for the Orléans community.**
- **The suggested zoning amendments will facilitate the construction of a new detached dwelling design that is compatible with existing and planned residential development within the Provence Orléans subdivision.**

Subsection 4.4 – Parks and Recreation Facilities

Policy 4.4.1 – Identify park priorities within Ottawa's growth areas

- 1) *The City shall provide parks through the following three mechanisms:*
 - a) *As a condition of development, the City shall acquire land for parks or cash-in-lieu as directed by the Planning Act and the City's Parkland Dedication By-law or any successor By-law;*
 - b) *The City may choose to lease or secure parks by agreement from other public agencies such as the National Capital Commission; or*
 - c) *The City may choose to buy land for parks with cash-in-lieu of parkland or through capital expenditures*
- 2) *All development, regardless of use, shall meet all of the following criteria to the satisfaction of the City:*

- a) Consider land acquisition for parks as directed by the Parkland Dedication By-law to meet community needs for both residential and non-residential development, with an emphasis on active recreation amenities and potential cultural development with new parks acquired to address gaps or community needs; and
 - b) Prioritize land for parks on-site over cash-in-lieu of parkland. Cash-in-lieu of parkland shall only be accepted when land or location is not suitable. The land to be conveyed shall, wherever feasible:
 - i. Be a minimum of 400 square metres or as described in the upcoming Land First Policy and updated Park Development Manual as directed by the Parks and Recreation Facilities Master Plan;
 - ii. Be free of encumbrances above and below ground when land for parks is obtained by parkland dedication; or in the case of land purchases for the creation of new parks in established areas, unless the encumbrances have been approved by the City where reasonable;
 - iii. Be of a usable shape, topography and size that reflects its intended use iv) Meet applicable provincial soil regulations; and
 - iv. Meet the minimum standards for drainage, grading and general condition.
- **The Provence Orléans subdivision Phase 5B includes a future park block which will be transferred to the City of Ottawa at the time of registration. This *Zoning By-law Amendment* application will not result in any changes to the future park block which is already zoned as *Parks and Open Space – O1*.**

Subsection 4.5 – Cultural Heritage and Archaeology

Policy 4.5.4– Conserve sites of archaeological value

- 1) The City shall conserve sites of archaeological value where the City's Archaeological Resource Potential Mapping Study indicates archaeological potential, an archaeological assessment will be required and reviewed as per provincial standards. Where sites of archaeological value are identified on federal lands, the National Capital Commission is the approval authority.
 - 3) The Archaeological Resource Potential Mapping Study shows the historic core of the City (as defined by the City limits at the time of its incorporation in 1855) as having archaeological potential, upon discovery of any archaeological resource during the course of construction in the City's historic core area, the site shall be immediately protected from further disturbance until a licensed archaeologist has completed an archaeological assessment and any necessary mitigation has been completed.
- **The Subject Site is not identified as containing “Archaeological Potential” as per the City of Ottawa’s GeoOttawa overlay.**
 - **A Stage 1 Archaeological Assessment was prepared by Paterson Group dated March 15, 2018 as part of the overall Provence Orléans subdivision. A subsequent Stage 2 Archaeological Assessment was completed by Paterson dated June 20, 2018. The report concluded that no archaeological remains, artifacts, or culturally significant soil profiles were encountered and no further archaeological study is required.**

- **A Review and Entry into the Ontario Public Register of Archaeological Reports letter was prepared by the Ministry of Tourism, Culture, and Sport (MTCS) dated September 5, 2018. The letter confirms that the MTCS is satisfied that the fieldwork and report are consistent with the MCM's *Standards and Guidelines for Consultant Archaeologists (2011)* and terms and conditions for archaeological licenses.**

Subsection 4.6 – Urban Design

Policy 4.6.1 – Promote design excellence in Design Priority Areas

- 1) *Design Priority Areas (DPAs) define the image of Ottawa as the capital of Canada, as a city of vibrant neighbourhoods and as a hub of economic activity. Many DPAs are centres of pedestrian activity, and certain areas will expect significant change and growth in accordance with this Plan. Design Priority Areas are identified in order to promote design excellence through the development review process, and with respect to capital projects in the public realm. They are identified on Schedule C7A and C7B, and include selected areas described in the tiers of priority outlined in Table 5, DPA Tiers of Priority and the following policies apply:*
 - a) *DPAs may be identified or modified by City Council through the designation of new Special Districts, the creation of new or updates to existing City plans or policies.*
 - b) *The City may adopt an Urban Design Framework that will guide the level of urban design review undertaken by City staff and the Urban Design Review Panel (UDRP). This may include criteria for the review of projects by the UDRP, such as different thresholds, or exemptions for review based on the 98 framework outlined in Table 5. This framework, along with the Public Realm Master Plan, may determine distinct DPAs for the public realm and for development review, once these documents are adopted.*
 - c) *Development review within the Kanata North Economic District will be guided by applicable policies of the Plan, including the Special Economic District policies contained in Section 6.6.3.2, and use of the UDRP will be optional.*
- **The suggested zoning will not result in any revisions to the approved layout or roadway configuration of the approved Draft Plan of Subdivision.**
 - **The suggested zoning amendments will facilitate the construction of a new detached dwelling and standard townhome design that is compatible with existing and planned residential development within the Provence Orléans subdivision.**
 - **Please refer to the approved Planning Rationale, Urban Design Brief, and Integrated Environmental Review Statement (IERS) prepared by Novatech dated November 9, 2018 for a detailed review of the City of Ottawa's *Urban Design Guidelines for Greenfield Neighbourhoods (2007)* and *Building Better and Smarter Suburbs (2015)*. The approved report discusses the applicable urban design guidelines pertaining to the layout and configuration of the approved subdivision design.**

Policy 4.6.6 – Enable the sensitive integration of new development of Low-rise, Mid-rise and High-rise buildings to ensure Ottawa meets its intensification targets while considering liveability for all

- 6) *Low-rise buildings shall be designed to respond to context, and transect area policies, and shall include areas for soft landscaping, main entrances at-grade, front porches or*

balconies, where appropriate. Buildings shall integrate architecturally to complement the surrounding context.

- **No amendments for building heights greater than low-rise or 4-storeys are proposed as part of this *Zoning By-law Amendment* application.**
- **The new detached dwelling and standard townhome design will be low-rise and include areas for soft landscaping with main entrances at grade.**

Subsection 4.7 – Drinking Water, Wastewater, and Stormwater Infrastructure

Policy 4.7.1 – Provide adequate, cost-effective drinking water, wastewater and stormwater infrastructure, and assist in meeting growth targets in the urban area

- 1) *To protect, improve or restore the quality and quantity of water in any receiving watercourse, development shall:*
 - a) *Conform to approved servicing plans including the Infrastructure Master Plan, the Strategic Asset Management Plan, the Wet Weather Infrastructure Master Plan, subwatershed studies or environmental management plans, approved master servicing studies and applicable local servicing studies; and*
 - b) *Not exceed the capacity of the existing infrastructure system.*
- 2) *The City will require that infrastructure is durable, adaptive and resilient to the current climate and future climate, including extreme weather events.*
- 3) *In order to mitigate the impacts of development and climate change on drainage systems, local plans will:*
 - a) *Demonstrate integration of receiving watercourse assessments and required mitigating works with the development of local plans, master drainage plans, environmental management plans and master servicing studies; and*
 - b) *Identify requirements for Low Impact Development and implementation plans in environmental management plans and/or master servicing studies based on water budget calculations.*
- 4) *The City will prepare and implement programs and policies to address the need for stormwater capacity to support a range of intensification development applications. This will include:*
 - a) *Design and study requirements for on-site stormwater management, considering a range of intensification development application; and*
 - b) *Regulatory mechanisms for on-site stormwater management to provide adequate capacity to support intensification in existing neighbourhoods.*
- 5) *Stormwater management to support development shall be appropriate to the urban or rural context as defined by transect areas and each of the following:*
 - a) *The requirements of approved subwatershed studies, environmental management plans and master servicing study;*
 - b) *Other relevant Council-approved studies, such as stormwater retrofit studies;*
 - c) *The Ottawa Sewer Design Guidelines and associated climate change considerations; and*
 - d) *The Ottawa Macdonald-Cartier International Airport Zoning Regulations as enacted under the federal Aeronautics Act.*
- 6) *As part of a complete application, all redevelopment applications will be required to:*

- a) *Identify and mitigate the impacts of additional runoff resulting from increased imperviousness through measures such as site-specific stormwater management; and*
 - b) *Implement site, grading, building and servicing design measures to protect new development from urban flooding.*
 - 7) *As part of a complete application, new applications for a plan of subdivision or condominium must demonstrate, to the satisfaction of the City and based on requirements identified in the Infrastructure Master Plan, that a legal and sufficient outlet can be established to support the proposed development.*
 - 8) *As a condition of Site Plan Control approval or draft plan of subdivision approval, proof of legal and sufficient outlet for proposed stormwater management and drainage systems will be required and satisfied prior to the early servicing or registration, whichever comes first.*
- **A Site Servicing and Stormwater Management Brief prepared by Novatech dated October 2025 has been submitted as part of the detailed design review by the City of Ottawa which confirms that the stormwater management criteria for the development can be achieved with the suggested zoning.**

Subsection 4.8 – Natural Heritage, Greenspace and the Urban Forest

Policy 4.8.1 – Protect the City’s natural environment through identification of a Natural Heritage System, Natural Heritage Features and related policies

- 1) *The Natural Heritage System consists of core natural areas and natural linkage areas. Natural Heritage Features occur both inside and outside the Natural Heritage System. The Natural Heritage System and the features within it are subject to a higher standard of protection than features outside the Natural Heritage System. Schedule C11 identifies Ottawa’s Natural Heritage System and, to the extent possible, Ottawa’s Natural Heritage Features as overlays. Natural Heritage Overlay policies appear in Subsection 5.6.4.*
- 2) *The City shall seek to improve the long-term integrity and connectivity of the Natural Heritage System through land use planning, development processes, acquisition and conservation of land and support for voluntary, private land conservation and stewardship.*
- 3) *The City recognizes the following natural heritage features, as defined in Ottawa’s Environmental Impact Study Guidelines:*
 - a) *Significant wetlands;*
 - b) *Habitat for endangered and threatened species;*
 - c) *Significant woodlands;*
 - d) *Significant valleylands;*
 - e) *Significant wildlife habitat;*
 - f) *Areas of Natural and Scientific Interest;*
 - g) *Urban Natural Features;*
 - h) *Natural Environment Areas;*
 - i) *Natural linkage features and corridors;*
 - j) *Groundwater features;*
 - k) *Surface water features, including fish habitat; and*
 - l) *Landform features.*

- 4) *The natural heritage overlay policies apply to all features in Policy 3) regardless of whether they appear on Schedules to the Official Plan.*
- **The Subject Site has no natural heritage features as per the *City of Ottawa Official Plan Schedule C11-C – Natural Heritage System (East)*. An *Urban Natural Feature* is identified south of the Subject Site.**
 - **The Subject Site does not feature any urban greenspaces as per the *City of Ottawa Official Plan Schedule C12 – Urban Greenspace*.**
 - **The Subject Site is not identified to be within a *Flood Plain* or containing any *Unstable Slopes* as per the *City of Ottawa Official Plan Schedule C15 – Environmental Constraints*.**
 - **An *Environmental Impact Statement* was prepared by Muncaster Environmental Planning Inc. dated July 4, 2018, and revised on September 10, 2018. The report concludes that the Subject Site has been identified as a suitable location for the proposed residential land development from a natural heritage perspective.**
 - **This *Zoning By-law Amendment* application will not result in any impacts to any natural heritage features.**

Policy 4.8.2 – Provide residents with equitable access to an urban forest canopy

- 1) *Ottawa’s urban forest includes all of the trees, and their growing environments, whether they grow singly, in groups or in woodlands, on both public and private property.*
- 2) *The City shall pursue an urban forest canopy cover target of 40 per cent with equity as a guiding principle, in part through the development of sub-targets based on evolving urban form, climate resiliency, and environmental factors.*
- 3) *Growth, development and intensification shall maintain the urban forest canopy and its ecosystem services, in accordance with Subsection 4.8.2, Policy 6) and the following:*
 - a) *Preserve and provide space for mature, healthy trees on private and public property, including the provision of adequate volumes of high-quality soil as recommended by a Landscape Architect;*
 - b) *On urban properties subject to site plan control or community planning permits, development shall create tree planting areas within the site and in the adjacent boulevard, as applicable, that meet the soil volume requirements in any applicable City standards or best management practices or in accordance with the recommendation of a Landscape Architect;*
 - c) *Planning and development decisions, including Committee of Adjustment decisions, shall have regard for short-term, long-term and cumulative impacts on the urban forest at the neighbourhood and urban-wide scale;*
 - d) *When considering impacts on individual trees, planning and development decisions, including Committee of Adjustment decisions, shall give priority to the retention and protection of large, healthy trees over replacement plantings and compensation; and*
 - e) *Planning and development review processes shall support the goals and effective implementation of the Tree Protection By-law, including early consideration of trees in application and business processes.*
- 4) *The City shall consider trees to be an important element in:*
 - a) *Infrastructure design, especially in conjunction with Low Impact Development;*
 - b) *Good urban design;*
 - c) *Good park design;*

- d) *The design of the City's active mobility network; and*
 - e) *The design of local connections to the City's transit network.*
- **Accompanying Conceptual Site Plan and Landscape Plan have been prepared by Novatech in support of the *Zoning By-law Amendment* application to illustrate the suggested zoning amendments as shown in Appendix C. The Conceptual Landscape Plan demonstrates that the suggested zoning amendments will not impact soil volume requirements or streetscape tree planting required as part of the proposed development while acknowledging limitations due to servicing and utility conflicts. A Phase 5B Landscape Plan Drawing 117155-L5B REV 1 prepared by Novatech dated October 14, 2025 has been submitted as part of the detailed design review by the City of Ottawa and included in Appendix C as well.**

3.2.3 Transect Policy Area

Section 5 of the *Official Plan* addresses the six concentric policy areas called transects that represent a different gradation in the type and evolution of built environment and planned function of the lands within it, from most urban (*Downtown Core*) to least urban (*Rural*).

Subsection 5.4 – Suburban Transect

The *Suburban Transect* comprises neighbourhoods within the urban boundary located outside the *Greenbelt*. The objectives of the *Suburban Transect* as per Section 5.4 of the *Official Plan* include an evolution towards 15-minute neighbourhoods, enhancing mobility options and street connectivity, and provide direction for new development.

Policy 5.4.1 – Recognize a suburban pattern of built form and site design while supporting an evolution towards 15-minute neighbourhoods:

- 1) *The Suburban Transect's established pattern of built form and site design, in the existing built-up areas, is suburban, as described in Table 6, reflective of the conventional model described in Table 8.*
 - 2) *The Suburban Transect is generally characterized by Low- to Mid-density development. Development shall be:*
 - a) *Low-rise within Neighbourhoods;*
 - 3) *In the Suburban Transect, this Plan shall support:*
 - a) *A range of dwelling unit sizes in:*
 - i. *Multi-unit dwellings in Hubs and on Corridors; and*
 - ii. *Predominantly ground-oriented housing forms in Neighbourhoods located away from rapid transit stations and Corridors, with Low-rise multi-unit dwellings permitted near street transit routes*
- **The Subject Site is designated as *Neighbourhood* with an *Evolving Neighbourhood Overlay* as per the *City of Ottawa Official Plan Schedule B8 – Suburban (East) Transect*.**
 - **A low-rise, residential subdivision is proposed and will consist of architectural styles, building form, and setbacks that will be compatible with existing and planned future residential development.**

Policy 5.4.4 – Provide direction for new development in the Suburban Transect

- 1) *Greenfield development in the Suburban Transect will contribute to the evolution towards 15-minute neighbourhoods to the extent possible by incorporating:*
 - a) *A planned arrangement of streets, blocks, buildings, parks, public art, greenspaces, active transportation corridors and linear parks that create a sense of place and orientation, by creating view corridors, focal points and generally framing a high-quality public realm;*
 - b) *A fine-grained, fully-connected grid street network with short blocks that encourage connectivity and walkability and define greenspaces. All streets shall be access streets. Rear lanes shall be encouraged where appropriate to improve urban design and minimize curb cuts across sidewalks in order to support safer and more comfortable pedestrian environments. The Tewin community will be structured on a fine-grained, fully-connected street network that reflects Algonquin placekeeping and design principles;*
 - c) *Traffic flow and capacity may be permitted provided it minimizes negative impacts on the public realm, and maintains the priority of sustainable modes of transportation, and the safety of vulnerable road users;*
 - d) *Active transportation linkages that safely and efficiently connect residential areas to schools, places of employment, retail and entertainment, parks, recreational facilities, cultural assets and transit, natural amenities and connections to the existing or planned surrounding urban fabric, including to existing pedestrian and cycling routes;*
 - e) *Hubs and corridors that act as the focal point of the neighbourhood, consisting of higher-density residential, office employment, commercial services catering to neighbourhood, as well as neighbourhood or regional needs, and community infrastructure such as recreational facilities or institutional uses;*
 - f) *Hubs and corridors that closely integrate and safely connect pedestrians and cyclists to surrounding neighbourhoods, are oriented to reinforce the neighbourhood-focus function of streets, and that can be conveniently accessed by public transit, including rapid transit where relevant;*
 - g) *Treed corridors, including arterial roads and collector streets that are lined with building typologies containing small-scale, street-oriented convenience and neighbourhood commercial services and other neighbourhood-oriented uses, including medium-density residential uses;*
 - h) *Avoiding rear lotting on higher traffic streets by providing rear lane access for properties along arterials and major collector roads, or parallel local streets (window streets) and rear lanes for properties along arterials;*
 - i) *Screened parking lots, where surface parking is proposed, with visual impacts on the public realm mitigated by setbacks, landscaping, location on site or a combination of these measures; and*
 - j) *Planned design which optimizes the available supply, means of supplying, efficient use and conservation of energy.*
- **The suggested zoning will not result in any revisions to the approved layout or roadway configuration of the approved Draft Plan of Subdivision.**
 - **The suggested zoning amendments will facilitate the construction of a new detached dwelling and standard townhome design that is compatible with existing and planned residential development within the Provence Orléans subdivision.**

- **No amendments for building heights greater than low-rise or 4-storeys are proposed as part of this *Zoning By-law Amendment* application.**
- **Please refer to the approved Planning Rationale, Urban Design Brief, and Integrated Environmental Review Statement (IERS) prepared by Novatech dated November 9, 2018 for a detailed review of the City of Ottawa’s *Urban Design Guidelines for Greenfield Neighbourhoods (2007)* and *Building Better and Smarter Suburbs (2015)*. The approved report discusses the applicable urban design guidelines pertaining to the layout and configuration of the approved subdivision design.**

Policy 5.4.5 – Provide direction to Neighbourhoods located within the Suburban Transect

- 1) *Neighbourhoods located in the Suburban Transect and within a 15-minute neighbourhood shall accommodate residential growth to meet the Growth Management Strategy as outlined in Subsection 3.2, Table 3. The Zoning By-law shall implement the density thresholds in a manner which adheres to the built form requirements as described in Subsection 5.6.1 – Built Form Overlays, as applicable and that:*
 - a) *Allows and supports a wide variety of housing types with a focus on missing-middle housing, which may include new housing types that are currently not contemplated in this Plan;*
 - b) *Generally provides for up to 3 storey height permission, and where appropriate 4 storey height permissions to allow for higher-density Low-rise residential development; and*
 - c) *Provides an emphasis on regulating the maximum built form envelope, based on the context, that frames the public right of way.*
- **The suggested rezoning will contribute to a greater market-based range and mix of residential types within the Provence Orléans community.**
 - **No amendments for building heights greater than low-rise or 4-storeys are proposed as part of this *Zoning By-law Amendment* application.**

3.2.4 Overlay Policy Area

Subsection 5.6 – Overlays

The *Overlays* are intended to complement the underlying designations and provide additional policy direction to allow certain types of activities and provide built form guidance in evolving areas that are otherwise not included in the designation sections of the *Official Plan*. The objective of the *Overlay* as per *Section 5.6* of the *Official Plan* is to provide built form direction for the urban area where intensification is anticipated to occur.

Policy 5.6.1 – Evolving Neighbourhood Overlay

- 1) *The Evolving Neighborhood Overlay will apply to areas that are in a location or at stage of evolution that create the opportunity to achieve an urban form in terms of use, density, built form and site design. These areas are proximate to the boundaries of Hubs and Corridors as shown in the B-series of schedules of this Plan. The Evolving Neighborhood Overlay will be applied generally to the properties that have a lot line along a Minor Corridor; lands 150 meters from the boundary of a Hub or Mainstreet designation; and to*

lands within a 400-metre radius of a rapid transit station. The Overlay is intended to provide opportunities that allow the City to reach the goals of its Growth Management Framework for intensification through the Zoning By-law, by providing:

- a) Guidance for a gradual change in character based on proximity to Hubs and Corridors,
 - b) Allowance for new building forms and typologies, such as missing middle housing;
 - c) Direction to built form and site design that support an evolution towards more urban built form patterns and applicable transportation mode share goals; and
 - d) Direction to govern the evaluation of development.
- 2) Where an Evolving Neighborhood Overlay is applied:
- a) The Zoning By-law shall provide development standards for the built form and buildable envelope consistent with the planned characteristics of the overlay area, which may differ from the existing characteristics of the area to which the overlay applies; and
 - b) The Zoning By-law shall include minimum-density requirements as identified in Table 3a, and permissions to meet or exceed the density targets of Table 3b.

- **The suggested rezoning will achieve an efficient development and land use pattern by efficiently utilizing the Subject Site's land resources for residential development while contributing to additional density within the Suburban Transect.**
- **The amendments as part of the suggested zoning will maintain a built form and buildable envelopment that is compatible with the previously approved phases of the Provence Orléans subdivision.**

3.2.5 Designation

Section 6.3 – Neighbourhoods

The *Official Plan* notes that neighbourhoods are contiguous urban areas that make up the heart of communities. The intent of the *Official Plan* is to allow *Neighbourhoods* to permit a mix of building forms and densities. It is acknowledged that not all *Neighbourhoods* are at the same stage of development, maturity, and evolution. The intent of the *Official Plan* is to reinforce those *Neighbourhoods* that contain all elements and presently function as 15-minute neighbourhoods, guide those that have missing elements to gain them, and enable conditions for future 15-minute neighbourhoods.

Policy 6.3.1 – Define neighbourhoods and set the stage for their function and change over the life of this Plan

- 1) *Neighbourhoods are designated on the B-series of schedules.*
- 2) *Permitted building heights in Neighbourhoods shall be Low-rise, except:*
 - a) *Where existing zoning or secondary plans allow for greater building heights; or*
 - b) *In areas already characterized by taller buildings within the Neighbourhood designation.*
- 3) *Development in the Neighbourhood designation which seeks additional height beyond 4 storeys:*
 - a) *May be evaluated through a Zoning By-law amendment, without the need to amend this Plan, in cases that fall under the provisions of Subsection 6.3.1 Policy 2) but where the zoning does not provide corresponding permissions; and*

- b) *In all other cases, require an area-specific policy through an amendment to this Plan*
- 4) *The Zoning By-law and approvals under the Planning Act shall allow a range of residential and non-residential built forms within the Neighbourhood designation, including:*
- a) *Generally, a full range of Low-rise housing options sufficient to meet or exceed the goals of Table 2 and Table 3b;*
 - b) *Housing options with the predominant new building form being missing middle housing, which meet the intent of Subsection 6.3.2, Policy 1);*
 - c) *In appropriate locations including near rapid-transit stations, zoning may prohibit lower-density housing forms.*
 - d) *To provide for a range of local services and promote the emergence or strengthening of 15-minute neighbourhoods, the Zoning By-law may permit compatible and complementary small-scale non-residential uses and services (including retail, service, cultural, leisure and entertainment uses) that primarily serve residents within walking distance and that:*
 - i. *Are compatible with, and do not reasonably pose a risk of nuisance to, nearby residential uses;*
 - ii. *Are contained within building forms and site design compatible with low-rise, predominantly residential neighbours;*
 - iii. *Are appropriately integrated with the neighbourhood street network, pedestrian network and public realm;*
 - iv. *May establish building and site design standards specific to such uses, in order to ensure functional requirements and context sensitive building form are met;* v) *May restrict or prohibit motor vehicle parking in association with such uses; and*
 - v. *Limits such uses to prevent undue diversion of housing stock to non-residential use.*
- **The suggested zoning will contribute to a greater market-based range and mix of residential types within the Provence Orléans community. Specifically, the 27' detached dwelling and standard townhome designs will seek to provide for a more attainable housing option for future homeowners.**
 - **No amendments for building heights greater than low-rise or 4-storeys are proposed as part of this *Zoning By-law Amendment* application.**

Policy 6.3.2 – Guide the evolution of neighbourhoods based on their context, location, age, maturity and needs, generally towards the model of 15-minute neighbourhoods

- 1) *The Zoning By-law and approvals under the Planning Act will allow innovative buildings forms, including in the missing middle housing category, in order to strengthen, guide towards or seed conditions for 15- minute neighbourhoods. Innovative building forms include, but are not limited to: adaptive reuse of existing buildings into a variety of new uses; development of existing shopping centres; co-location of housing above City facilities including those facilities on land dedicated by parkland (libraries and recreation centres) as per Subsection 4.4.6, Policy 3), City-owned or other; development of a single lot or a consolidation of lots to produce missing middle housing; and by providing air-rights for housing above City infrastructure and facilities, including transit facilities*
- **It is suggested that the low-rise residential subdivision designated as *Neighbourhood* be rezoned to a *Residential Third Density, Subzone YY, Urban Exception 2989* –**

R3YY[2989] zone which permits various residential land uses, housing typologies, and a maximum building height of 4-storesys.

- **The proposed development will feature low-rise residential subdivision. The low-rise residential subdivision will consist of architectural styles, building form, and setbacks that will be compatible with existing and planned future residential development.**

3.2.6 Protection of Health and Safety

Section 10 – Protection of Health and Safety

Environmental conditions whether occurring naturally or not can result in hazards to human life or health and damage or loss to property value. The environmental conditions or constraints to development may include natural hazards such as flood plains and unstable soils to hazards resulting from human activity such as contaminated sites, mine hazards, and land affected by noise. In accordance with *Section 10* of the *Official Plan*, development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to health or safety or of property damage and shall not create new or aggravate existing hazards.

Policy 10.1 – Prevent Injury, loss of life and property damage

Policy 10.1.1 – Natural Hazards: Flooding Hazards and Erosion Hazards

- 1) *Development and site alteration shall not be permitted in the 1 in 100-year flood plain or in an erosion hazard area.*

Policy 10.1.4 – Natural Hazards: Unstable soils or bedrock

- 1) *Development shall generally be directed to areas outside of unstable soils or bedrock as defined as a Hazardous Site in the Provincial Policy Statement.*
- **The Subject Site is not identified to be within a *Flood Plain* or containing any *Unstable Slopes* as per the *City of Ottawa Official Plan Schedule C15 – Environmental Constraints*.**
 - **This *Zoning By-law Amendment* application will not result in any natural or human-made hazards.**

Policy 10.1.6 – Contaminated sites

- 1) *Required environmental site assessments, remedial or risk assessment/risk management activities and associated reports shall be completed in compliance with the applicable regulations and requirements, as amended, and shall be completed, signed and stamped by a Qualified Person as defined by applicable regulation. Where required, the City may use the holding provisions of the Planning Act to ensure receipt of satisfactory verification of suitable environmental condition prior to development.*
- 2) *Phase one environmental site assessment reports shall include the City of Ottawa Historical Land Use Inventory reports and all other required environmental information sources that are reasonably and publicly available through public regulatory bodies and industry standard private environmental databases.*

- 3) *When contamination is identified in a proposed development property, the contaminants are to be fully delineated and a remedial action plan and/or a risk assessment / risk management plan to address the contamination shall be submitted to the City. A building permit may be issued on a phased basis to allow for the implementation of the plans concurrent with the required demolition and earth work.*
 - 4) *When the filing of a Record of Site Condition is required, a copy of the Record of Site Condition and all supporting documentations along with the Ministry's acceptance shall be submitted to City. The City shall consider a Record of Site Condition as acknowledged by the Ministry when:*
 - a) *It has been confirmed that the Record of Site Condition has passed the Ministry's audit; or*
 - b) *It has been confirmed that the Record of Site Condition will not be audited by the Ministry.*
- **A Phase I ESA was prepared by Paterson Group dated January 20, 2018. The findings and conclusions from the report include:**
 - **No environmental concerns were identified with respect to the historical use of the Subject Site, although it appears that there may be remnant building materials (concrete) present in the vicinity of the former house and farm buildings. The remnant building materials are not considered to pose a significant risk to the subject land but should be removed off-site for proper disposal in conjunction with future site development.**
 - **Two (2) potentially contaminating activities (PCAs) were identified at the City of Ottawa Trim Road Garage Depot, located within the Phase I-ESA study area, however, these PCAs are not considered to represent areas of potential environmental concern on the Subject Site.**
 - **Based on the findings of the assessment, a Phase II – Environmental Site Assessment is not required for the subject property.**

The Zoning By-law Amendment application is in conformity with the policies of the City of Ottawa Official Plan (2022).

4.0 PUBLIC CONSULTATION STRATEGY

- The City will be posting signage on the Subject Site which provides members of the public with details of the proposed development and means of contacting the file lead to provide comments and/or questions.
- Digital copies of all required supporting studies and plans will be made available for public viewing through the City of Ottawa's Development Applications webpage (<https://devapps.ottawa.ca/en/>).
- Community organization(s) will be notified of the details of the proposed development through a 'heads up' by City of Ottawa staff.

5.0 CONCLUSION

It is our assessment that the proposed development is consistent with the *Provincial Planning Statement (2024)*, conforms to the *City of Ottawa Official Plan (2022)* and complies with the provisions of *Zoning By-law 2008-250*. This Planning Rationale, Public Consultation Strategy, and Urban Design Brief along with all updated and previously approved associated technical studies support the proposal to rezone the Subject Site.

The proposed development is an appropriate and desirable addition to the Orléans community and represents good planning.

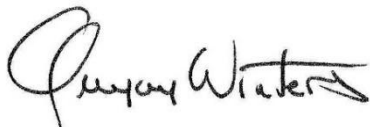
NOVATECH

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Reviewed by:



Greg Winters, MCIP, RPP
Director, Planning & Development

**Appendix A
Approved Draft Plan of Subdivision
Dated July 5, 2019**

Appendix B
Preliminary 4M-Plan
Prepared by Annis O’Sullivan Vollebekk
Dated August 13, 2025

Appendix C
Conceptual Site Plan dated September 4, 2025 and
Conceptual Landscape Plan dated September 8, 2025
Phase 5B Landscape Plan dated October 14, 2025
Prepared by Novatech





